

GARRISON CONCOURSE

SHPO INV. # CW-GRC-001

Location: The concourse is located at the junction of T.H. 169, T.H. 18 and Central Avenue in the City of Garrison.

Introduction: The large stone concourse was built by the CCC between 1936 and 1939. Much of the site remains as it was originally designed. Several traffic islands are located west of the overlook wall. A sandy swimming beach was constructed to the north of the wall and the lakefront to the south is riprapped with boulders and filled with natural wetland vegetation. Changes at the concourse include a prominently mounted fiberglass "walleye", the original landscaping in the island, the granite curbing and flagstone walk along the inside of the wall, the single-lane approach into the concourse and the wood-framed Visitor's Information booth.

The base of the wall at the water front shows signs of deterioration—rock facing is missing and has fallen into the water, stone joints require repairing and stabilizing, etc. In several locations along the walkway granite is missing. The site is an excellent example of the craftsmanship and quality of construction completed during the period of the CCC.

Architect's Survey Date: October 7, 1999

Plans/Sketches:

1. MHD Dimensioned Plan, SP 15, 154B, dated 12/36; p. 1/1
2. MHD Concourse Plan "9-034", dated 12/37; p. 1/2
3. MHD Planting Plan "9-034", dated 12/37; p. 2/2
4. MHD Planting Plan around Concourse "9-034", dated 4/38; p. 4/4
5. MHD Accomplishments Map, dated 3/3/39
6. Gemini Sketch, dated 1998, with MJBA field notes, dated 10/99
7. MnDOT "Planned Construction in Garrison" and Office Memo from G. Bottemiller to C. Braun, dated 9/98 showing proposed locations of new flagpoles and the current location of the site access road.
8. MJBA Field Note Sketches (2 pages), dated 10/7/99
9. MJBA Restoration Recommendations on MHD #9-034, p. 1/2

**MNDOT HISTORIC ROADSIDE DEVELOPMENT
STRUCTURES INVENTORY**

CW-GRC-001
CS 1804
Garrison Concourse

Historic Name Other Name	Garrison Concourse	CS # SHPO Inv #	1804 CW-GRC-001
Location	E side of TH 169 at the jct of TH 169 and TH 18	Hwy District Reference Point	TH 169 3A 233.7
City/Township County Twp Rng Sec USGS Quad UTM	Garrison, City of Crow Wing 44N 28W Sec 13 Garrison Z15 E436550 N5126810	Acres Rest Area Class	1 4
Designer	National Park Serv (Unspecified) Nichols, A R, Consult Land Arch	SP #	169-18-23-4 169-35-23-1 1804-08
Builder	Civilian Conservation Corps (CCC)	SHPO Review #	
Historic Use Present Use	Roadside Parking Area Roadside Parking Area	MHS Photo #	013532.09-24 013533.01-11
Yr of Landscape Design	1936-39	MnDot Historic Photo Album	Nic 1.12 Nic 1.13' Nic 1.24 Nic 1.31 Nic 5.17 Nic 5.18 Nic 7.33 Ols 1.58 Ols 1.59 Ols 1.60
Overall Site Integrity	Intact/Slightly Altered		
Review Required	Yes		
National Register Status	Eligible, see Statement of Significance		
Historic Context	Federal Relief Construction, 1933-1943 Roadside Development on Minnesota Trunk Highways, 1920-1960		

Table of Site Structures

Feat #	Type	Year Built
01	Marker	1927
02	Overlook Wall	1936-39
03	Curb, Stone	1937-39
04	Flagpole, Stone	Ca. 1939
05	Bench(es), Other	Ca. 1939
06	Other Feature	Ca. 1985
07	Info Booth	Ca. 1990

NOTE: Landscape features are not listed in this table

Fieldwork Date	08-03-97
Prep by	Gemini Research Dec. 98 G1. 25
Prep for	Site Development Unit Cultural Resources Unit Environmental Studies Unit

Stabilization/Preservation/Restoration

1. Spatial Organization and Land Patterns

a. Functional Relationships:

- Assessment: The Garrison Concourse is located at the junction of T.H. 169 and T.H. 18 on the western shore of Mille Lacs Lake, adjacent to Garrison's central business district. T.H. 169 travels in a north-south direction through the site. T.H. 18 angles through its northern end.

The original site design included:

- 1) a stone concourse that projects into the lake,
- 2) an oblong island set within the concourse,
- 3) a smaller oblong island at the entrance to the concourse,
- 4) adjacent shoreline north and south of the concourse, and
- 5) the T.H. 169/18 intersection. The highway intersection originally included six landscaped traffic control islands that aesthetically enhanced the intersection, safely channeled vehicles through it, and visually supported the concourse. (There were also four long, narrow islands on T.H. 18 to the northwest, outside of the site boundaries.) Sheet 4 of the CCC "Accomplishment Map" for Minnesota S.P. 15 (the Garrison CCC camp) shows the original site plan. (Signed March 1939, the Accomplishment Map depicts CCC roadside development along Mille Lacs between the Aitkin/Crow Wing county line and Vineland Bay, and extending north of Garrison along T.H. 18 to the T.H. 6 junction.) The plan sheet entitled "Planting Plan Around Concourse" (plan no. 9-034, dated March 31, 1938) also shows the T.H. 169/18 junction, but does not extend as far along T.H. 18 to the northwest.

- 1) The concourse itself is relatively intact. (See Overlook Wall below.)
- 2) The island within the concourse was designed with granite curbing, which is intact, and four structures: a granite flagpole base and four curved concrete benches. One of the benches is missing. (See Benches and Marker below.) The island had a formal landscape design that is only partly intact.
- 3) The oblong entrance island, which was originally encircled by granite curbing, was removed in 1982 (S.P. 1804-44).
- 4) Landscaping on the shoreline within the site included beach improvement. The eastern edge of T.H. 169 was originally lined with granite curbing that extended several hundred feet north and south of the concourse entrance drive. About 640' of this curbing, all located south of the concourse, is intact. The original plans also show a curb-cut access to the shoreline located about north of the concourse. It is apparently gone. A similar curb-cut access, located about 475' south of the concourse centerline, does not show on the plans, but is presumed to be original because of the pattern of its granite curbing. The shoreline north of the concourse includes one structure, the Tauer Marker. Fabricated in 1927, the marker predates the site design. (The marker does not appear on original plans. It is not known whether it has always stood at its current location.) There are also two culvert outlets built of lake boulders north of the concourse. They are presumed to be CCC-built.
- 5) The six landscaped islands in the T.H. 169/18 junction were originally encircled by granite curbing. By 1981 the two largest islands had been merged into one long island, and the smallest, northwesternmost island had been lengthened. (Note: by 1981 the four, narrow, stone-curbed islands outside of the site boundaries on T.H. 18 had also been removed.) In 1982 the shape of the triangular island northwest of the concourse was changed somewhat, two narrow concrete medians were built east of it, and new concrete curbs were installed throughout the intersection. (See Sheet 19 of 40, S.P. 1804-44, for existing conditions in 1981 and for the 1982 changes. The lower drawing on Sheet 19 also appears to match current conditions.)

- Recommendations:

Stabilization: None.

Preservation: It is recommended that Mn/DOT retain ownership of all existing right-of-way within Garrison to continue visual enhancement of the intersection and to help buffer the site from future development. This includes public ownership of land adjacent to one of the state's largest lakes -- a role that may become increasingly important as the region experiences more commercial and residential development in the future.

Restoration: Same as Preservation with the following addition: Replace the island that was removed from the entrance to the concourse drive returning this entrance to its original design.

b. Visual Relationships:

- Assessment: The concourse itself provides an exceptional view of Mille Lacs Lake, one of the few lakes in the state that is so large that one cannot see the opposite shoreline. The view from the concourse also includes beaches to the north and south; the T.H. 169/18 roadbeds to the west, northwest, and southwest; and the commercial buildings of downtown Garrison to the west, northwest, and southwest.

The site was designed to be viewed by vehicles driving through it, by city residents and customers in downtown Garrison, by visitors on the adjacent beaches, and even by boats on the lake.

Currently views from the site are fairly intact with two changes: the number of cars on the highway has increased considerably, and surrounding commercial buildings have increased in number and have been remodeled and replaced. Despite these changes, however, surrounding land use patterns are consistent with those of the 1930s, and the T.H. 169/18 junction still retains its grass- and tree-planted islands and shoreline.

The site is threatened by highway expansion and likely commercial and residential development nearby. T.H. 169 is scheduled to be widened to a four-lane highway and/or realigned, and the T.H. 169/18 intersection will be redesigned. The impact of these highway changes is likely to seriously alter the site.

- Recommendations:

Stabilization/Preservation/Restoration: Retain Mn/DOT ownership of all existing right-of-way in the vicinity. (See Spatial Organization above.) Retain all tree- and grass-planted islands, boulevards, and shoreline. Retain open views north and south down the shoreline from the concourse.

2. Topography:

- Assessment: The site is generally flat. The park area to the north of the wall slopes gently toward the swimming beach and lakefront. The natural terrain to the north and south of the wall remains as it was.

- Recommendations: None

3. Vegetation

- Assessment: The plant materials shown on the plan sheet "Planting Plan Around Concourse" (plan no. 9-034, dated March 31, 1938) appear to match those seen in original photos of the site. The concourse site was landscaped by the CCC, which also landscaped T.H. 169 and T.H. 18 north and south of the Garrison.

The original planting plan intended that the concourse site, including its traffic islands, be softened with mowed grass and shaded with tall deciduous trees. The plan shows existing deciduous and evergreen trees on the shoreline north and south of the concourse. It specifies that two groups of 18 Common Juniper shrubs be planted on the island at the entrance to the concourse. Four American

Elms are shown north and south of this island.

The island within the concourse is drawn with a more formal arrangement of 8 American Elms spaced in a grid and several rows of shrubs. The shrubs include 272 Morrows Honeysuckle defining and sheltering the stone flagpole area at the center of the island, and 80 Garland Spirea and 32 American Dogwood formally placed around the curved benches near the northern and southern ends of the island. (See plan for placement.)

The plan for the traffic islands in the T.H. 169/T.H. 18 intersection show similar plantings. The three long traffic islands on the western side of T.H. 169 were to be planted with groups of 4-9 American Elms at each end of each island and a cluster of 11-18 Black Hills Spruce between the elms. The triangular traffic island was to have a group of 9 Common Juniper shrubs placed at each point of the triangle. The semicircular island at the northern end of the site was to be planted with a line of 100 Red Rugosa shrubs flanked by 8-9 Common Juniper shrubs, with 6 American Elms providing shade. (See plan for placement.)

On the lakeshore north and south of the concourse, many trees shown on the original plans are gone. There are currently one mature spruce, two mature oak trees, and clumps of linden, ash, and a Norway pine north of the concourse, some planted circa 1975. South of the concourse, there are currently one birch and about four ash and linden, all planted circa 1975. The lakeshore both north and south of the concourse consists of sand beach at the water's edge and mowed grass adjacent to T.H. 169/T.H. 18, with scattered clumps of shade trees.

On the concourse island, most of the original plantings are gone. Currently there are about three Spirea near the northern end of the island and about six near the southern end, and three ash trees scattered about. Four circular flowerpots have been placed on the island and a fifth is in front of the information booth.

The traffic islands in the western half of the site currently have mowed grass and widely spaced trees, most of them deciduous. Few, if any, of these plantings appear to be original. Most probably date from circa 1975. The triangular island at the northern end of the site has no plants except mowed grass.

The eastern and western edges of the site (along both sides of T.H. 169) are planted with mowed grass and widely scattered trees, mostly deciduous. Some of the trees may date from the 1930s, but most appear to date from the 1960s or 1970s.

Nearly all American Elms within the site are gone. A few mature elms stand on the western side of T.H. 169 south of the site boundaries.

Most of the plants are in good condition.

- **Recommendations:**

Stabilization: Cut back weeds and brush from all stonework including curbs. Establish and follow a regular schedule of fertilizing, mowing, pruning, and trimming.

Work Period: ASAP.

Preservation: Cut back weeds and brush from all stonework including curbs. Remove circular flower pots from the site. Establish and follow a regular schedule of fertilizing, mowing, pruning, and trimming. **Work Period:** ASAP.

Restoration: Restore the original planting plan on all islands, lakeshore, highway right-of-way, and around the concourse itself, retaining existing trees. *Plantings on the concourse island should receive top priority.* If plants specified in the original plans are not available, use substitute plants of similar size, shape, color, and texture. Keep stone curbing free of weeds. Establish and follow a regular schedule of fertilizing, mowing, pruning, and trimming. **Work Period:** 1 - 3 years.

4. Circulation

a. Access roadways:

- Assessment: Both T.H. 169 and T.H. 18 travel through the site, T.H. 169 along its length and T.H. 18 at its northern end. Both highways serve the Mille Lacs-Aitkin-Brainerd lakes area and carry recreational traffic that is often heavy. T.H. 169 is one of the principal thoroughfares between the Twin Cities and Brainerd. (See Spatial Organization above for a description of the islands in the T.H. 169/18 junction.)

In addition to the two highways themselves, the site includes an entrance drive that loops around the concourse island. The drive was originally gravel and has been paved with asphalt. Overlays of asphalt now partly obscure the face of the stone curb. The drive was altered in 1982 when the small oblong island at its entrance was removed and the entrance was reduced to a single 44'-wide opening.

Madison Street, a local north-south street, runs along the western edge of the site. It was originally Garrison's primary commercial street and continues to serve the stores and restaurants that face the concourse and lake.

Traffic on T.H. 169 and T.H. 18 is often heavy through the site and travels at about 40 mph. Because of the relatively slow speeds, finding and turning into the concourse entrance drive is not difficult.

T.H. 169 is scheduled to be widened to a four-lane highway in the near future. The site's islands, granite curbing, plantings, access, and other features are likely to be affected by the project. One possible alternative (not currently preferred) is to bypass downtown Garrison entirely by shifting the highway westward and turning "old" T.H. 169 into a county road at this location.

- Recommendations:

Stabilization: None.

Preservation and Restoration: Keep the speed limit on T.H. 169 and T.H. 18 past the site as low as possible to allow vehicles to safely view the site and turn into the concourse entrance, and to support pedestrian movement. Regrade and repave the concourse's entrance drive/parking area to reestablish the original 6-inch curb visual face. Replace the island that was removed from the entrance to the concourse drive. Consider realigning T.H. 169 west of downtown to preserve the integrity of this historically and architecturally significant site, avoid traffic increases, and enhance pedestrian movement within the site and between the lakeshore and downtown Garrison. **Work Period:** 1 - 5 years.

b. Parking areas:

- Assessment: The site was designed with parking on both the eastern and western sides of the concourse island. The western side of the concourse island was designed with a rectangular cutout that accommodates several cars. (The length of this rectangular cutout was designed to match the length of the concourse's eastern lookout bay.) On the eastern side of the island, cars parallel park. Both parking areas are intact, although they have been paved with asphalt that has partly obscured the face of the stone curbing and obstructed the view and functioning of several catch basins. Small sections of the entrance/drive areas are cracked and warrant repair.

Additional parking is located along Madison Street, the local street that is located on the western edge of the site and serves the businesses of downtown Garrison. Visitors must cross T.H. 169 to walk from Madison Street to the concourse itself. The intersection of T.H. 169 and Central Avenue (which intersects Madison and T.H. 169) was originally designed with a pedestrian crosswalk.

It has been removed so that the site no longer has provision for safe pedestrian movement between Madison Street and the lakeshore.

- Recommendations:

Stabilization: Repair asphalt cracks in the entrance drive/parking area. **Work Period:** ASAP.

Preservation and Restoration: Reestablish a controlled pedestrian crosswalk at the intersection of T.H. 169 and Central Avenue to allow and encourage pedestrians to move between Madison Street and the concourse itself. Regrade and repave the concourse's entrance drive/parking area to reestablish the original curb depth.
Work Period: 1 - 5 years.

c. Pedestrian walks (flagstone surfaces)

- **Assessment:** The concourse structure was designed with a granite flagstone walkway, edged with granite curbing, that was replaced by a concrete walkway and concrete curbing in the early 1990s. This newer concrete sidewalk has a 4"-high concrete curb poured against the base of the granite overlook wall. The concrete walk slopes toward the entrance drive/parking area and provides curb cuts in three locations.

The concourse island originally had an intersecting series of gravel sidewalks. The original plan shows the surfaces as flagstone. North-south sidewalks ran along the eastern and western edges of the island and in two parallel lines down the center. East-west sidewalks ran through the island's midpoint and near the northern and southern ends where four curved benches were placed. (See 1947 MHD photo, envelope label 47-1074.) The island sidewalks have been altered so that there is just one east-west poured concrete sidewalk, which widens to a concrete plaza under the fish. (There is a 16'-square flagstone pad around the granite flagpole base. See Flagstone Pad below.)

A short section of concrete sidewalk has been added at the new information booth.

Several original plans show a walking trail along the beach north and south of the concourse. Currently there is no distinct shoreline walking trail visible, but the beaches are used for walking as per the original design intent.

The site was designed with a pedestrian crosswalk at the intersection of T.H. 169 and Central Avenue to allow pedestrians to walk between the concourse and Madison Street. This pedestrian crossing has been removed.

About 1960, poured concrete sidewalks were added along the western side of T.H. 169 starting about one block south of Central Avenue and continuing southward for about two blocks. Poured concrete sidewalks have also been added to T.H. 18 across the northern edge of the site (extending east and west from Madison St. to T.H. 18/T.H. 169).

Today, walking across T.H. 169/T.H. 18 and along the eastern side of the highway is often difficult due to the amount of traffic.

If curbstones are removed during the widening of T.H. 169, all pieces must be salvaged and reused in and around the concourse, wherever needed.

Current plans for the reconstruction of T.H. 169 include discussion of a bike trail along the western shore of Mille Lacs that would presumably include the concourse site.

Interpretive walking and biking trails could be developed that run from Garrison Creek (on the northern edge of Garrison) to at least the T.H. 169 Culvert at St. Alban's Bay (south of the Garrison Rest Area) and which link and interpret all of the CCC landscaping and stone structures along T.H. 169/T.H. 18 and Mille Lacs Lake.

- **Recommendations:**

Stabilization: None.

Preservation and Restoration: Remove the concrete sidewalk and curb from the inner side of the concourse. Carefully clean off concrete residue from granite wall. (See Overlook Wall below.) Install new granite flagstone of the same color range and textures that exist at the flagpole monument. (This assumes that the original

flagstone was removed prior to the concrete's installation.) Install new granite curbing to match curbing around concourse island. Curb cuts will be required. (See Accessibility Considerations below). Remove all asphaltic material that splashed onto the surface of the curbstones during the 2000 resurfacing project. **Work Period:** 1 - 3 years.

Remove existing concrete sidewalk and plaza under fish and restore the original pattern of gravel walkways on the concourse island. **Work Period:** 1 - 3 years.

Reestablish a controlled pedestrian crosswalk at the intersection of T.H. 169 and Central Avenue to allow pedestrians to move between Madison Street and the concourse itself. **Work Period:** 1 - 5 years.

Participate in plans for possible future development of a bike and/or shore trails through the site. Interpret all CCC-built landscaping and structures along T.H. 169 and T.H. 18. Any trail or similar new work must be done with the utmost sensitivity and carefully scrutinized before implementation. **Work Period:** 1 - 5 years.

5. Water Features: Not applicable

6. Structures, Furnishings and Objects

a. Bench (es), stone

- **Assessment:** Three curved, concrete seats atop two single pieces of granite remain on the island. Historic photos show four benches; two at the north and south ends each. The benches are not as originally located. The benches have settled unevenly and are no longer at optimum sitting height from the ground. The reinforced concrete seats have sagged also. Original plans and photos show seven additional benches along the wall, which were installed. The benches had concrete seats and stone bases. All are missing.

- **Recommendations:**

Stabilization/Preservation: Reset existing benches in present locations to provide a nearly level sitting plane with a sitting height of about 16" above grade. Secure seats to stone pedestals if necessary. **Work Period:** ASAP.

Restoration: On the island add one bench with a matching concrete seat and stone pedestals. Replace any deteriorated portions of the existing seats and pedestals matching design of the originals. All benches shall be reset or set plumb and relocated to their original locations with a finished seat height of about 16" above grade.

Install seven new matching benches along the overlook wall in the locations shown on the original design plans. Design and construction shall match the design shown on the 1937 design plans. **Work Period:** 1 - 3 years.

b. Curb, stone

- **Assessment:** Extensive granite curbing originally lined the concourse walkway, encircled the oblong island within the concourse, encircled the concourse entrance island, encircled the six islands in the T.H. 169/18 junction, encircled four narrow islands on T.H. 18 northwest of the site boundaries, and lined the eastern side of T.H. 169/18 both north and south of the concourse.

Only two sections of granite curbing remain today. The first is a 640'-long section located along the eastern edge of T.H. 169. The curbing begins about 385' south of the concourse centerline and continues southward along T.H. 169 for about 640'. It is interrupted by a 28'-wide curb cut for an access road onto the lakeshore. The curbing is comprised of granite blocks that are about 12" to 30" long. The blocks are rock-faced and appear to be about 8"-12" deep. Some of the blocks were placed so that their blasting tool marks are exposed (a technique seen in other federal relief stonework in the state and presumably used to add interest to the stonework). Originally, 6" of the curb face was exposed.

In 2000, the T.H. 169 roadway through the site was resurfaced with a mill and inlay, leaving less than 2" of curb face visible along some portions of the curbing. (Previous highway overlays had already reduced the curb height from 6" to about 2".) During the recent mill and inlay, asphalt and tar was inadvertently spilled on some portions of the granite curbing.

The second extant section of granite curbing encircles the concourse island. It is similar in color and style to the curbing just described. About 3"-6" of its original 6" curb face is exposed above the asphalt of the parking area.

- **Recommendations:**

Stabilization: None.

Preservation: Remove the broken stones and replace with new ones. Repoint all joints with matching mortar color and texture. Remove all asphalt material that splashed onto the surface of the curbstones during the 2000 resurfacing. **Work Period:** 1 - 3 years.

Restoration: Remove all broken stones and replace. Remove and repoint all joints. Clean faces of the curbstones of all asphalt and/or concrete. Restore the 6" exposed face dimension of the stones along the west edge of the island. (See Accessibility Considerations.) **Work Period:** 1 - 3 years.

c. Curb, concrete

- **Assessment:** Curb and gutter along the site access roadway is in good condition.
- **Recommendations:** None, except to protect the curb and gutter during construction.

d. Flagpole(s), other

- **Assessment:** Steel channels surround the stepped pipe flagpole, which is mounted atop the granite marker (Gemini refers to this item as Flagpole Base). The condition of the pipe is good; however its working mechanisms are worn and cumbersome to use. Mn/DOT has received requests from the City of Garrison asking for the removal of monument and flagpole and to replace it with three lighted flagpoles--this is noted in the "Office Memorandum" of 9/3/98 from Galen R. Bottemiller of Baxter to Carol Braun.
- **Stabilization, Preservation/Restoration:** Replace all working mechanisms, pulleys, lanyards, etc. Materials must match style and historic context. For flag to remain up at all times, install recessed up lights in Marker top.

e. Flagstone Plaza (around Marker)

- **Assessment:** The 16'-0" x 16'-0" pink and gray granite flagstone plaza around the base of the flagpole marker is the only area of flagstone on the island installed as originally designed. It has settled unevenly and requires repair. The general condition of the stones is good.
- **Recommendations:**
Stabilization/Preservation/Restoration: Remove all stone and clean thoroughly. Number each piece for reinstallation in the same location and configuration. Regrade and install new compacted sand sub-base as required. Reinstall the flagstone in the same general pattern and grout full the joints as originally specified. **Work Period:** 1 - 3 years.

f. Information Booth

- **Assessment:** This is a non-historic, wood-framed structure located between the island and T.H. 169 on the north side of the concourse entrance road. It appears that the hexagonal-shaped building is used seasonally. A concrete walk with a curb cut exists from the parking area to the booth. About 10'-0" to the south of the booth is an electrical service panel that projects out of the ground about 15 inches.
- **Recommendations:**

Stabilization: None.

Preservation: Relocate the structure north of the northwestern corner of the concourse and nestle within a tree grouping to minimize its visual impact. Remove the existing concrete walk and electrical service panel, restore the grass turf and granite curb, and install a compacted gravel walkway providing handicapped access from the concourse parking area to the booth. Attach the electrical service panel to the wall surface of the booth. **Work Period:** 1 - 3 years.

Restoration: Remove the present structure and design a new one that complements the historic context of the site. Relocate the new structure north of the northwestern corner of the concourse. Remove the existing concrete walkway and install a flagstone surface as originally shown on the 1937 drawings. Provide a handicapped curb cut to meet accessibility requirements. Remove the existing electrical service panel. Restore the grass turf. Mount the new power panel inside the new booth or on the wall. **Work Period:** 1 - 3 years.

g. Marker (Flagpole Base)

- **Assessment:** The marker is constructed of split granite that is randomly laid and rises out of the surrounding flagstone plaza. It consists of a stone base that is about 14 inches high by 9'-4" square. The remaining stone marker rises to about 9'-4" above the base. The two courses at the top are inset and stepped and form the base for the steel flagpole. (See item e. above for flagstone plaza/pad).

Mortar joints are in poor condition. Repairs have been attempted and were done carelessly staining some of the surrounding stone surfaces. A piece of granite was missing from south side of the base and was not on the site.

- **Recommendations:**

Stabilization: Attempt to find the missing granite stone and install in its original location. If this is not possible, secure a matching piece from a nearby quarry and install in the original location to prevent further degradation to the marker's structural integrity. **Work Period:** 0 - 1 year.

Preservation/Restoration: Same as stabilization and add the following: remove all existing stone joints. Repoint with matching mortar and clean entire stone surfaces. **Work Period:** 0 - 3 years.

h. Marker (Tauer Monument): This is located to the north of the concourse; this marker is in good condition and needs no work. Refer to Spatial Organization above for additional information.

i. Other feature (Bronze Plaque @ Flagpole Marker)

- **Assessment:** The plaque is in good condition; interpretive issues exist according to comments by Gemini. Further discussion regarding the contents of this plaque follows in "Other Considerations" below.

- **Recommendations:**

Stabilization/Preservation/Restoration: Protect plaque during restoration of flagpole, stone marker and flagstone walkway. **Work Period:** During all adjacent or nearby construction.

j. Other feature (Catch Basins)

- **Assessment:** Six steel grated with integral curb catch basins are located symmetrically around the concourse parking area as indicated on 12/37 drawing with annotations by MJBA, dated 12/99. The stamped manufacturer's name is James B. Clow, Chicago. They are in good condition; however, several have been partially concealed when the new concrete walk was installed.

- **Recommendations:**

Stabilization: None.

Preservation/Restoration: Protect units during the removal of the concrete curb and walk and/or regrading of drives. Grout joints adjacent to new stone curbs and

flagstone walks. Clean off all residue concrete. Work Period: 1 - 3 years.

k. Overlook wall

- **Assessment:** The large boulder and granite wall was built according the design plans and is in fair condition. The base courses have begun and are continuing to erode at the water level in several locations exposing the boulder core of the structure. Evidence of settling or wall rotation is not apparent. Several stones are missing along the wall's top. The mortar joints are in poor to fair condition. Concrete mortar topping has been added to the top of the wall. When the concrete curbing/walk that was recently added is removed, the granite surfaces must be carefully cleaned.
- **Recommendations:**
Stabilization: None is recommended as this feature requires significant repairs and the work recommended should be completed at one time and as completely as possible to restore its historic prominence and importance. However, if funding is not obtained within 1 to 3 years, areas of decay on the wall must be addressed and corrected to eliminate further degradation. Work Period: 1 - 2 years.
Preservation/Restoration: Dam the lake around the base and let the stone coursing and construction thoroughly dry out. Remove all deteriorated portions of the structural substrate and stone, including damaged or plugged vitreous piping. Carefully clean the surfaces and install new grout and replace missing stones. All stones that are not extant shall match the original design and shape of the units being replaced. Following the curing period, remove the dam allowing the water to again meet the wall. Replace the stone units at the missing locations along the wall's upper and top portions. Remove the concrete topping. Repoint all exposed joints to match. Clean all masonry surfaces of all residue mortar, bird droppings, and vegetation as required. All coursing along the walk must be cleaned of any concrete residue. See Sidewalk below. Work Period: 1 - 3 years.

l. Sidewalk: Refer to Pedestrian Walkways above.

m. Statue (Fish)

- **Assessment:** The mounted fiberglass fish is a non-historic tourism feature that is invasive to the historic integrity of the site. It is in good condition and sits on a wooden pedestal that has a sign mounted to it. The concrete slab below the fish is 17'-6" by 28'-0" and is not original to the site. A USGS marker is embedded in the concrete slab and is located at the northwestern corner of the slab.
- **Recommendations:**
Stabilization: None.
Preservation/Restoration: Remove the fish, sign and concrete slab from this location and site. Its size and prominent location seriously compromises the historic integrity of this large National Register eligible site. Several optional locations include on the western side of T.H. 169/18 north of the site; or out into the lake about 100 yards from the concourse wall. Work Period: ASAP.

7. Accessibility Considerations: In lieu of installing curb cuts raise the roadway/parking surface to match the curb height at the following locations: east side of the island with access to both the island and the overlook wall; northwestern and southwestern corners of the overlook walls. The grading of the new asphalt surface must be done with care and sensitivity to preserve the granite curbing on the concourse site. The northwestern access should serve the new walkway to the relocated Information Booth. Refer to that item above. Provide one handicapped parking space with signing on the asphalt surface.

8. Health and Safety Considerations: Access and use of the concourse should be prohibited during the construction. Other parts of the site can remain public service during construction.

- 9. Environmental Considerations:** Protect the lake and the landscape from construction material run-off and remove all debris daily.
- 10. Other Considerations/Recommendations:** Modern highway lamp poles have been added to the site along T.H. 169 and T.H. 18. Replacing these with historically appropriate poles and fixtures ought to be considered along the boundaries of the site. These will enhance and reinforce the significance of this historic roadside stop as well as providing a visual reminder to motorists of its presence.

Signage is recommended to be done as soon as possible to raise the public's awareness of this site's historic importance and educational value. Place an interpretive marker or panel at the site that describes its history, designers, and builders. Place the marker either north or south of the concourse entrance drive (not on the concourse island or too close to the concourse structure). The marker design should be simple and unobtrusive. If necessary, create a sensitively designed, hard-surfaced access to the panel such as "grasscrete."

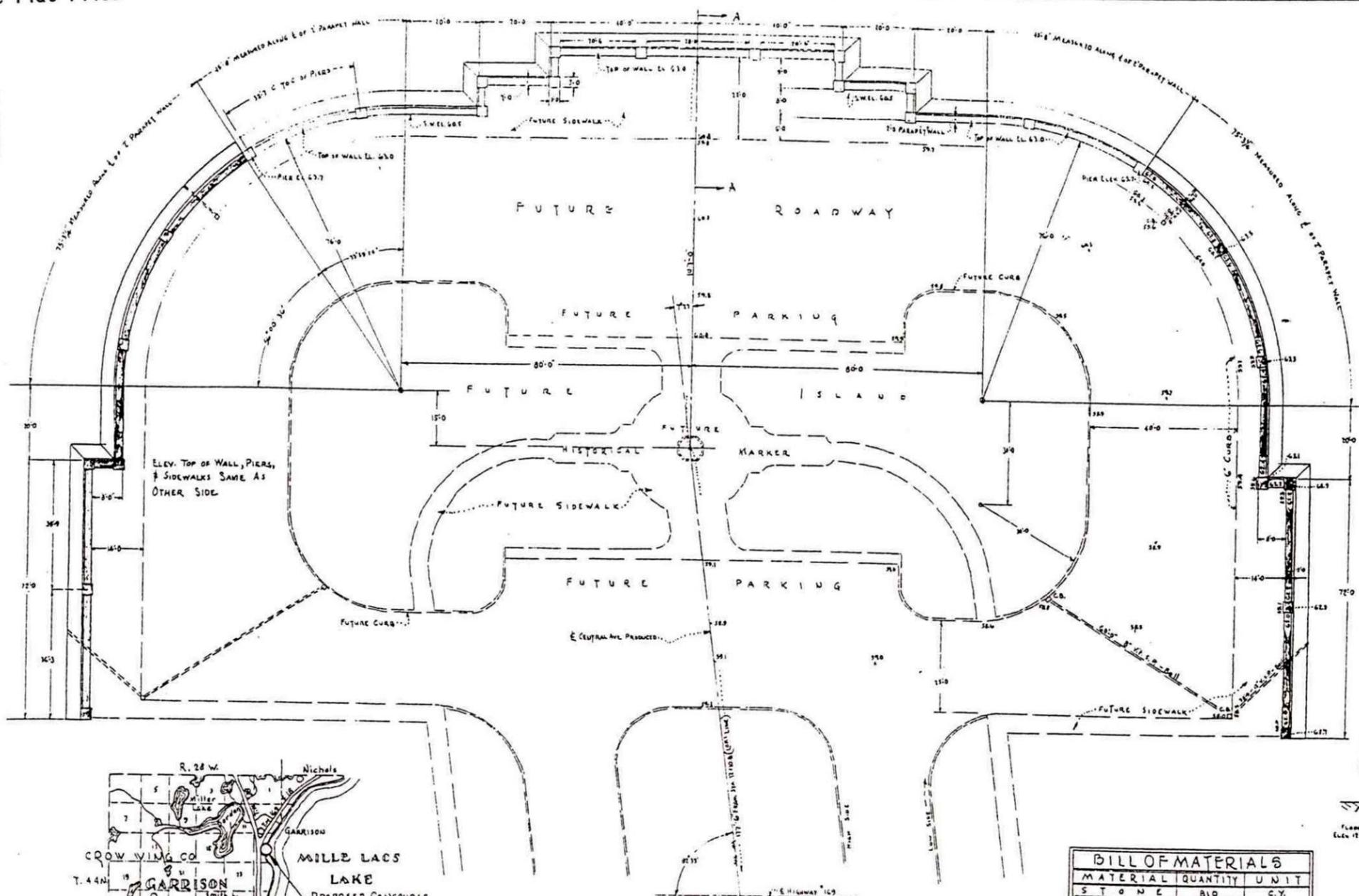
To update the cultural accuracy of the flagpole marker text, the SHPO recommends leaving the existing plaque on the flagpole base, but adding new site interpretation whose text should be developed in consultation with the SHPO. Either combine this text with the new interpretive marker just described, or place it near it.

The site was built as part of a joint CCC, National Park Service, and MHD project to improve and landscape many miles T.H. 169 and T.H. 18 near Mille Lacs Lake for recreational and commercial purposes. The project included highway realignment, roadside landscaping, and the construction of several stone bridges, scenic overlooks, stone curbing, the Garrison Concourse, and the Garrison Rest Area. It was the most extensive roadside development project undertaken by the CCC in the state. It is recommended that the following sites, all part of the project, be linked together with site interpretation: Garrison Concourse, Kenney Lake Overlook, Garrison Pedestrian Underpass, T.H. 169 Culvert at St. Alban's Bay, and Whitefish Creek Bridge.

- 11. Conclusion:** The concourse is an excellent example of the work of the MHD Roadside Development Division during its formative years. It illustrates many of the Division's stated goals such as designing aesthetics into highways rather than "beautifying" them later, promoting the state's tourism industry, and creating waysides and scenic overlooks that allow travelers to both rest from driving and enjoy exceptional scenic surroundings. The site illustrates the interaction between National Park Service designers and those at the state level and embodies the strong partnership between the MHD and federal Depression relief agencies.

The Garrison Concourse has strong statewide significance because of its aesthetic qualities, the scale of its architecture and associated landscape design, the quality of its masonry, its historical associations and its highly visible location. Providing appropriate interpretive and highway signage is a high priority and should be done as soon as possible. Its preservation and/or restoration are of highest priority.

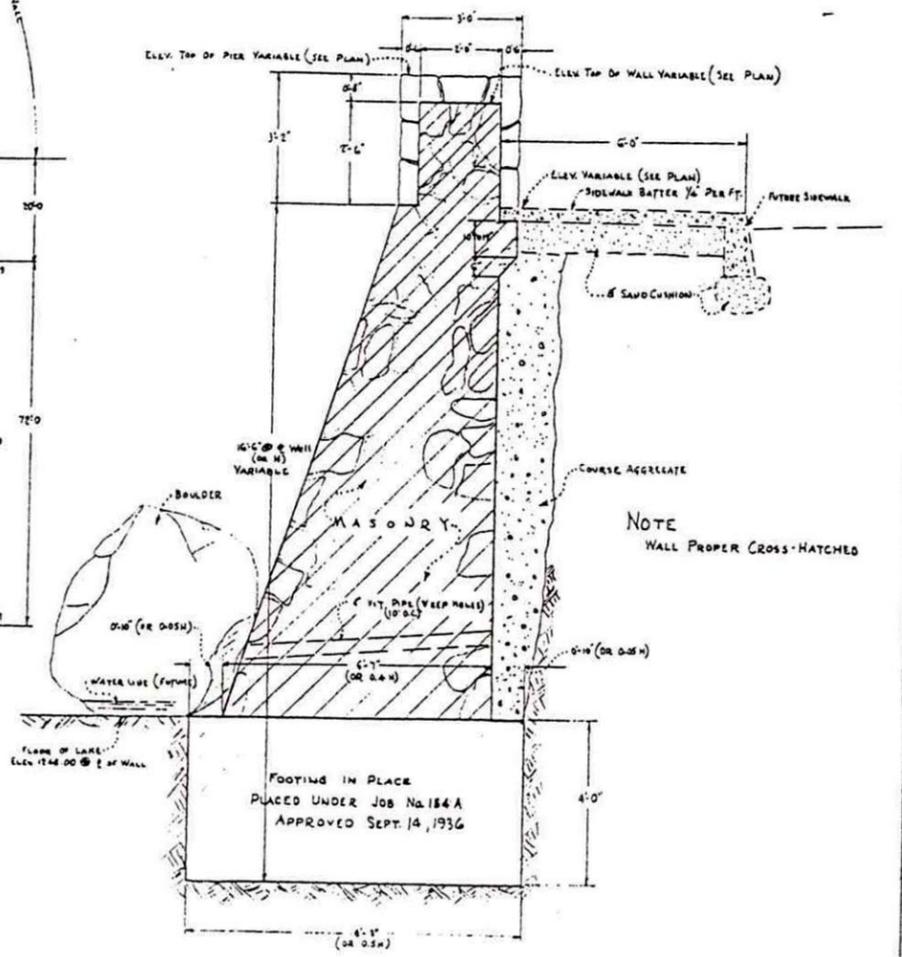
	Stabilization	Preservation	Restoration
Spatial Organization and Land Patterns	<i>Includes 10% cont.</i>	<i>Includes 5% cont.</i>	<i>Includes 5% cont.</i>
Off-site impacts			
Functional relationships			
Visual relationships			
Cultural landscape limits (land acquisition)			
Topography			
Character-defining feature			
Non-contributing corrective work			
Vegetation	4,400	11,760	86,369
Circulation			
Access road and internal roadways			
Parking areas	20,029	191,676	191,676
Pedestrian walks		243,298	243,298
Paths and trails (signage/picnic table path)			8,820
Water Features			
Structures, Furnishings and Objects			
Bath house			
Bench(es), other			
Bench(es), stone	3,960	3,780	40,740
Bridge/culvert			
Cave			
Council ring			
Curb, stone		33,482	33,482
Curb, concrete		3,259	10,147
Dam			
Dock			
Drinking fountain(s)			
Entrance Wall			
Fireplace(s), other			
Fireplace(s), stone			
Flagpole(s), other	2,640	2,520	2,520
Flagpole(s), stone			
Flagstone pad	4,048	3,864	3,864
Footbridge			
Foundation of building			
Gravestone			
Guardrail, stone--Other			
Info board			
Info booth		12,600	58,800
Marker			
Other feature--Plaque	792	2,520	2,520
Overlook wall	145,455	700,182	700,182
Picnic shelter(s)			
Picnic table(s), other			
Picnic table(s), stone			
Privies			
Refuse container(s), stone			
Restroom building			
Retaining wall			
Rock garden			
Sea wall			
Sidewalk		4,284	4,284
Signpost, other			
Signpost, stone			
Spring water outlet			
Statue (fish)		5,880	5,880
Storage building			
Trail steps			
Wall			
Well/pump			
Accessibility Considerations		4,032	4,032
Health and Safety Considerations			
Environmental Considerations			
Other Considerations (signage)	12,672	12,096	12,096
ESTIMATED COSTS	\$193,996	\$1,235,233	\$1,408,710



GENERAL PLAN A
SHADED AREA INDICATES PORTION OF WALL TO BE BUILT FROM THESE PLANS
SCALE 1/4" = 1'-0"

UNSHADED AREA INDICATES PORTION OF WALL BUILT UNDER JOB 154
APPROVED JUNE 18, 1936

BILL OF MATERIALS		
MATERIAL	QUANTITY	UNIT
STONE	810	C.Y.
CEMENT	2400	SAK
PLASTER SAND	250	C.Y.
VITRIFIED TILE PIPE (4")	212	LIN. FT.
LIME	175	SAK

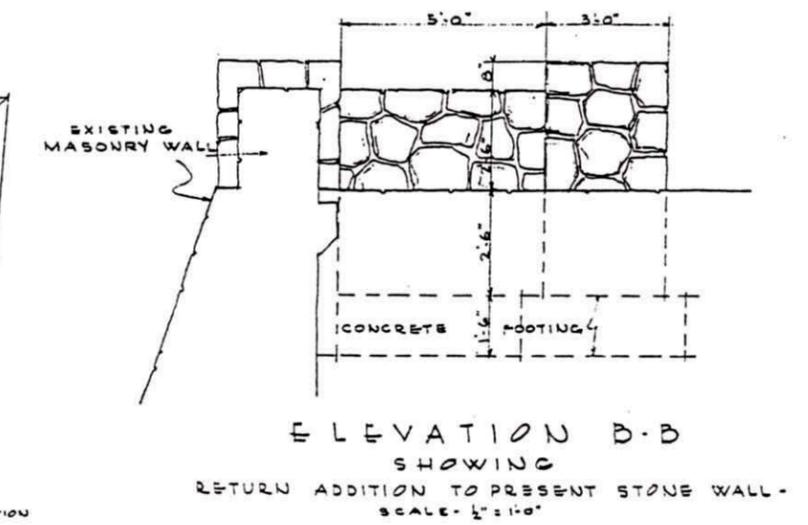
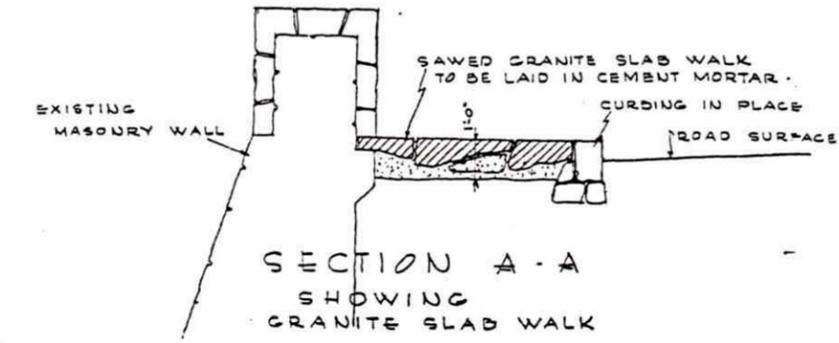
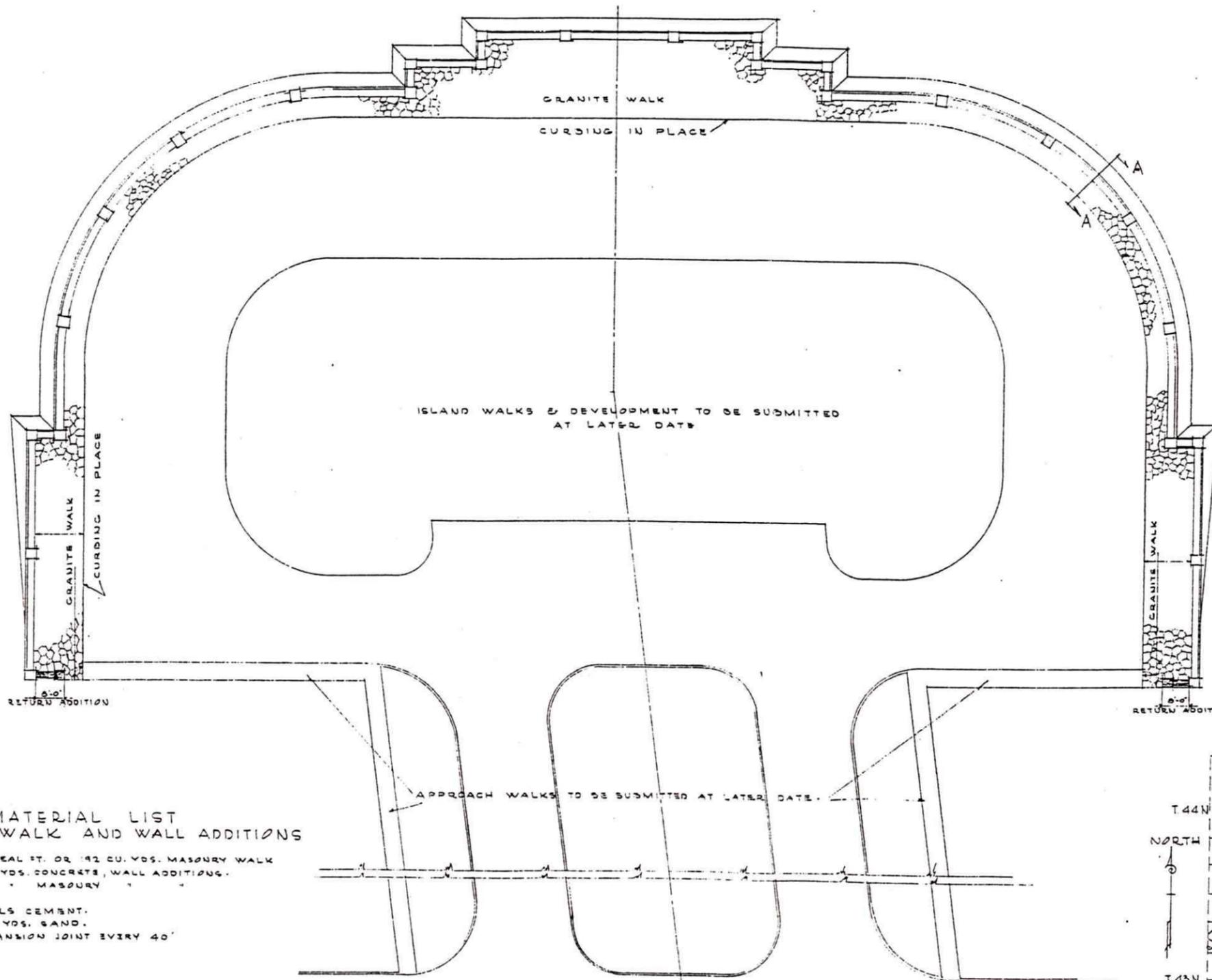


SECTION A-A
SCALE 1/2" = 1'-0"

SD-MINN
MIL-9017
CONCOURSE 12/36

STATE OF MINNESOTA
DEPARTMENT OF HIGHWAYS
RECOMMENDED FOR APPROVAL
APPROVED

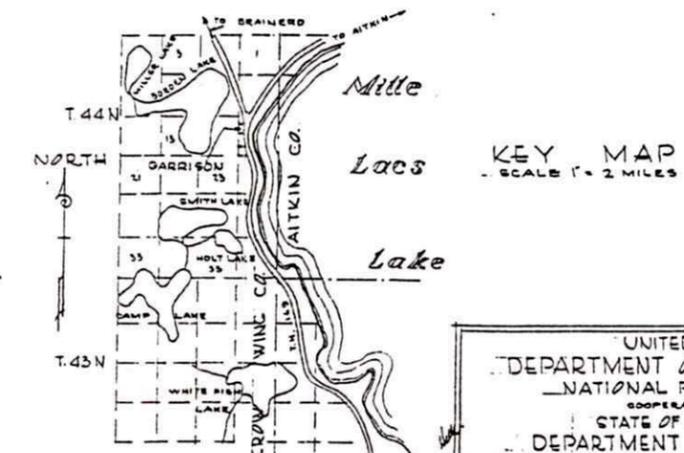
DEPARTMENT OF THE INTERIOR
NATIONAL PARK SERVICE
AND
STATE OF MINNESOTA
DEPARTMENT OF HIGHWAYS
BRANCH OF RECREATIONAL PLANNING & STATE COOPERATION
REGION II
MINNESOTA S.P. 15 JOB 154 B
MILLE LACS LAKE STATE PARK
SCALE AS NOTED DATE DECEMBER 17, 1936
DESIGNED BY
DRAWN BY
CHECKED BY
SHEET NO. 1 OF 1



MATERIAL LIST FOR WALK AND WALL ADDITIONS

- 454 LINEAL FT. OR 192 CU. YDS. MASONRY WALK
- 3.55 CU. YDS. CONCRETE, WALL ADDITIONG.
- 7.47 " " MASONRY " "
- 186 BBLs CEMENT.
- 78 CU. YDS. SAND.
- 1/2" EXPANSION JOINT EVERY 40'

PLAN CONCOURSE
SCALE - 1/2" = 1'-0"

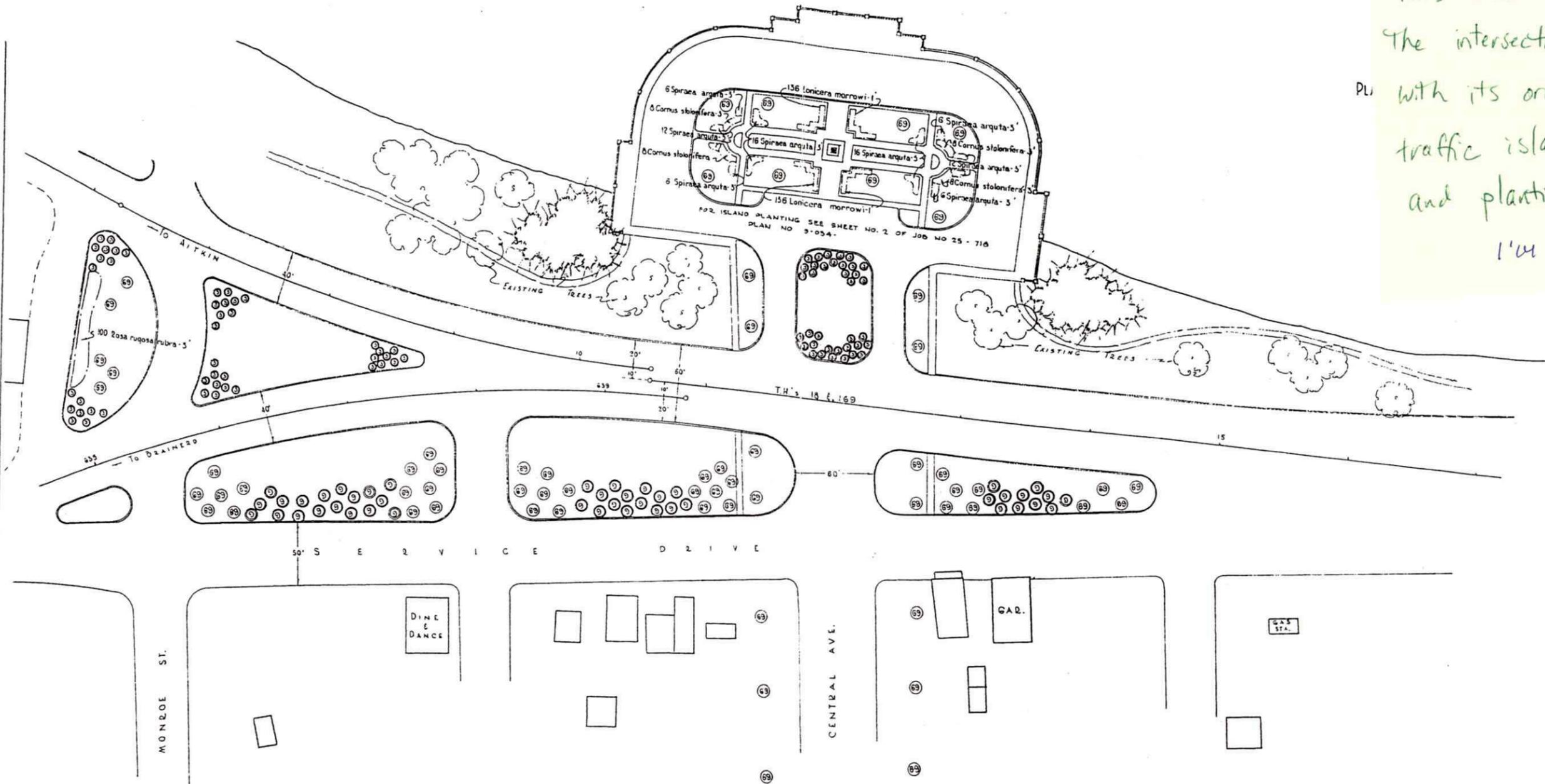


UNITED STATES DEPARTMENT OF THE INTERIOR NATIONAL PARK SERVICE COOPERATING WITH STATE OF MINNESOTA DEPARTMENT OF HIGHWAYS		
DRAWN BY N.S. BRIDGEMAN DESIGNED BY S.M.A.	MINNESOTA SP15 MILLES LACS LAKE CONCOURSE WALK SCALE AS SHOWN - DATED 6-6-1937	JOB NO. P. WA 25 710 PLAN NO. 9-034 SHEET NO. 1-33
CHECKED BY J. H. H.	PREPARED BY J. H. H.	DATE 10-8-37
RECOMMENDED FOR APPROVAL ENGINEER J. H. H.	RECOMMENDED J. H. H.	DATE 12-6-37
APPROVED CONSTRUCTION ENGINEER J. H. H.	DATE 12-6-37	APPROVED REGIONAL OFFICER J. H. H.

12/37
"9-034"

Copied from MnDOT Site
Develop Unit flat files

CONCOURSE.
This sheet shows
the intersection best
with its original
traffic islands
and plantings
I've got this on
1-8-01



PLANT MATERIAL LIST

INDEX	SCIENTIFIC NAME	TREE	RANGE	UNIT	QUANTITY	SIZE	REMARKS
3	<i>Juniperus communis depressa</i>	Evergreen Shrub	Average	Shrub	80	24" x 30" x 6" B	
9	<i>Picea canadensis albertiana</i>	Evergreen Tree		Tree	43	5'-9" Hi. D 6"	
69	<i>Ulmus americana</i>	Shade Tree			63	1 1/2" - 2" cal.	
	<i>Cornus stolonifera</i>	Deciduous Shrub		Shrub	32	2'-3" Hi.	
	<i>Lonicera morrowi</i>				272	2'-3" Hi.	
	<i>Rosa rugosa rubra</i>				100	2'-3" Hi.	
	<i>Spiraea arguta</i>				80	2'-3" Hi.	
	<i>Poa pratensis</i>	Grass Seed		Pound	132		
	<i>Trifolium repens</i>				60		
	<i>Lolium perenne</i>				36		

ESTIMATED QUANTITIES

ITEM	QUANTITY	UNIT	REMARKS
Installing Evergreen Shrubs	80	Shrub	
Installing Evergreen Trees	43	Tree	
Installing Shade Trees	63	Tree	
Installing Deciduous Shrubs	484	Shrub	
Seeding	2.0	Acre	

ROADSIDE DEVELOPMENT

RECOMMENDED FOR APPROVAL

Handwritten signatures and dates:
 Approved: *[Signature]* 3-24-58
 Approved: *[Signature]* 3-27-58

UNITED STATES
DEPARTMENT OF THE INTERIOR
NATIONAL PARK SERVICE
COOPERATING WITH
STATE OF MINNESOTA
DEPARTMENT OF HIGHWAYS

MINNESOTA S.P.15
MILLE LACS LAKE
PLANTING TREES & SHRUBS

SCALE 1" = 40' DATE March 31, 1958 SHEET NO. 4 OF 4

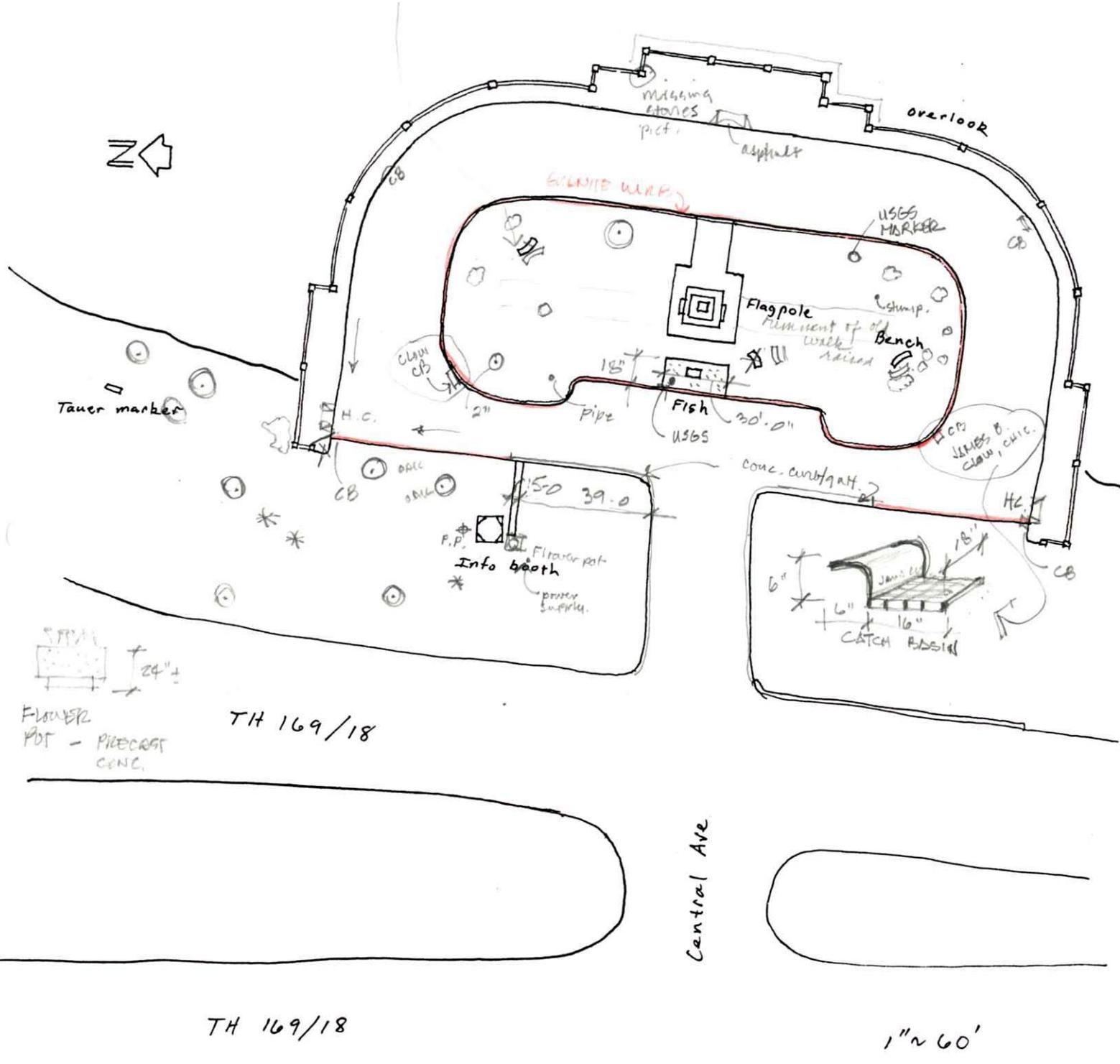
DRAWN BY: *[Signature]* JOB NO. 25-718
 DESIGNED BY: *[Signature]* PLAN NO. 3-054
 CHECKED BY: *[Signature]*
 PREPARED BY: *[Signature]* DATE 4-1-58
 RECOMMENDED BY: *[Signature]* DATE 4-7-58
 APPROVED BY: *[Signature]* DATE 4-4-58

GARRISON

USE

MBA Field Notes
10/99

BENCH SEAT IS ABOUT 12" ABOVE GRASS Mille Lacs Lake



FLOWER POT - PRECAST CONC.

TH 169/18

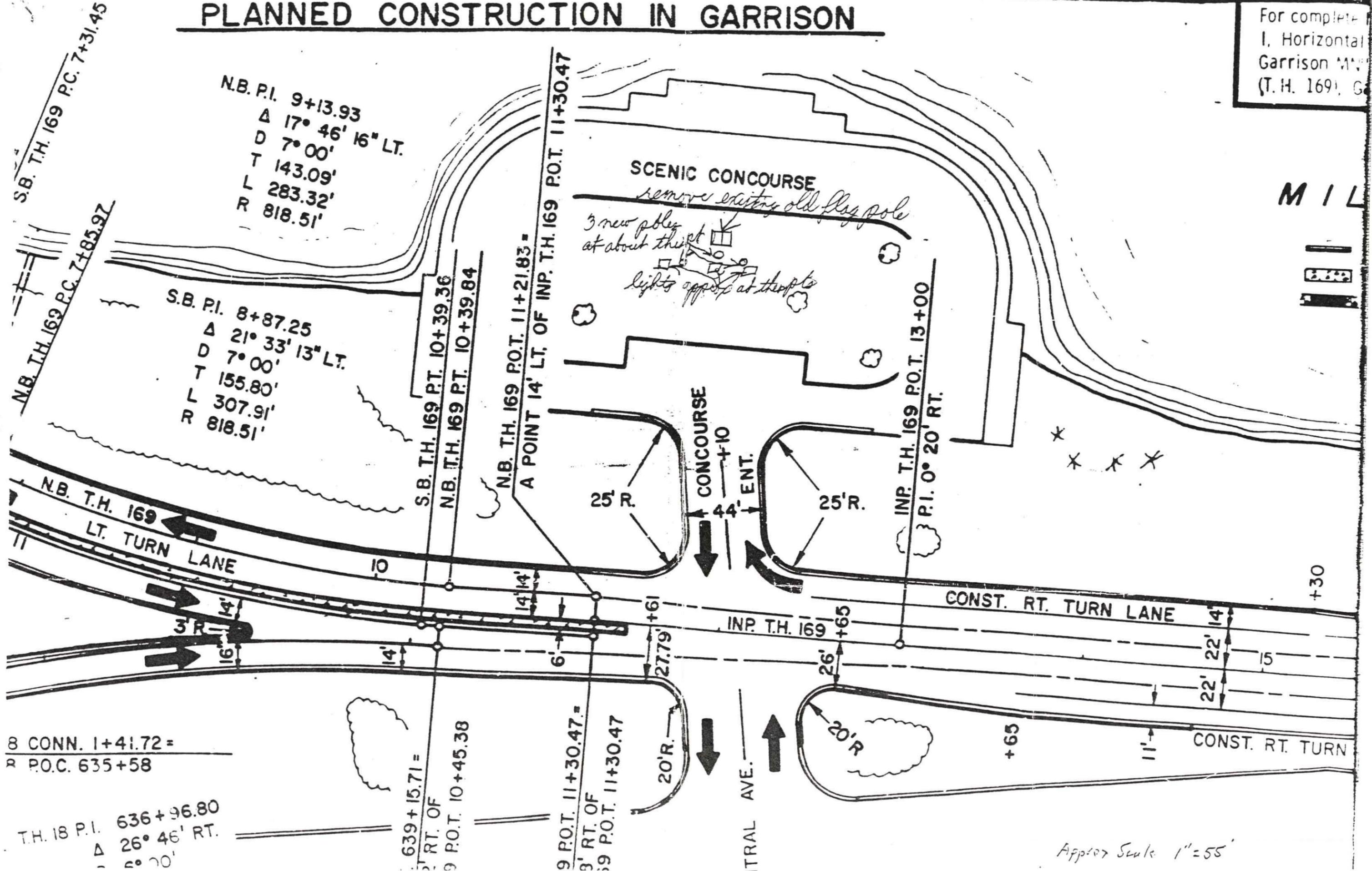
TH 169/18

Central Ave

1" ~ 60'

PLANNED CONSTRUCTION IN GARRISON

For complete
I, Horizontal
Garrison M...
(T.H. 169), G...



DEPARTMENT: Field Operations Division

STATE OF MINNESOTA
OFFICE MEMORANDUM

DATE : September 3, 1998

TO : Carol Braun - M.S. 686
Engineering Service

FROM : Galen R. Bottemiller - Baxter 
Maintenance Technician

PHONE : 218-828-2469

SUBJECT : C.S. 1804 (T.H. 169)
Garrison Concourse

The District has received inquiries from the City of Garrison concerning the possibility of installing three lighted flag poles within the Garrison Concourse. They also mentioned the removal of an existing flag pole which was constructed as part of a granite stone monument.

They had previously requested and received permission to make use of the existing flag pole. However, I believe difficulties have occurred due to the tangling of the rope in the top pulley assembly.

Attached is a rough sketch showing the proposed flag pole and light locations for review to determine if the proposal is acceptable in light of the historical significance of the location. If found to be unacceptable, it would be appreciated if acceptable alternatives would be recommended.

If you have any questions, please contact me at your convenience.

Attachment:

cc:

D.L.Raisanen/G.Niemi

Joe Hudak - M.S. 676

City of Garrison - P.O. Box 439 - Garrison, MN 56450

Richard Bouley - P.O. Box 154 - Garrison, MN 56450



M J B A

- ARCHITECTURE
- INTERIOR DESIGN
- HISTORIC PRESERVATION

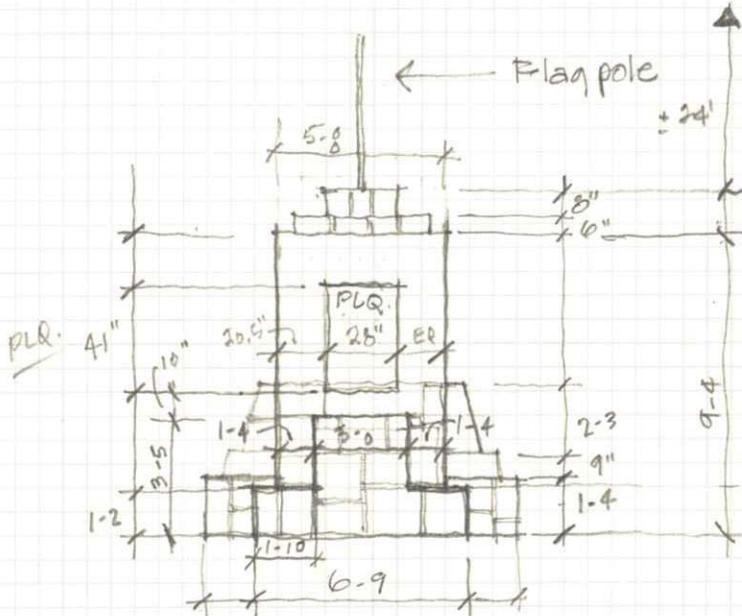
MICHAEL J. BURNS ARCHITECTS, LTD.

824 CENTER AVENUE, MOORHEAD, MN 56560 ■ 2878 LILAC LANE NE, FARGO, ND 58102
■ (218) 233-6620 ■ (701) 298-0140 ■ FAX: (218) 233-6621

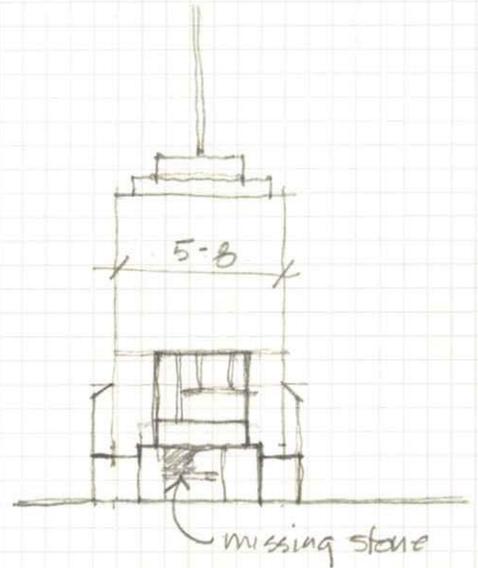
Project UNDOT R.P.A. Project No. 9919

Subject GARRISON CONCOURSE Date OCT. 7, 1999

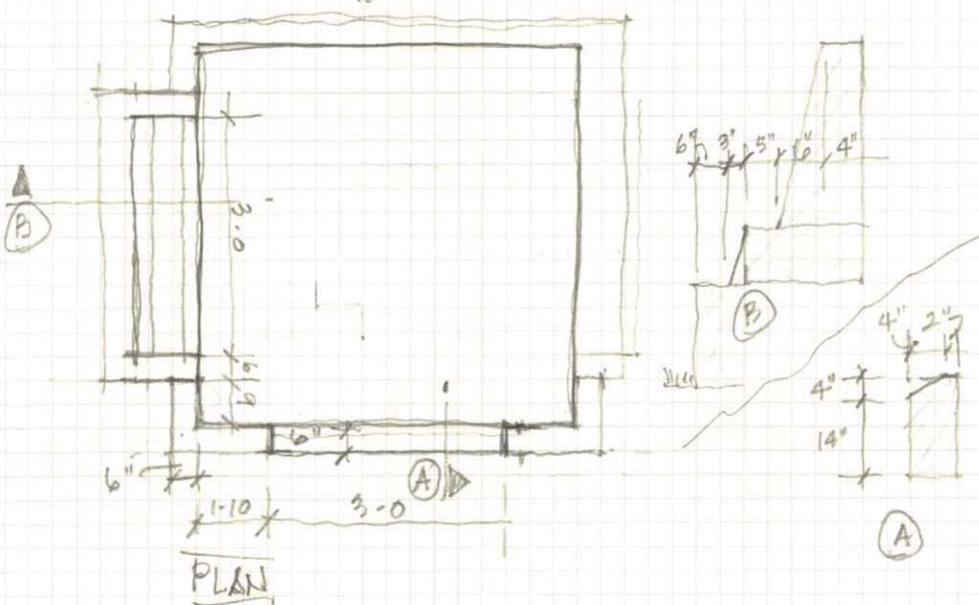
HISTORIC MARKER/MONUMENT



East ELEVATION
WEST SIM (NO PLAQUE)



SOUTH ELEVATION
(NORTH SIM)



PLAN





M J B A

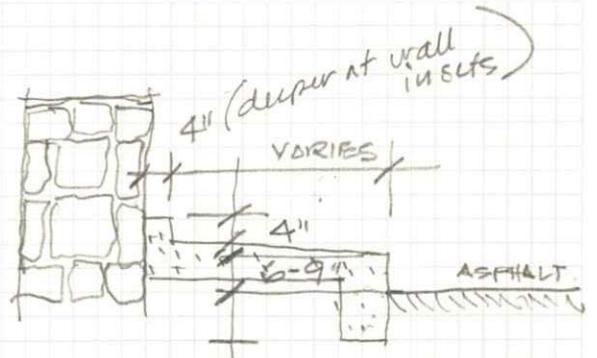
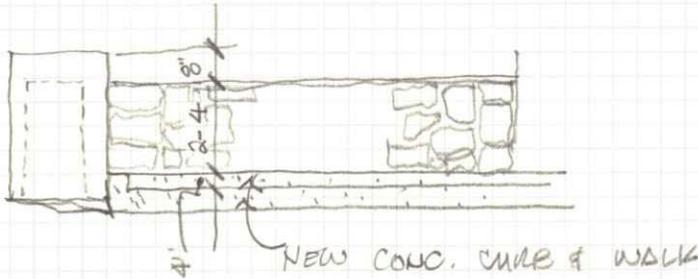
- ARCHITECTURE
- INTERIOR DESIGN
- HISTORIC PRESERVATION

MICHAEL J. BURNS ARCHITECTS, LTD.

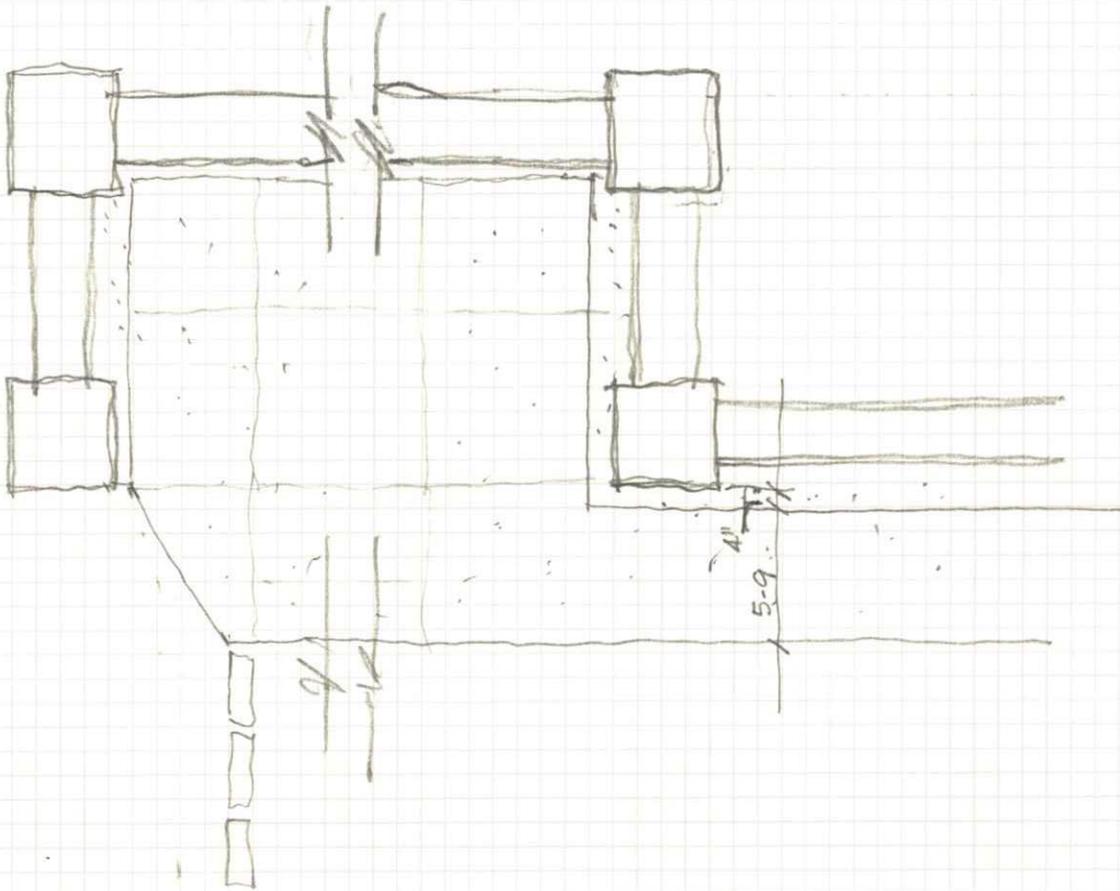
824 CENTER AVENUE, MOORHEAD, MN 56560 ■ 2878 LILAC LANE NE, FARGO, ND 58102
■ (218) 233-6620 ■ (701) 298-0140 ■ FAX: (218) 233-6621

Project _____ Project No. 1919

Subject GARRISON CONCOURSE Date 10-7-19



SECTION (ASSUMED)



PLAN @ NW CORNER.



1. View of Concourse looking NE from Ped. Underpass (SHPO #CW-GRC-005)



2. Looking SE from north edge of site at North side of wall



3. Looking South at parking area and partial North wall



4. View of North wall at water's edge



5. Looking SE @ northeast face of Concourse wall



6. Looking NW at Site Entrance and Visitor's Info. Booth from south wall



7. View of the SW edge of the concourse wall, walk and asphalt paving



8. Looking SW at Site Entrance from island



9. Looking NW @ island and NW corner of concourse wall



10. Looking West @ Island, Granite Curb, Monument, Fish and Site Entrance



11. Looking South from Island showing Elements' relationships



12. Looking North from Island showing Elements' relationships



13. East view of monument, granite curb and concrete walk



14. SE view of monument, flagstone walk and repairs?



15. Close-up of flagstone walk at monument



16. Close-up of flagstone walk at monument



17. View of SW corner of Wall, concrete walk/handicapped curb cut, tree and vegetation



18. View of SW Concourse Wall looking North from beach



19. View of South Wall @ water's edge looking north



20. View of South Wall from water's edge—looking NE



21. View of inside NW corner of wall, concrete walk, curb cut and asphalt—looking NW from Island



22. Close-up of NW corner of Concourse Wall



23. View of concrete walk and interior wall showing missing stones at pier—looking NW



24. Close-up of pier missing stones



25. Close-up of existing wall and concrete conditions



26. View of Interior of wall and curb cut looking South



28. Original Catch Basin/granite curb—note excessive turf and asphalt



29. Original catch basin at new concrete walk's edge—original steel head is covered with concrete



30. Original Catch Basin @ SW Walk and Wall
(Granite Curb is only about 1" above asphalt surface)



31. North Outer Wall Condition
(Far pier is missing stones)



32. South Outer Wall Condition
(Missing stones visible)



33. Base of Wall at Beach on North Side



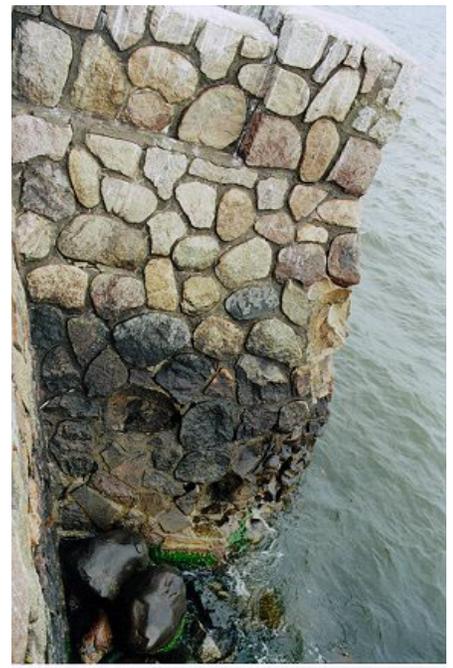
34. North Wall looking East



35. Base of South Wall (Note--vegetation)



36. Outer Wall Condition (Note—ledge & joints)



37. South Wall Pier (Note—missing stones)



38. Tauer Monument located on Northern part of Site

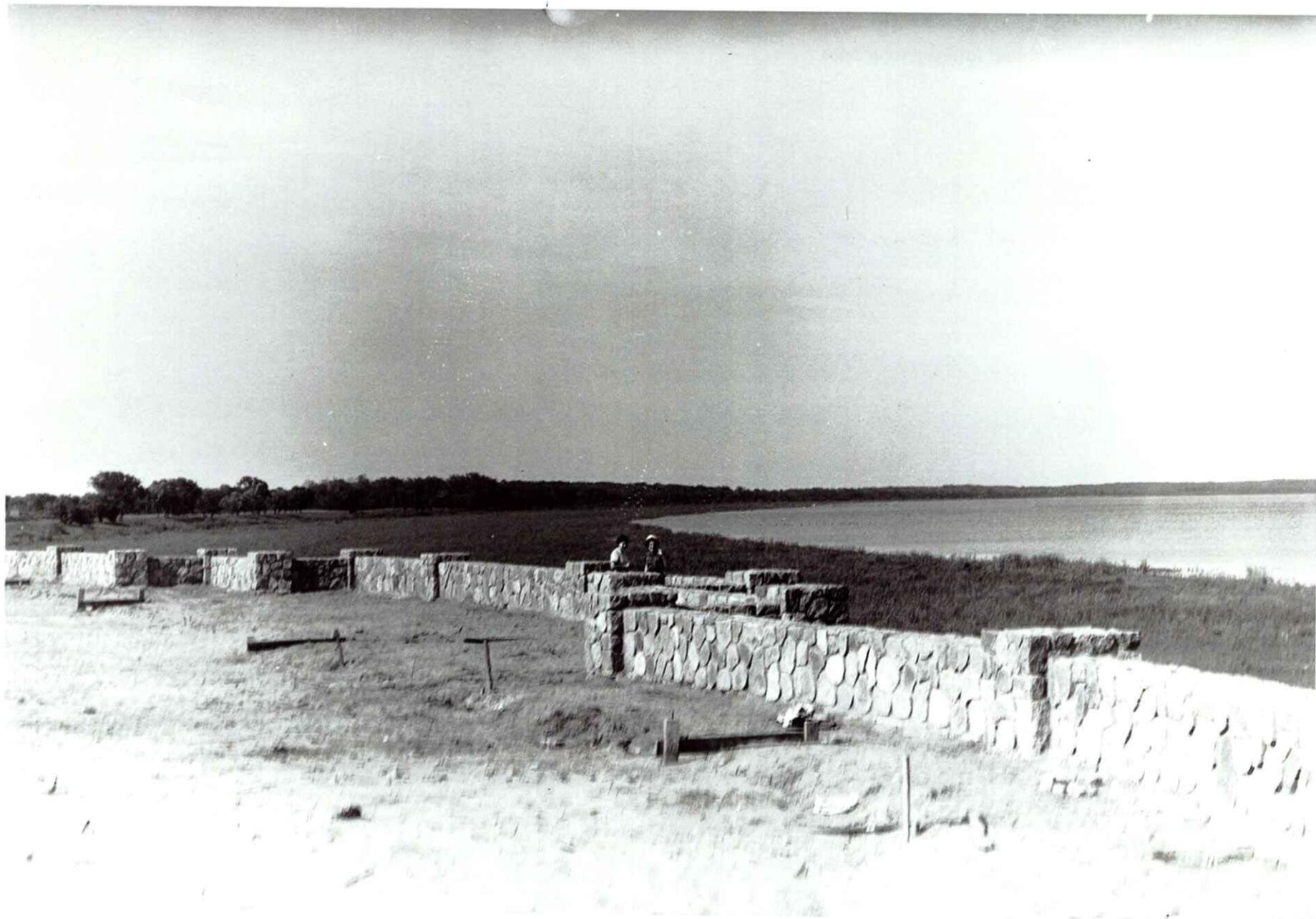


39. Visitor's Information Booth & Walk (Note—granite curb; handicap access; tilted electrical service box)

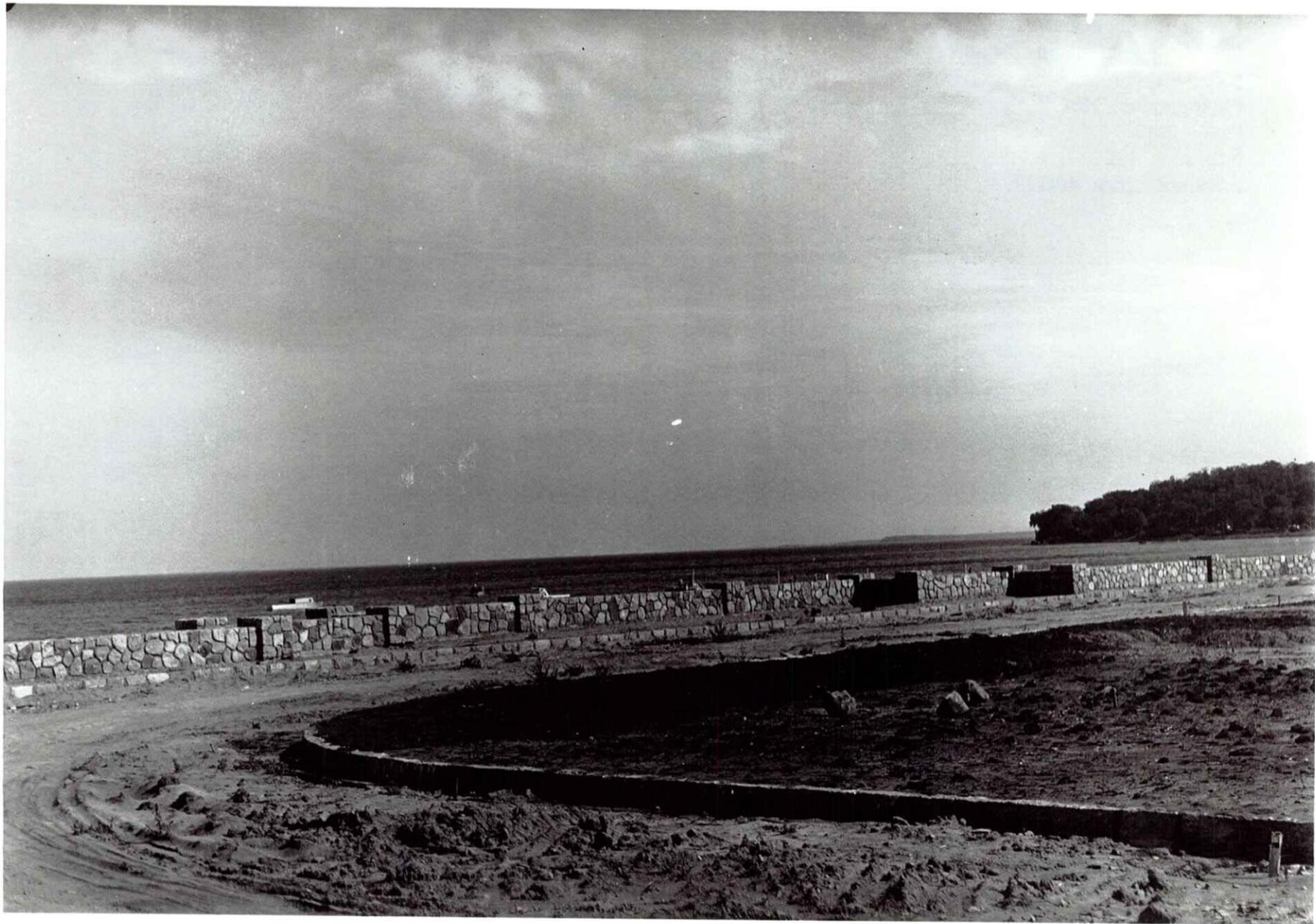


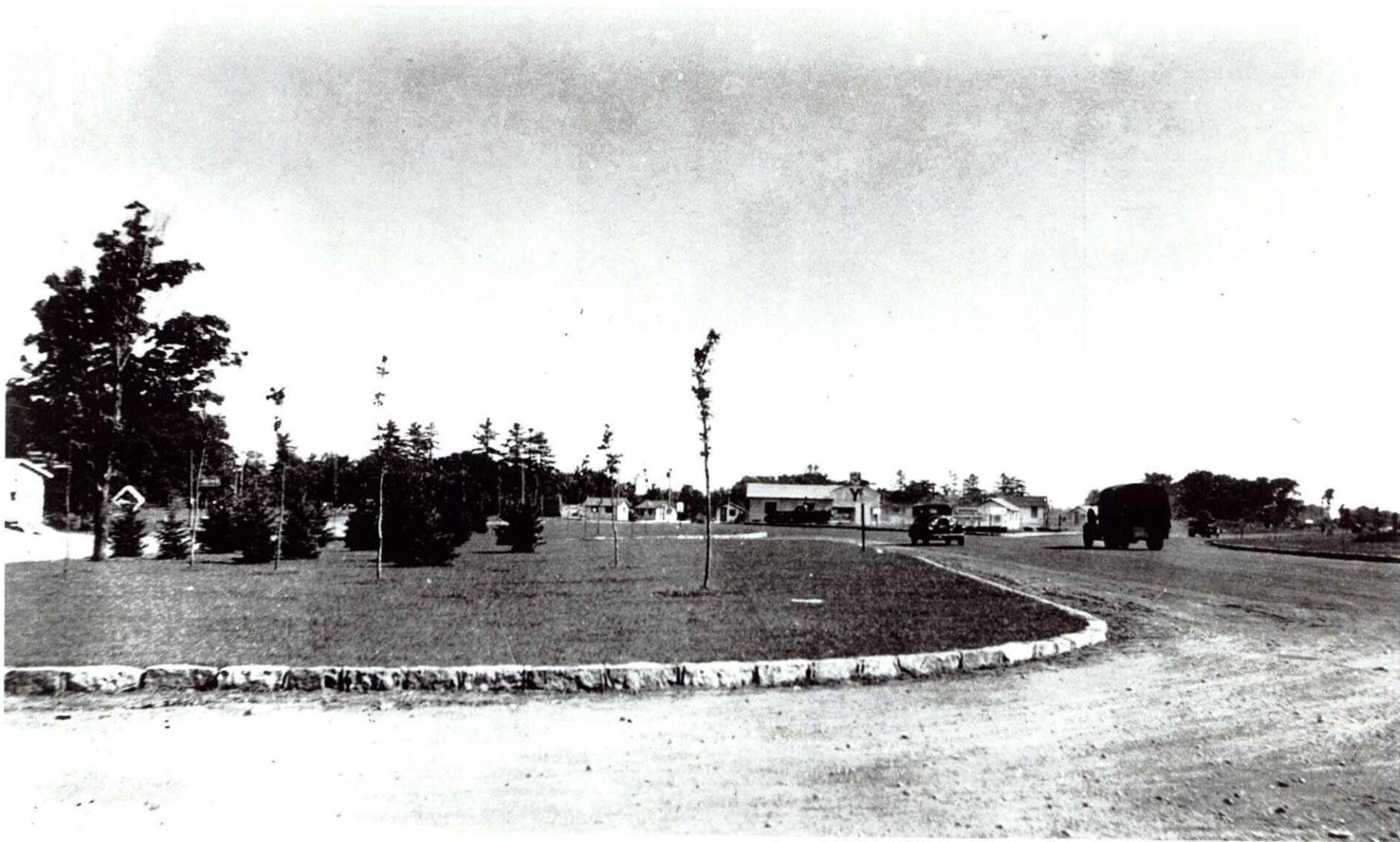
40. Geophysical Elevation Marker @ Fish Plaza
(Note—granite curb elevation is nearly flush with the newer asphalt drive and parking area)

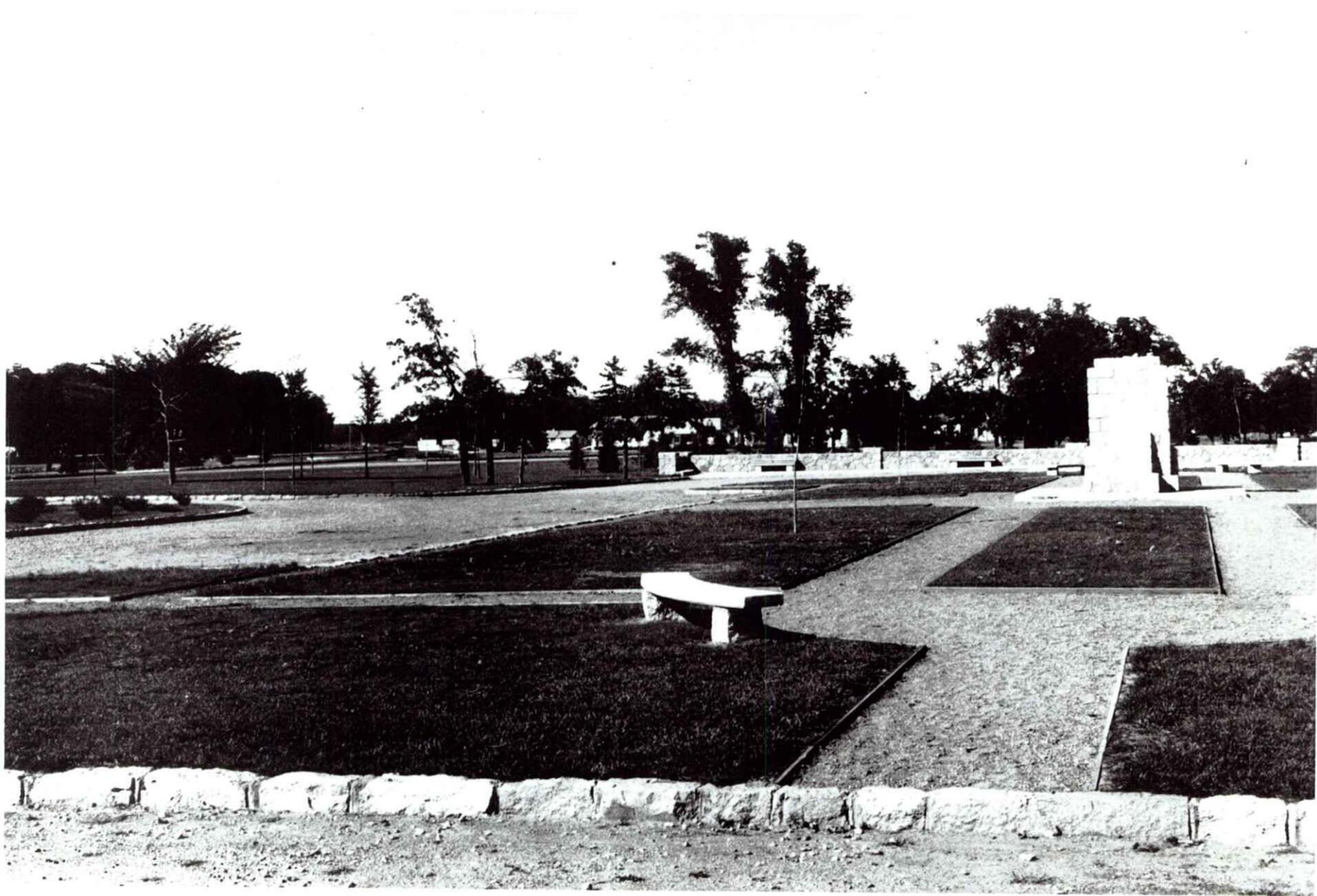














SITE BOUNDARIES

■ RECOMMENDED BOUNDARY OF NATIONAL REGISTER-ELIGIBLE PROPERTY

The recommended boundary of the National Register-eligible property is shown by the dashed line on the sheets entitled "Garrison Concourse Site Boundaries" (two pages) and accompanying sheets entitled "Mille Lacs Lake CCC Roadside Development, Garrison Section" (four pages).

The base maps for the "Site Boundaries" sheets are a Minnesota Department of Transportation (Mn/DOT) right-of-way map and an aerial photo. The base maps for the "Mille Lacs Lake CCC Roadside Development" sheets are a Mn/DOT right-of-way map and an aerial photo.

The eastern boundary of the National Register-eligible property follows the shoreline of Mille Lacs Lake, which is also the Mn/DOT right-of-way line. The western boundary follows the Mn/DOT right-of-way line. The southern boundary is drawn at the southern end of the original granite curbing that remains on the eastern side of T.H. 169. The northwestern boundary (creating the northwestern arm of a "Y") is also drawn to encompass original granite curbing. The northeastern arm of the "Y"-shaped property is 250' long.

Boundary Justification

The recommended boundary of the National Register-eligible property generally conforms to the historical boundary of the roadside park and associated intersection as they were envisioned by the Minnesota Department of Highways and the National Park Service in early construction plans.

■ RECOMMENDED BOUNDARY OF MN/DOT HISTORIC SITE CONSERVATION ZONE

The recommended boundary of the Mn/DOT Historic Site Conservation Zone is also shown on the accompanying sheets. The Conservation Zone encompasses both the National Register-eligible property, marked by the dashed line, and adjacent areas marked by the solid line.

Boundary Justification

The Mn/DOT Historic Site Conservation Zone is recommended to provide a special management zone that includes both the National Register-eligible site and a larger area that encompasses part of the historic property's early physical and visual "context" or setting.

Preserving the property's physical and visual setting will help protect its historic integrity and enhance the public's understanding of, and appreciation for, the historic site design. The Conservation Zone will help buffer the site from elements that may detract from its historic character.

It is recommended that the Conservation Zone boundaries include the National Register-eligible property and additional land described as follows:

The Conservation Zone for the Garrison Concourse is combined with the Conservation Zone for two nearby related properties, the Garrison Pedestrian Underpass (Bridge 5265) (CW-GRC-005) and the T.H. 169 Culvert at St. Alban's Bay (CW-GRT-002). All three properties were built and landscaped as part

of the same CCC roadside development project. Jointly sponsored by the CCC, the National Park Service, and the Minnesota Department of Highways, this project was the most extensive roadside development project undertaken by the CCC in the state. The project included highway realignment, roadside landscaping, and the construction of several stone bridges and scenic overlooks including the Garrison Rest Area, the Garrison Concourse, Whitefish Creek Bridge, the Garrison Pedestrian Underpass, the T.H. 169 Culvert at St. Alban's Bay, the Garrison Creek Culvert, and the Kenney Lake Overlook (on T.H. 18). The sheets entitled "Mille Lacs Lake CCC Roadside Development, Garrison Section" show a subsection of this designed historic landscape.

The Conservation Zone boundaries in the Garrison area generally follow current Mn/DOT right-of-way lines (which tend to be the same as 1930s highway right-of-way lines in this area). Most of the Conservation Zone is currently owned by Mn/DOT. Near the southern edge of Garrison, the Conservation Zone includes the former site of the CCC camp, now an undeveloped wooded parcel.

It is recommended that Mn/DOT retain all current right-of-way within the Conservation Zone. It is further recommended that Mn/DOT preserve the Conservation Zone by taking such actions as special right-of-way planting and maintenance, acquiring additional property or scenic easements, and/or creating partnership agreements with individuals or groups interested in preserving the historic property and its setting. The Mn/DOT Cultural Resources Unit should be consulted regarding these activities.

In particular, it is recommended that all portions of the Conservation Zone be rehabilitated and maintained in a manner consistent with the original design intent. The original roadside landscaping included contouring the highway slopes, planting thousands of native trees and shrubs, installing hundreds of feet of granite curbing, and creating well-landscaped traffic islands, among other work. Mn/DOT should work closely with the City of Garrison and the Mn/DNR toward this goal, and historic plans and photos should be used to guide treatment activities.

It is also recommended that the roadside development sites within the Conservation Zone be linked by bicycle and pedestrian paths and jointly interpreted with uniform signs or markers that discuss the designers and builders of the larger roadside development project.

■ MORE INFORMATION

For detailed information on the Garrison Concourse's structures, landscape, and significance, refer to:

"Accomplishment Map" of CCC roadside development work along Mille Lacs, Minnesota Department of Highways and National Park Service, signed March 1939.

Mn/DOT Historic Roadside Development Structures Inventory form for Garrison Concourse (Gemini Research, Dec. 1998).

Mn/DOT Historic Roadside Development Structures Preservation and Restoration Report for Mn/DOT Historic Roadside Development Structures Preservation and Restoration Report for Cold Spring Roadside Parking Area (Michael J. Burns Architects and Gemini Research, 2001).

Prepared by Gemini Research May 1, 2004.

Garrison Concourse Site Boundaries



MnDOT ROW



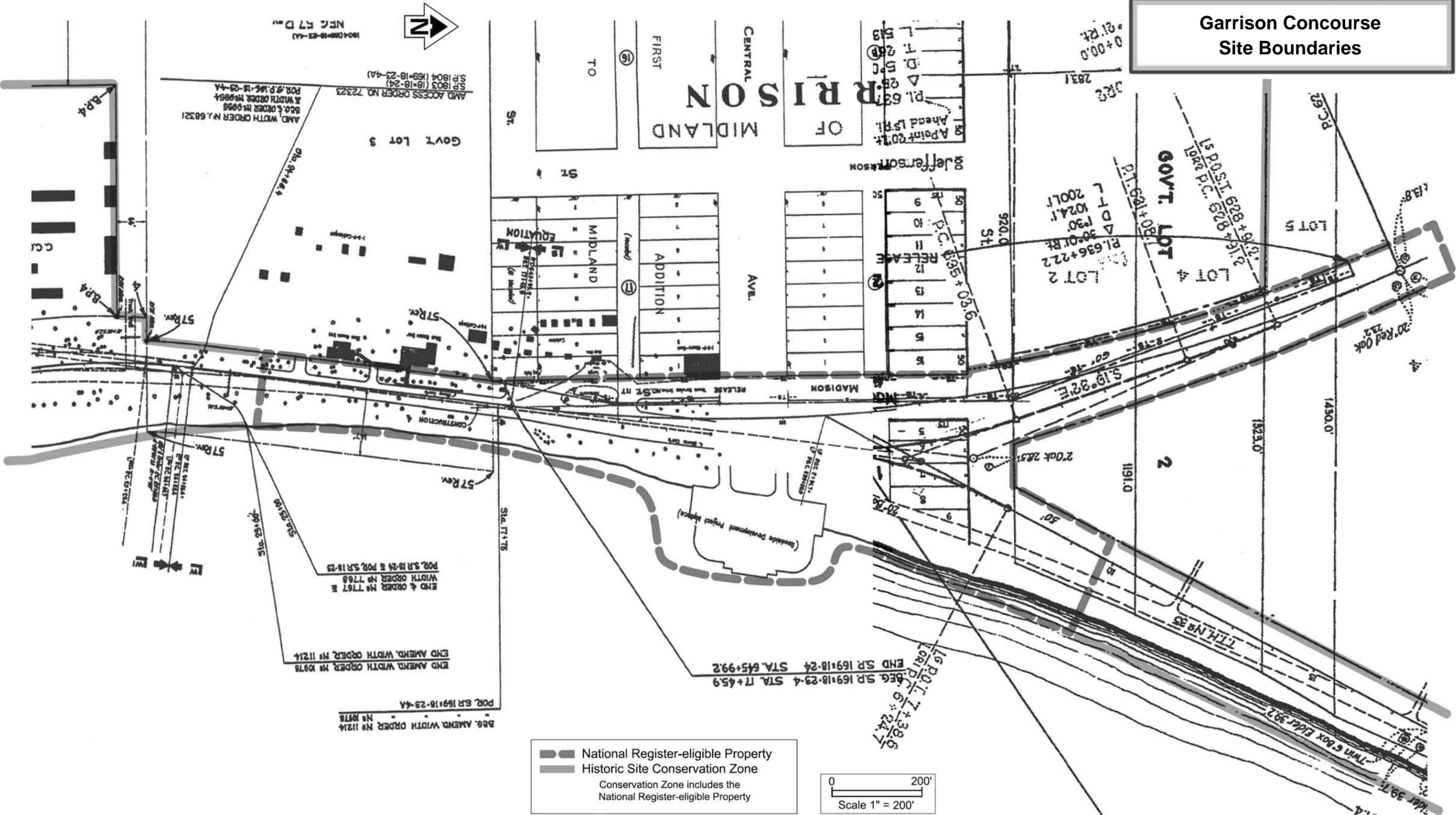
— National Register-eligible Property
— Historic Site Conservation Zone
Conservation Zone includes the
National Register-eligible Property

0 200'
Scale 1" = 200'

See "Mille Lacs Lake CCC Roadside Development,
Garrison Section" for entire Conservation Zone

Photo taken Spring 1999

**Garrison Concourse
Site Boundaries**



MILLE LACS

National Register-eligible Property
 Historic Site Conservation Zone
 Conservation Zone includes the
 National Register-eligible Property

0 200'
 Scale 1" = 200'

See "Mille Lacs Lake CCC Roadside Development, Garrison Section" for entire Conservation Zone