

NOTES & GUIDELINES

GENERAL INFORMATION:

1. THE CONTRACTOR SHALL FURNISH, INSTALL AND MAINTAIN THE DEVICES IN THIS TRAFFIC CONTROL PLAN UNLESS OTHERWISE NOTED.
2. FIELD CONDITIONS MAY REQUIRE MODIFICATIONS OF THIS LAYOUT AS DEEMED NECESSARY BY THE ENGINEER.
3. ALL DISTANCES ARE APPROXIMATE.
4. THE CONTRACTOR IS RESPONSIBLE FOR PROTECTING ANY WORK AREAS NEAR TRAFFIC IN ACCORDANCE WITH THE MN MUTCD.
5. AN ANNUAL FALL REVIEW OF ALL TRAFFIC CONTROLS WILL BE MADE TO PREPARE FOR WINTER MAINTENANCE OF THE PROJECT. THIS MAY INCLUDE ADJUSTMENTS OR EXCHANGE OF ONE TRAFFIC CONTROL DEVICE FOR ANOTHER. READJUSTMENTS MAY AGAIN BE REQUIRED IN THE SPRING.
6. IF THE CONTRACTOR DECIDES TO PERFORM THE CONSTRUCTION WORK IN A SEQUENCE OTHER THAN SHOWN IN THIS TRAFFIC CONTROL PLAN THE CONTRACTOR SHALL PROVIDE COMPLETE REVISED TRAFFIC CONTROL PLANS TO BE APPROVED BY THE ENGINEER.

SIGNING:

1. ALL TRAFFIC CONTROL DEVICES, INCLUDING OVERHEAD SIGNS, ON ROADS OPEN TO TRAFFIC THAT ARE NOT CONSISTENT WITH TRAFFIC OPERATION SHALL BE COVERED, REMOVED OR REVISED AS DIRECTED BY THE ENGINEER.
2. WHEN SIGNS ARE PLACED, THEY SHALL BE MOUNTED ON POSTS DRIVEN INTO THE GROUND AT THE PROPER HEIGHT AND LATERAL OFFSET AS SHOWN IN THE "TYPICAL TEMPORARY SIGN FRAMING & INSTALLATION DETAILS" IN THE PLAN. IF THIS IS NOT POSSIBLE, THEY WILL BE MOUNTED ON PORTABLE SUPPORTS AS APPROVED BY THE ENGINEER. WHEN THE SIGNS ARE REMOVED THE SIGN POSTS SHALL ALSO BE REMOVED AS SOON AS POSSIBLE.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY EXTRA SIGNING NEEDED TO FACILITATE TRAFFIC SWITCHES OR FOR TRANSITIONING TRAFFIC FROM ONE STAGE TO ANOTHER.
4. ALL ORANGE WARNING AND ORANGE GUIDE SIGNS SHALL BE FABRICATED WITH SIGN SHEETING MATERIAL AS LISTED ON THE MnDOT APPROVED PRODUCT LIST FOR "SHEETING FOR RIGID TEMPORARY WORK ZONE SIGNS, DELINEATORS, AND MARKERS".
5. BARRICADES SHALL BE FABRICATED WITH SIGN SHEETING MATERIAL AS LISTED ON THE MnDOT APPROVED PRODUCT LIST FOR "BARRICADE SHEETING",
6. LONGITUDINAL DROPOFFS SHALL BE SIGNED AS SHOWN IN THE "TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS" FIELD MANUAL UNLESS OTHERWISE SPECIFIED IN THESE PLANS.
7. THE CONTRACTOR SHALL COORDINATE THE PLACEMENT OF THE FINAL SIGNS TO ASSURE THAT THE FINAL SIGNS ARE PLACED AS NEEDED, OR PROVIDE TEMPORARY SIGNING AT THEIR EXPENSE UNTIL THE FINAL SIGNING IS PLACED.

PAVEMENT MARKING:

1. THE ENGINEER'S INVOLVEMENT IN THE APPLICATION OF THE MATERIAL SHALL BE LIMITED TO FIELD CONSULTATION AND INSPECTION. THE CONTRACTOR WILL PLACE NECESSARY "SPOTTING" AT APPROPRIATE POINTS TO PROVIDE HORIZONTAL CONTROL FOR STRIPING AND TO DETERMINE NECESSARY STARTING AND CUTOFF POINTS. LONGITUDINAL JOINTS, PAVEMENT EDGES AND EXISTING MARKINGS MAY SERVE AS HORIZONTAL CONTROL WHEN SO DIRECTED.
2. OBLITERATE ANY CONFLICTING PAVEMENT MARKINGS AS DIRECTED BY THE ENGINEER.
3. TRANSITIONS AND TAPERS FOR TEMPORARY STRIPING SHALL BE WET REFLECTIVE OR SUPPLEMENTED WITH TEMPORARY RAISED PAVEMENT MARKERS AND SHOULD BE A MINIMUM OF 6" IN WIDTH.
4. INTERIM STRIPING SHALL CONSIST OF ALL CENTERLINE, NO PASSING ZONES, PAINTED ISLANDS, AND LANE LINES (INCLUDING TURN LANE LINES).
5. TRPM'S (TEMPORARY RAISED PAVEMENT MARKERS) SHOULD BE USED TO SUPPLEMENT THE LONG TERM (MORE THAN 3 DAYS) EDGELINES ON ALL TRANSITION AREAS.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE LOCATION AND PLACEMENT OF TEMPORARY AND FINAL STRIPING. MnDOT TRAFFIC PERSONNEL WILL ASSIST IN THE SPOTTING OF TRANSITION AREAS, GORES AND TAPERS.

BARRIER & DELINEATION:














1. THE CONTRACTOR SHALL FURISH, INSTALL AND MAINTAIN TOP MOUNTED BARRIER DELINEATORS WITH A MINIMUM OF 24 SQ. IN. OF REFLECTIVE SURFACE AREA AND BE PLACED AT 25' SPACES ON TOP OF THE BARRIER WHEN THE BARRIER IS WITHIN 10' OF TRAFFIC UNLESS OTHERWISE NOTED OR AS DIRECTED BY THE ENGINEER. IF THE TRAFFIC ENGINEER REQUIRES SIDE MOUNTED BARRIER DELINEATORS, THEY WILL HAVE A MINIMUM OF 12 SQ. IN. OF REFLECTIVE SURFACE AREA AND BE PLACED AT 12.5' SPACES. IF A SMALLER APPROVED BARRIER DELINEATOR IS USED IT SHALL BE AT ONE HALF THE SPACING AND ONE HALF THE BID PRICE.

CONSTRUCTION INFORMATION SIGNING:




1. THE CONTRACTOR SHALL USE CONSTRUCTION INFORMATION SIGNING AS SHOWN IN THE PLAN AND WHICH ARE TO BE USED AS FOLLOWS:
2. G20-X1 CLOSURE NOTICE SIGNS PAIRED WITH G20-X3 WORK ENDS SIGNS TO DISPLAY THE CORRECT START DATE AND AN ESTIMATED FINISH DATE AS APPROVED BY THE PROJECT ENGINEER.
3. G20-X2 WORK ZONE ADVANCE NOTICE SIGNS WITH THE CORRECT STARTING DATE DISPLAYED BEFORE WORK BEGINS. ONCE WORK BEGINS, THE START DATE LEGEND SHALL BE COVERED BY THE SUGGESTED PLAQUE CONTAINED IN THIS PLAN. IF NO ALTERNATE MESSAGE IS SUGGESTED OR IF DIRECTED BY THE PROJECT ENGINEER, THE CORRECT ESTIMATED FINISH DATE, MONTH, OR SEASON SHALL BE DISPLAYED.
4. CONSTRUCTION INFORMATION SIGNING NOT VISIBLE TO THE MOTORING PUBLIC ONCE WORK BEGINS WILL BE MOVED BY THE CONTRACTOR TO A SITE IN ADVANCE OF THE WORK ZONE OR CLOSURE AS DIRECTED BY THE PLAN OR PROJECT ENGINEER.

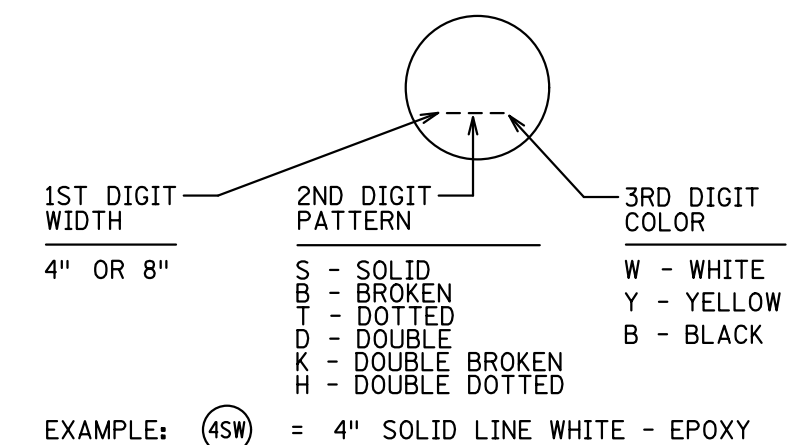
TRAFFIC CONTROL DEVICES & SYMBOLS LEGEND

SYMBOL DESCRIPTION

-  AREA CLOSED TO TRAFFIC / WORK AREA
-  TRAFFIC CONTROL SIGN
-  TYPE C BARRICADE = 
-  DRUM-LIKE CHANNELIZER (TYPE B) = 
-  TYPE A FLASHING WARNING LIGHT
-  FLASHING ARROW BOARD TYPE C =  (4' X 8' UNLESS OTHERWISE NOTED).
-  SOLID LINE PAVEMENT MARKING WITH TEMPORARY RAISED PAVEMENT MARKERS AT 10' SPACES
-  CONCRETE BARRIER WITH DELINEATORS AT 30' SPACES
-  IMPACT ATTENUATOR
-  PAVEMENT MESSAGE (LEFT ARROW SHOWN)

STRIPING KEY

-  CIRCLE - EPOXY
-  TRIANGLE - PAINT
-  PENTAGON - REMOVABLE PREFORMED PLASTIC MARKING



INDEX

TRAFFIC CONTROL SHEET NO. DESCRIPTIONS

1	TITLE SHEET
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5	ADVANCED SIGNING
6	DETOURS
7	ALTERNATE PEDESTRIAN ROUTE
8	STAGING
9	TYPICALS

I HEREBY CERTIFY THAT SHEETS _____ THROUGH _____ OF _____ THIS PLAN WERE PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA

DATE 5/31/2016 LIC. NO. _____ DESIGNER TECH NAME HERE

TITLE: **TRAFFIC CONTROL TITLE SHEET**

STATE PROJ. NO. 0000-000 (TH 000) SHEET NO _____ OF _____ SHEETS

PLOTTED/REVISED: 5/31/2016

DISTRICT #: METRO
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TRAFFIC CONTROL TABULATION



ITEM	UNIT	STAGE ONE	STAGE TWO	STAGE THREE	STAGE FOUR	TOTAL
PORTABLE PRECAST CONC BARRIER DES 8337	LIN FT					
RELOCATE PORT PRECAST CONC BAR DES 8337	LIN FT					
IMPACT ATTENUATOR BARRELS	EACH					
RELOCATE IMPACT ATTENUATOR BARRELS	EACH					
IMPACT ATTENUATOR ASSEMBLY	AMBY					
RELOCATE IMPACT ATTENUATOR ASSEMBLY	AMBY					
TUBE DELINEATOR	EACH					
REPLACE TUBE DELINEATOR	EACH					
TRAFFIC CONTROL	LUMP SUM					
PORTABLE CHANGEABLE MESSAGE SIGN	UNIT DAY					
MEDIAN BARRIER DELINEATOR	EACH					
RAISED PAVEMENT MARKER TEMP.	EACH					
4" SOLID LINE PAINT	LIN FT					
4" BROKEN LINE PAINT	LIN FT					
8" DOTTED LINE PAINT (WR)	LIN FT					
8" SOLID LINE PAINT (WR)	LIN FT					
PAVEMENT MARKING REMOVAL	SQ FT					
PAVEMENT MARKING REMOVAL	LIN FT					
POLICE OFFICER	HOUR					
PORTABLE CHANGEABLE MESSAGE SIGN	UNIT DAY					
WORKERS PRESENT SPEED LIMIT	UNIT DAY					
DYNAMIC MERGE SYSTEM	UNIT DAY					
TRAFFIC CONTROL SUPERVISOR	HOUR					
TRAFFIC CONTROL SUPERVISOR	EACH					
TRAFFIC CONTROL SUPERVISOR	LUMP SUM					
REMOVABLE PREFORMED PAVEMENT MARKING TAPE	LIN FT					
REMOVABLE PREFORMED PLASTIC MASK (BLACK)	LIN FT					

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 IPLOT NAME: fcsampleplan2
 PATH & FILENAME: IP_PWP-d11896\Ntcsampleplan.dgn

PAY ITEM TABULATION CHART

TRAFFIC CONTROL TABULATION SHEET

"R" SERIES			
SIGN	SIGN NO.	COLOR	SIZE
	R1-2	RED ON WHITE	36X36X36
	R2-1	BLACK ON WHITE	26X30
	R3-7L	BLACK ON WHITE	30X30
	R3-7R	BLACK ON WHITE	30X30
	R3-30AG	BLACK ON WHITE	36X30
	R9-9a	BLACK ON WHITE	24X18

DEVICES			
ITEM	SIGN NO.	COLOR	SIZE
	TYPE A		
	TYPE B		
	TYPE C		
	ARROW BOARD		96" X 48"
	AUDIBLE MESSAGE DEVICE		

"W" SERIES			
SIGN	SIGN NO.	COLOR	SIZE
	W20-1	BLACK ON ORANGE	48" x 48"
	W21-5	BLACK ON ORANGE	48" x 48"
	W21-X5R	BLACK ON ORANGE	48" X 48"
	W4-2R	BLACK ON ORANGE	48" X 48"
	W20-X3	BLACK ON ORANGE	48" X 48"
	W1-4R	BLACK ON ORANGE	48" X 48"
	W1-6	BLACK ON ORANGE	48" X 24"

"G" SERIES			
SIGN	SIGN NO.	COLOR	SIZE
	G20-2A	BLACK ON ORANGE	48" X 24"

"M" SERIES			

MISCELLANEOUS			

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DISTRICT #: METRO
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TRAFFIC CONTROL TABULATION CHART

PLOTTED/REVISED: 2/3/2016

SPECIAL SIGN DETAILS

DISTRICT #: METRO
PLOT NAME: fcsampleplan4
PATH & FILENAME: IP_PWP-dll896\Ntcsampleplan.dgn

DRAWN BY:			CHECKED BY:			CERTIFIED BY _____ LIC. NO. _____ DATE _____			STATE PROJ. NO. 0000-000 (TH 000) SHEET NO. _____ OF _____ SHEETS		
						LICENSED PROFESSIONAL ENGINEER			SPECIAL SIGN DETAILS		

SIGN DATA

SIGNS TO BE PLACED ON DRIVEN U-POSTS, SHALL BE PLACED IN ACCORDANCE WITH TABLE 1 OR TABLE 2 BELOW. IF THE TTC PLAN PLACES TTC DEVICES ADJACENT TO EXISTING STRUCTURES, THEN THEY SHOULD BE PLACED SLIGHTLY BEYOND THAT STRUCTURE, SO AS TO NOT COMPROMISE THAT STRUCTURE AND THE NEW DEVICE'S CRASHWORTHINESS (NO MORE THAN TWO U-POSTS WITHIN 84 INCHES, ALIGNED IN THE SAME PLANE). SIGN PANELS SHALL BE PLACED ON SIGN STRUCTURES TO MEET THE 5' MIN DEPICTED ON THE TYPICAL RURAL DESIGN DETAIL, THE 7' MIN DEPICTED ON THE TYPICAL URBAN DESIGN DETAIL, OR 7' MIN AND 9' MIN DEPICTED ON THE TYPICAL MOUNTING DETAIL ON THIS SHEET.

TABLE 1

STANDARD CONSTRUCTION SIGNS IN MN/DOT STANDARD SIGNS AND MARKINGS MANUAL

PANEL SIZE (IN.)	POSTS			
	NO. & TYPE	SPACING (IN.)	KNEE BRACES QUANT.	LENGTH (FT.)
24 x 24	2-U	18		13
30 x 24	2-U	18		13
36 x 30	2-U	24		13
36 x 36	2-U	18		14
42 x 36	2-U	30		14
48 x 48	2-U	30		15
60 x 60	2-U	42	1	16
72 x 72	2-U	42	2	17
96 x 54	2-U	54	2	16
168 x 132	4-U	48	4	20

GENERAL NOTES:

1. POST LENGTHS ARE APPROXIMATE AND INCLUDE EMBEDMENT, BUT DO NOT INCLUDE ADDITIONAL LENGTH REQUIRED FOR SPLICE.
2. SEE MN/DOT STANDARD SIGNS AND MARKINGS MANUAL FOR PUNCHING HOLES.

TABLE 2

SPECIAL DESIGN CONSTRUCTION SIGNS

PANEL SIZE		POSTS			
LENGTH (IN.)	HEIGHT (IN.)	NO. & TYPE	SPACING (IN.)	KNEE BRACES QUANT.	LENGTH (FT.)
54 - 96	78	2-U	42	2	20
102 - 138	78	3-U	45	3	20
144 - 180	78	4-U	45	4	20

DESIGNER NOTE:

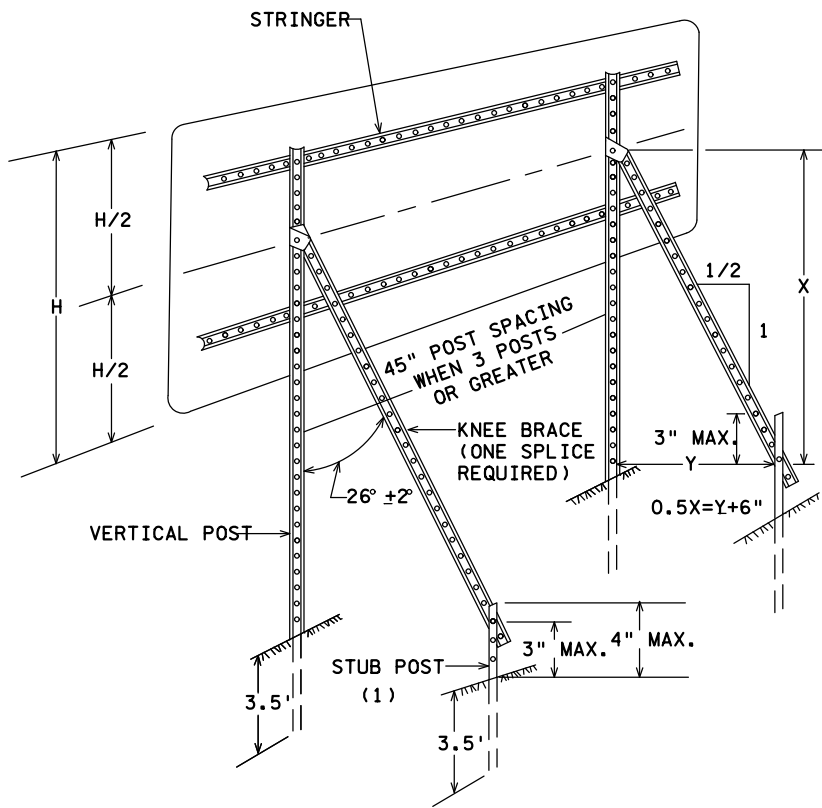
INCLUDE SPECIAL SIGN DETAILS IN THE TRAFFIC CONTROL PLAN IN TABLE TWO.

NOTES:

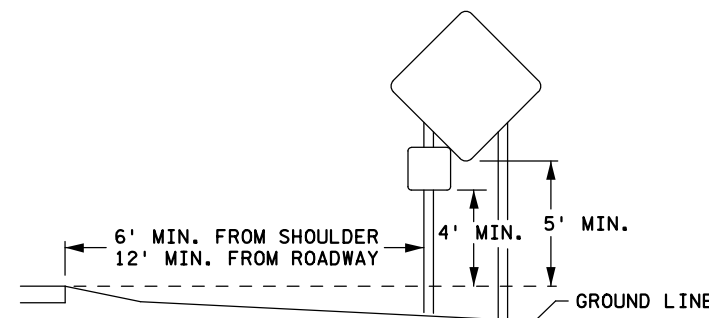
FOR TEMPORARY CONSTRUCTION SIGN FRAMING, THE CONTRACTOR MAY USE GRADE 5 ZINC PLATED BOLTS FOR ALL BOLTED CONNECTIONS, EXCEPT FOR THE KNEE BRACE CONNECTION TO THE REAR STUB POST, WHICH SHALL UTILIZE A 5/16 INCH STAINLESS STEEL BOLT AND NYLON INSERT LOCK NUT. ADDITIONAL SIGN FRAMING DETAILS CAN BE FOUND IN THE TRAFFIC ENGINEERING MANUAL PART 6.

IF THE CONTRACTOR ELECTS TO USE SOME OTHER TYPE OF SIGN SUPPORT (OTHER THAN U-CHANNEL SIGN POSTS) FOR MOUNTING CONSTRUCTION SIGNS, DETAILS OF THE PROPOSED SIGN STRUCTURE SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PRIOR TO ORDERING THE SIGN STRUCTURE COMPONENTS. ANY SIGN STRUCTURE TO BE SUBMITTED TO THE ENGINEER SHALL BE AN FHWA ACCEPTED BREAKAWAY SIGN SUPPORT. SIGN STRUCTURE SHALL ALSO BE APPROVED FOR 90 MPH WIND LOAD.

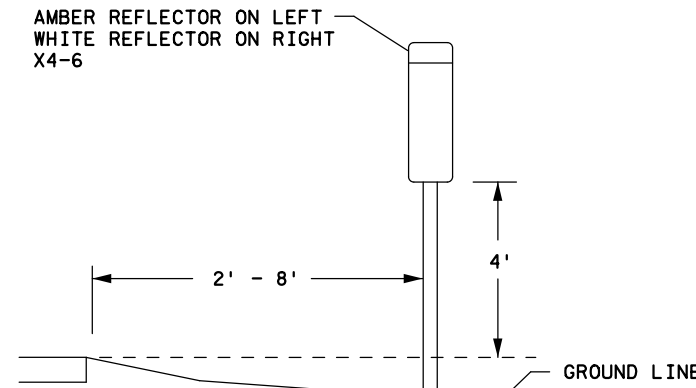
GUIDE SIGNS SHOWN TO BE COVERED SHALL BE COVERED WITH THE SAME COLOR AS THE SIGN BACKGROUND. THE CONTRACTOR SHALL PLACE COVERS OR ADDITIONAL SIGNS USING A MINIMUM 1/8" PLASTIC SPACER BETWEEN THE INPLACE SIGN AND THE COVERING MATERIAL. HOLES WILL BE DRILLED IN THE COVER AND THE INPLACE SIGN AND SHALL BE PLACED IN ACCORDANCE TO THE SIGN PANEL DETAIL. SPACERS ARE REQUIRED. MID-PANEL SPACING SHALL BE NO GREATER THAN 24".



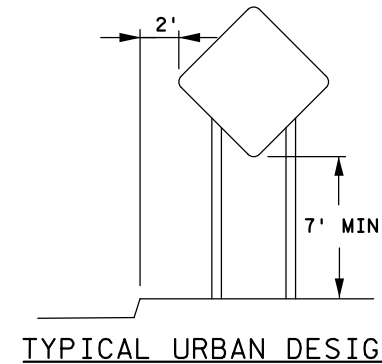
TYPICAL "A-FRAME" INSTALLATION TYPE "D" SIGNS



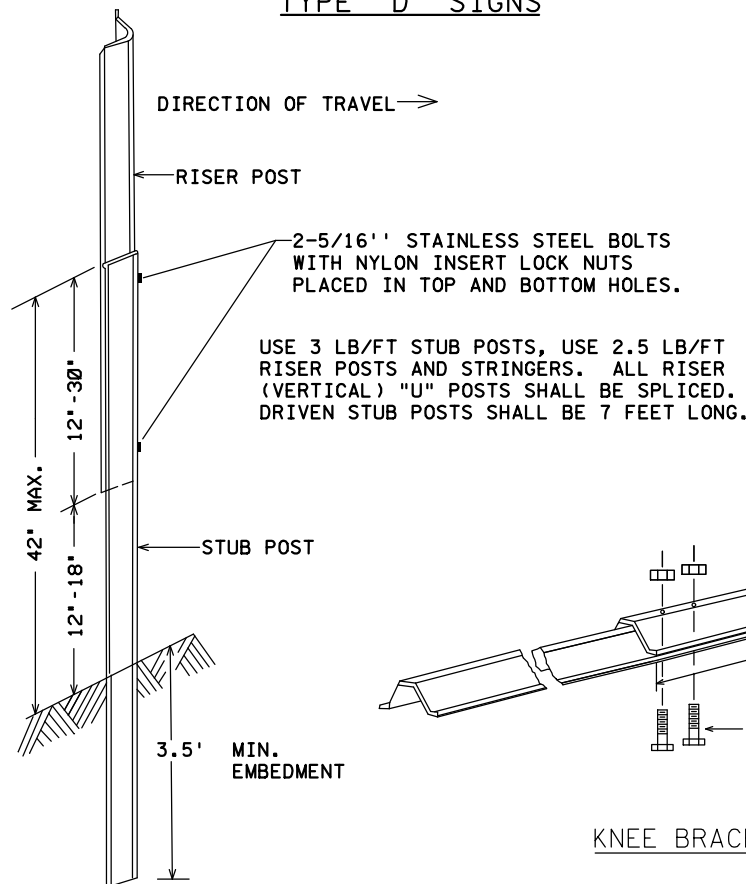
TYPICAL RURAL DESIGN



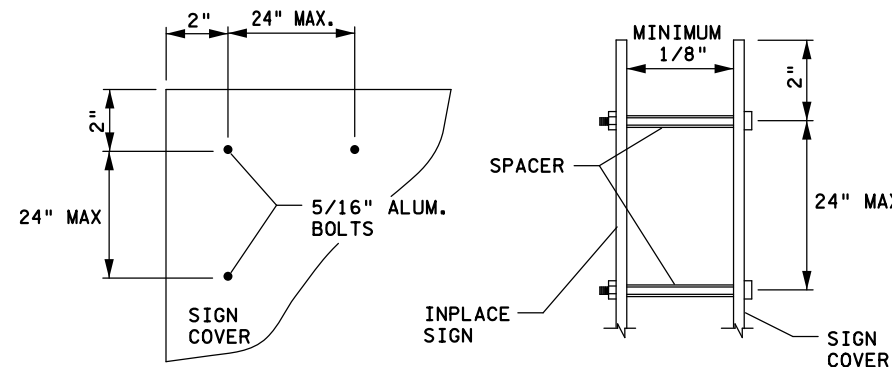
DELINEATION MOUNTING



TYPICAL URBAN DESIGN

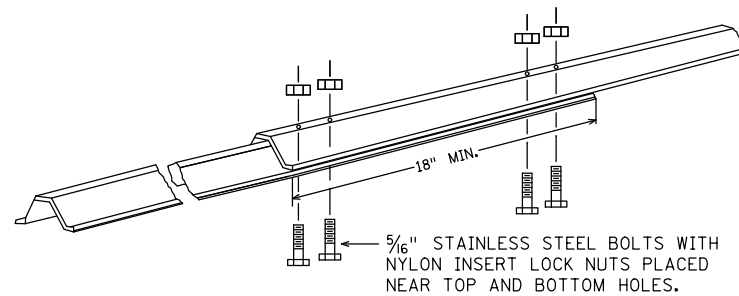


"U" POST BREAKAWAY SPLICE

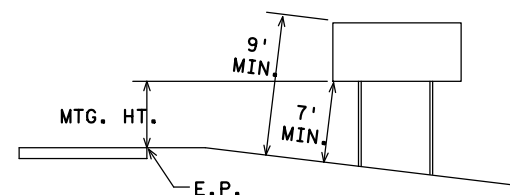


SIGN PANEL OVERLAY

SPACER SHALL BE A MATERIAL THAT DOES NOT HARM THE SIGN SHEETING FACE (SUCH AS PLASTIC OR RUBBER). REFER TO TRAFFIC ENGINEERING MANUAL CHAPTER 8 FOR MORE INFORMATION ON COVERING SIGNS



KNEE BRACE STRUCTURAL SPLICE



TYPICAL MOUNTING

(1) OFFSET STUB POST 1' TOWARD ROADWAY RELATIVE TO VERTICAL POST.

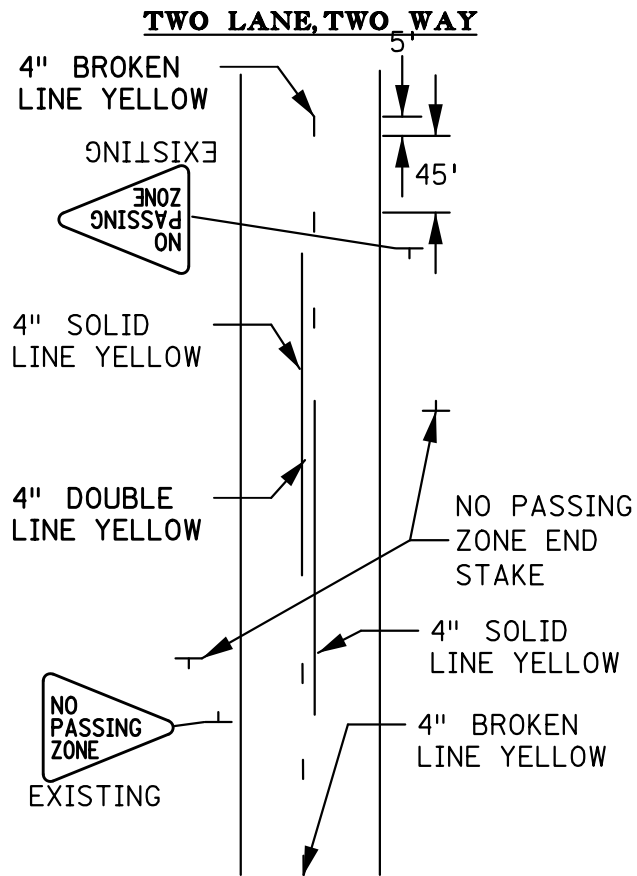
TYPICAL TEMPORARY SIGN FRAMING AND INSTALLATION DETAILS

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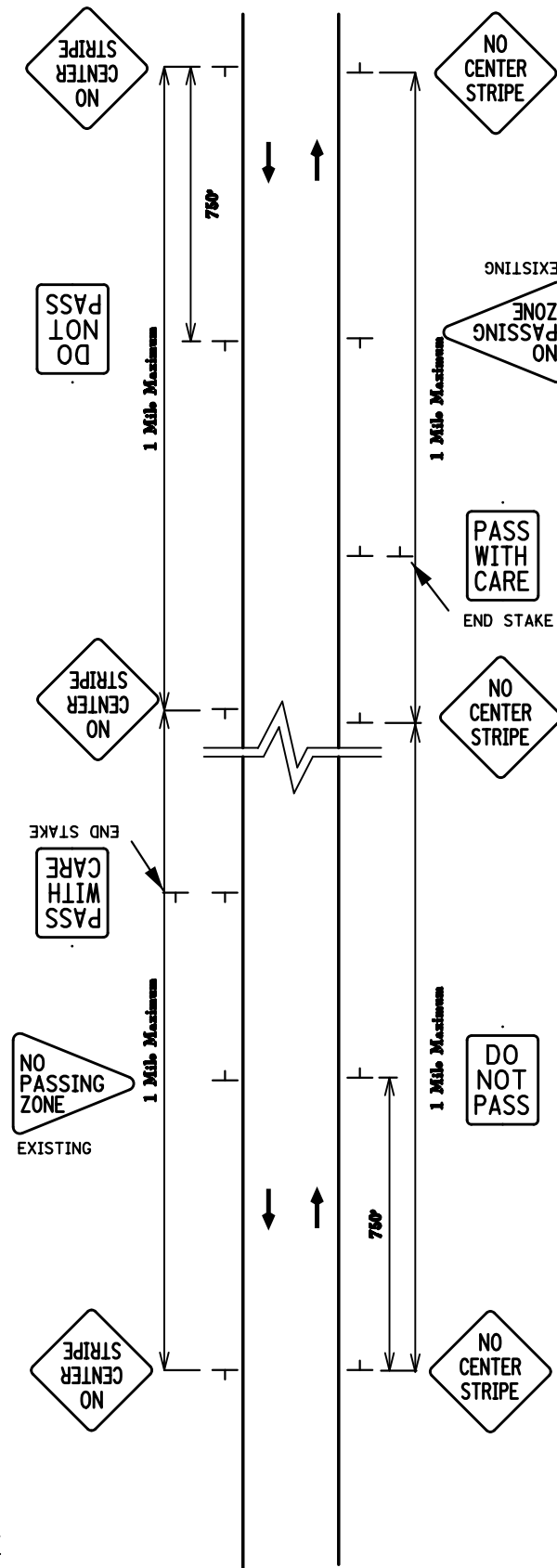
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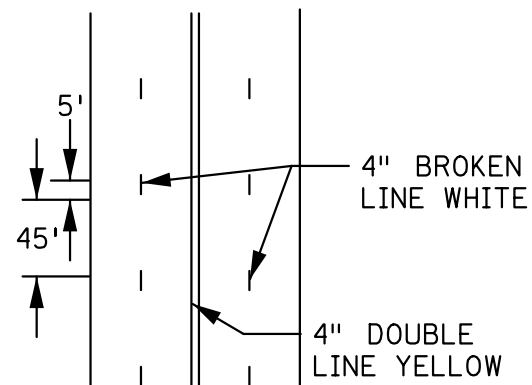
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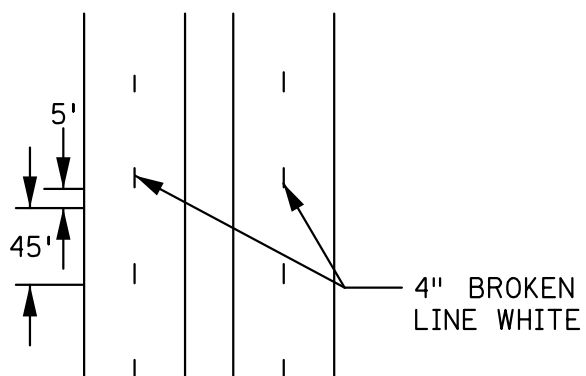
**TWO LANE, TWO WAY
WITHOUT MARKINGS
LESS THAN 400 ADT**



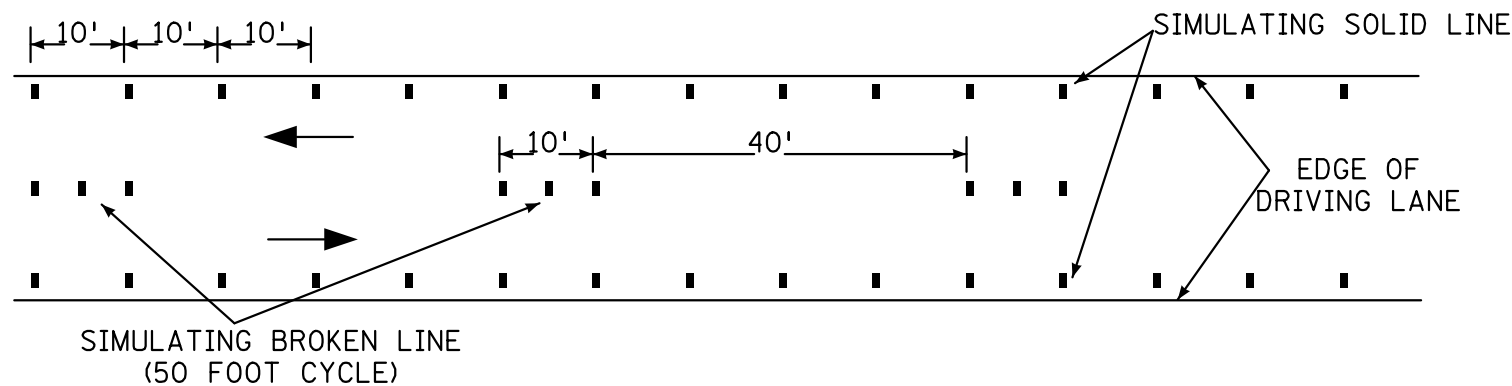
MULTI-LANE, UNDIVIDED



MULTI-LANE, DIVIDED



SIMULATING A SOLID LINE AND A BROKEN LINE (50 FOOT CYCLE) WITH TRPMS



USING TRPMS AS INTERIM PAVEMENT MARKING

WHEN TRPMS ARE USED TO SIMULATE A LINE THE FOLLOWING GUIDELINE APPLIES:

SKIP STRIPE - USES 3 TRPM'S PER 10' SKIP STRIPE ON 5' CENTERS WITH A 40' GAP

SOLID LINE - USES TRPM'S ON 10' CENTERS ON TANGENTS, FLATTER GRADES AND CURVES UNDER 6 DEGREES. FOR CURVES OVER 6 DEGREES AND STEEP GRADES, THIS SPACING SHALL BE REDUCED TO 5' CENTERS.

GENERAL NOTES:

SEE SPECIAL PROVISIONS FOR INTERIM PAVEMENT MARKING GUIDELINES

THESE INTERIM PAVEMENT MARKING GUIDELINES APPLY TO ALL TEMPORARY TRAFFIC CONTROL ZONES OF AT LEAST 300' IN LENGTH ON TANGENT AND 50' ON CURVES OF 6 DEGREES OR GREATER.

1. FOR ALL PROJECTS GREATER THAN 1.25 MILES IN LENGTH, INTERIM SKIP STRIPE PAVEMENT MARKINGS SHALL USE THE SAME CYCLE LENGTH AS FINAL PAVEMENT MARKINGS (50') AND SHALL BE A MINIMUM OF 5' LENGTH.
2. ON PROJECTS GREATER THAN 300' IN LENGTH, BUT LESS THAN 1.25 MILES IN LENGTH, THE INTERIM MARKING SHALL MATCH THE CYCLE LENGTH AT EITHER END OF THE PROJECT. THE INTERIM STRIPE SHALL BE 5' IN LENGTH.
3. INTERIM STRIPING SHALL CONSIST OF ALL CENTERLINE, NO PASSING ZONES, PAINTED ISLANDS, AND LANE LINES (INCLUDING TURN LANE LINES).

ALL INTERIM MARKINGS SHALL BE PLACED PRIOR TO REMOVING LANE CLOSURE OR OPENING THE ROADWAY TO TRAFFIC.

FINAL MARKINGS AND ALL OTHER PAVEMENT MARKINGS INCLUDING EDGELINES, CHANNELIZING LINES, TURN LANE LINES, LANE REDUCTION TRANSITIONS, GORE MARKINGS AND OTHER LONGITUDINAL MARKINGS AND THE VARIOUS NON-LONGITUDINAL MARKINGS (STOP LINES, RAIL ROAD CROSSINGS, CROSSWALKS, WORDS, SYMBOLS, ETC) SHOULD BE PLACED WITHIN 14 CALENDAR DAYS OF OPENING ROAD TO TRAFFIC.

NOTE: WHEN FINAL MARKINGS ARE TO BE EPOXY AND PAINT IS USED FOR INTERIM SOLID LINES, A 10 MIL THICK LAYER APPLICATION OF A WATER-BASED TRAFFIC MARKING PAINT SHALL BE USED. WITH A 10 MIL LAYER OF PAINT APPLIED, BEADS SHOULD BE APPLIED AT A RATE OF 6 LBS/GAL. REMOVAL OF THE 10 MIL LAYER OF PAINT IS NOT REQUIRED PRIOR TO PLACING THE EPOXY.

USING SIGNING AS INTERIM PAVEMENT MARKING

ON LOW VOLUME ROADS WITH AN ADT (AVERAGE DAILY TRAFFIC) OF LESS THAN 400 VEHICLES, THE SIGNS MAY BE USED IN LIEU OF PAVEMENT MARKINGS FOR UP TO 14 CALENDAR DAYS (SEE SECTION 5A.1 OF THE MMUTCD) OR AS DIRECTED BY THE PROJECT ENGINEER.

1. IF NO INTERIM MARKINGS ARE USED A "NO CENTER STRIPE" SIGN (W8-12) SHALL BE USED FOR EACH DIRECTION OF TRAVEL. THIS SIGN SHALL BE REPEATED AT MAJOR INTERSECTIONS OR ONE MILE INTERVALS, WHICHEVER IS GREATER.
2. IF NOT ALREADY IN PLACE, A "DO NOT PASS" SIGN (R4-1) SHALL BE PLACED ON THE RIGHT SIDE OF THE ROAD AT THE BEGINNING OF THE ZONE OPPOSITE OF THE "NO PASSING ZONE" SIGN (W14-3)

INTERIM PAVEMENT MARKINGS