NOTES & GUIDELINES

GENERAL INFORMATION:

- 1. THE CONTRACTOR SHALL FURNISH, INSTALL AND MAINTAIN THE DEVICES IN THIS TRAFFIC CONTROL PLAN UNLESS OTHERWISE NOTED.
- 2. FIELD CONDITIONS MAY REQUIRE MODIFICATIONS OF THIS LAYOUT AS DEEMED NECESSARY BY THE ENGINEER.
- 3. ALL DISTANCES ARE APPROXIMATE.
- 4. THE CONTRACTOR IS RESPONSIBLE FOR PROTECTING ANY WORK AREAS NEAR TRAFFIC IN ACCORDANCE WITH THE MN MUTCD.
- 5. AN ANNUAL FALL REVIEW OF ALL TRAFFIC CONTROLS WILL BE MADE TO PREPARE FOR WINTER MAINTENANCE OF THE PROJECT. THIS MAY INCLUDE ADJUSTMENTS OR EXCHANGE OF ONE TRAFFIC CONTROL DEVICE FOR ANOTHER. READJUSTMENTS MAY AGAIN BE REQUIRED IN THE SPRING.
- 6. IF THE CONTRACTOR DECIDES TO PERFORM THE CONSTRUCTION WORK IN A SEQUENCE OTHER THAN SHOWN IN THIS TRAFFIC CONTROL PLAN THE CONTRACTOR SHALL PROVIDE COMPLETE REVISED TRAFFIC CONTROL PLANS TO BE APPROVED BY THE ENGINEER.

SIGNING:

- 1. ALL TRAFFIC CONTROL DEVICES, INCLUDING OVERHEAD SIGNS ON ROADS OPEN TO TRAFFIC THAT ARE NOT CONSISTENT WITH TRAFFIC OPERATION SHALL BE COVERED, REMOVED OR REVISED AS DIRECTED BY THE ENGINEER.
- 2. WHEN SIGNS ARE PLACED, THEY SHALL BE MOUNTED ON POSTS DRIVEN INTO THE GROUND AT THE PROPER HEIGHT AND LATERAL OFFSET AS SHOWN IN THE TYPICAL TEMP SIGN FRAMING & INSTALLATION DETAILS IN THE PLAN. IF THIS IS NOT POSSIBLE THEY WILL BE MOUNTED ON PORTABLE SUPPORTS AS APPROVED BY THE ENGINEER. WHEN THE SIGNS ARE REMOVED THE SIGN POSTS SHALL ALSO BE REMOVED AS SOON AS POSSIBLE.
- 3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY EXTRA SIGNING NEEDED TO FACILITATE TRAFFIC SWITCHES OR FOR TRANSITIONING TRAFFIC FROM ONE STAGE TO ANOTHER.
- 4. ALL ORANGE WARNING AND ORANGE GUIDE SIGNS SHALL BE FABRICATED WITH SIGN SHEETING MATERIAL AS LISTED ON THE MODOT APPROVED PRODUCT LIST FOR "SHEETING FOR RIGID TEMPORARY WORK ZONE SIGNS".
- 5. BARRICADES SHALL BE FABRICATED WITH SIGN SHEETING MATERIAL AS LISTED ON THE Modot Approved Product List for Barricade Sheeting.
- 6. LONGITUDINAL DROPOFFS SHALL BE SIGNED AS SHOWN IN THE "TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS" FIELD MANUAL UNLESS OTHERWISE SPECIFIED IN THESE PLANS.
- 7. THE CONTRACTOR SHALL COORDINATE THE INSTALLATION OF THE FINAL SIGNS TO ASSURE THAT THE FINAL SIGNS ARE PLACED AS NEEDED, OR PROVIDE TEMPORARY SIGNING AT THEIR EXPENSE UNTIL THE FINAL SIGNING IS PLACED.

PAVEMENT MARKING:

- 1. OBLITERATE ANY CONFLICTING PAVEMENT MARKINGS AS DIRECTED BY THE ENGINEER.
- 2. PAINT, POLYMER LANE TAPE AND/OR TRPM'S ARE ACCEPTABLE TEMPORARY STRIPING ALTERNATIVES ACCORDING TO ACTUAL CONDITIONS ENCOUNTERED AS DIRECTED BY THE ENGINEER. GENERALLY, ONLY PAINT WILL BE USED BEFORE MAY 1ST OR WHEN THE OTHER MANUFACTURERS' SPECIFICATIONS CAN NOT BE MET.
- 3. TRPM'S (TEMPORARY RAISED PAVEMENT MARKERS) SHOULD BE USED TO SUPPLEMENT THE LONG TERM (MORE THAN 3 DAYS) EDGELINES ON ALL TRANSITION AREAS WHEN THE CONDITIONS ARE WITHIN THE MANUFACTURERS' SPECIFICATIONS.
- 4. SEE 1404 IN THE SPECIAL PROVISIONS FOR STRIPING SPOTTING RESPONSIBILITIES.

TRAFFIC CONTROL DEVICES & SYMBOLS LEGEND

SYMBOL DESCRIPTION

TRAFFIC CONTROL SIGN

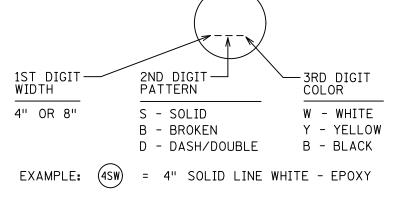
└── TYPE III BARRICADE =

TYPE A FLASHING WARNING LIGHT

PAVEMENT MESSAGE (CROSS WALK) PREF TAPE

STRIPING KEY





INDEX

TRAFFIC CONTROL SHEET NO. DESCRIPTIONS

TC1 TRAFFIC CONTROL TITLE SHEET

TC2 PAY ITEM AND STANDARD SIGN TAB SHEET

TC3-TC7 PEDESTRIAN DETOUR

DETAILS

TC8-TC9 TYPICALS

I HEREBY CERTIFY THAT SHEETS TC1 THROUGH TC9 OF THIS PLAN WERE PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

ENGINEER: LICENSE #_____

SIGNATURE:_____ DATE: \$IDATE1\$

DESIGNER
TITLE:

TRAFFIC CONTROL TITLE SHEET

STATE PROJ. NO.

SHEET NO. TC1 OF TC9 SHEETS

TRAFFIC CONTROL PAY ITEM TABULATION						TC
ITEM	UNIT	STAGE ONE	STAGE TWO	STAGE THREE	STAGE FOUR	TOTAL
TRAFFIC CONTROL	LUMP SUM	.25	. 25	.25	. 25	1
REMOVABLE PREFORMED PAVEMENT MESSAGE TAPE	SQ.FT	288				288

.25

.25

NOTES:

ALTERNATE PEDESTRIAN ROUTE

REMOVABLE PREFORMED PAVEMENT MARKING TAPE

(1) 8" SOLID LINE WHITE.

STANDARD SIGN TABULATION

122

.25

LIN FT LUMP SUM

"R" SERIES				
SIGN	SIGN NO.	COLOR	SIZE (IN. X IN.)	
SIDEWALK CLOSED	R9-9	BLACK ON WHITE	30" X 18"	
SIDEMALA CLOSED ANICAD CROSS NERE	R9-11R	BLACK ON WHITE	48" X 36"	
SIDEMALK CLOSED AREA CROSS NERE	R9-11L	BLACK ON WHITE	48" X 36"	
SIDEWALK CLOSED CROSS HERE	R9-11aL	BLACK ON WHITE	48" X 24"	
SIDEWALK CLOSED CROSS HERE	R9-11aR	BLACK ON WHITE	48" X 24"	
STOP STOP RED	R10-6	BLACK ON WHITE	36" X 48"	

<u>"M" SERIES</u>						
SIGN	SIGN NO.	COLOR	R SIZE (IN. X IN.)			
DETOUR	M4-9mbT	BLACK ON ORANGE	30" X 24"			
DETOUR DETOUR	M4-mb	BLACK ON ORANGE	30" X 24"			

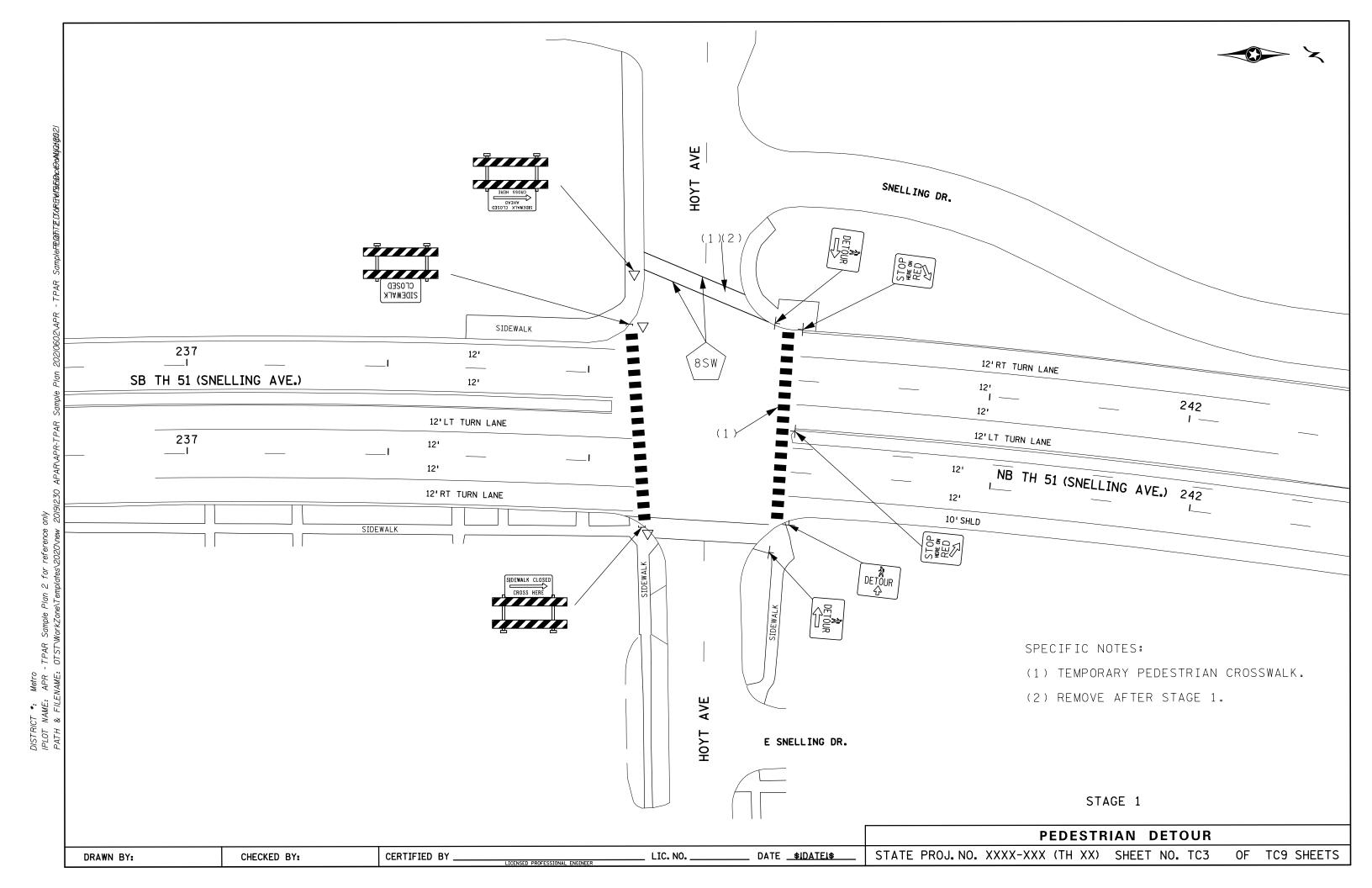
"W" SERIES				
SIGN	SIGN NO.	COLOR	SIZE (IN. X IN.)	
À	W11-2	BLACK ON YELLOW - GREEN	48" × 48"	
	W16-7mP	BLACK ON YELLOW - GREEN	36" × 24"	

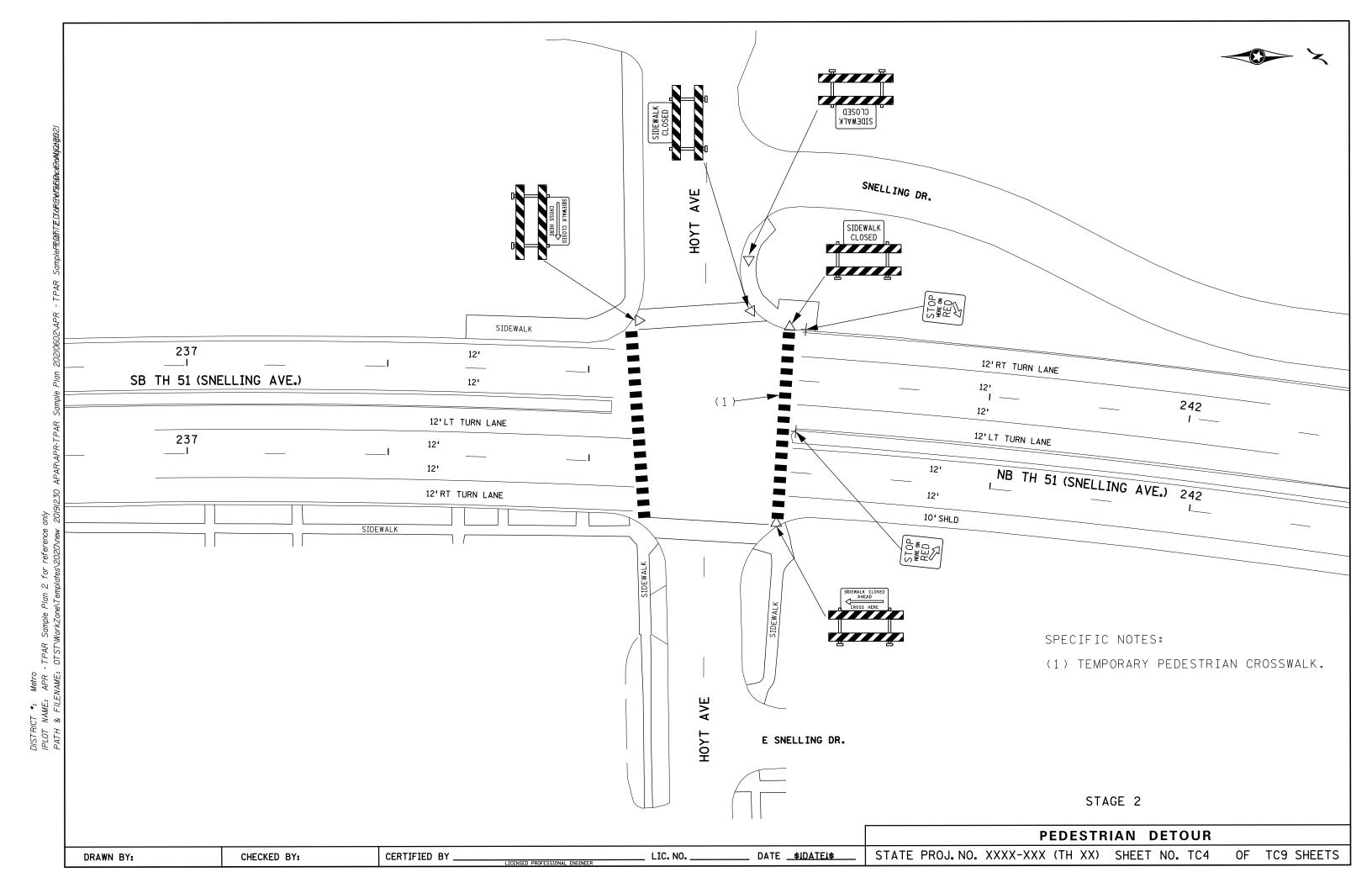
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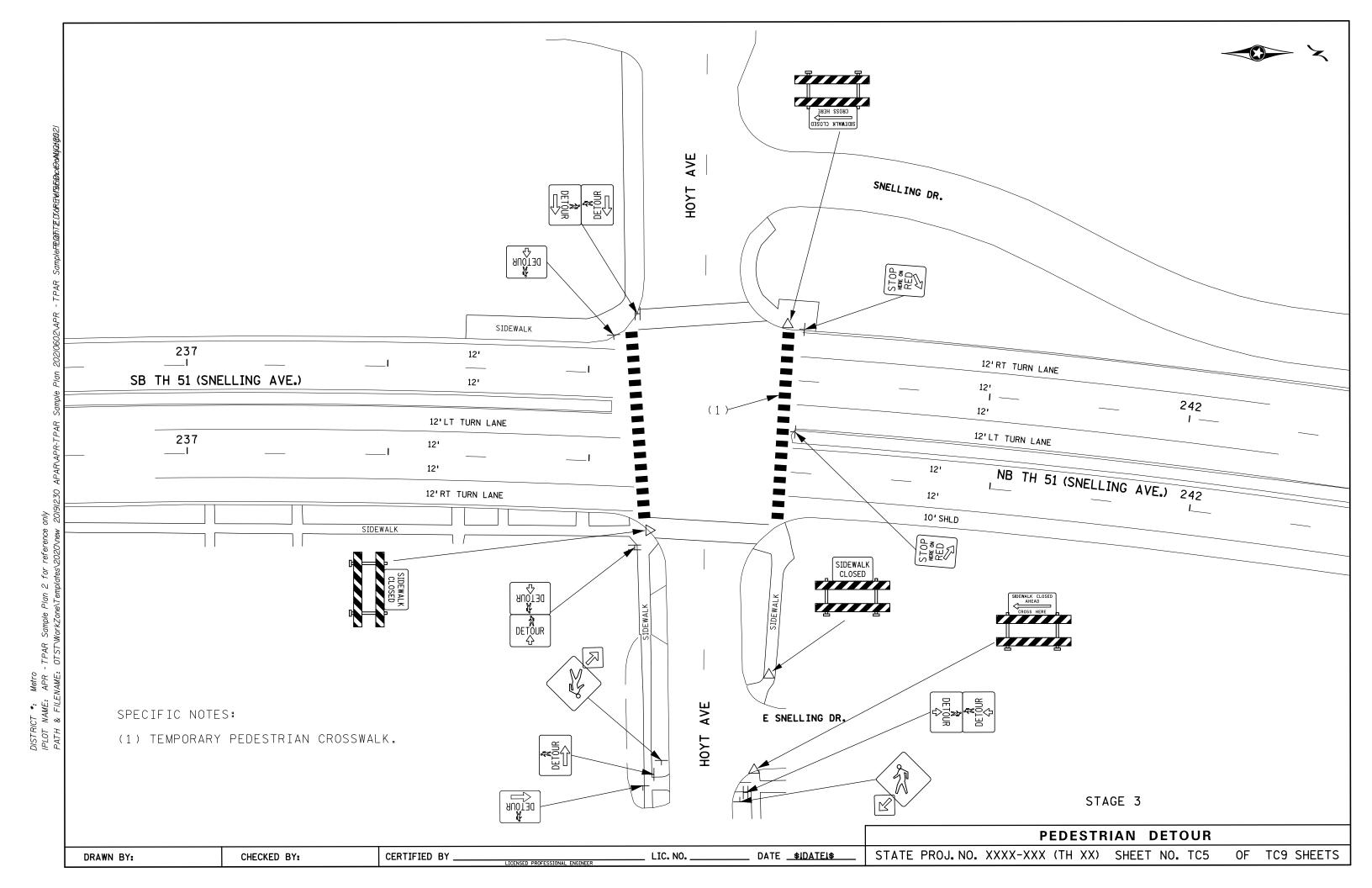
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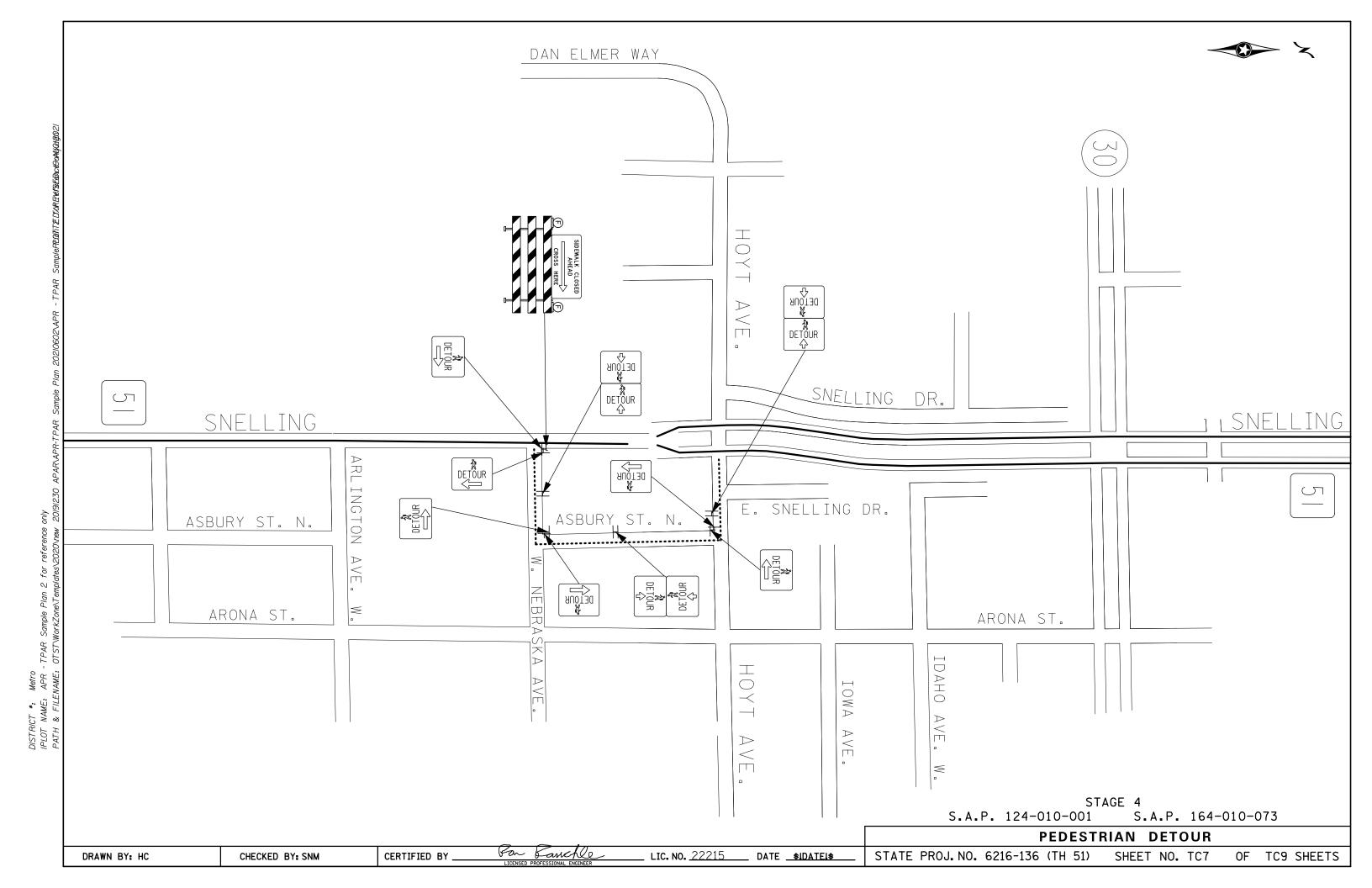
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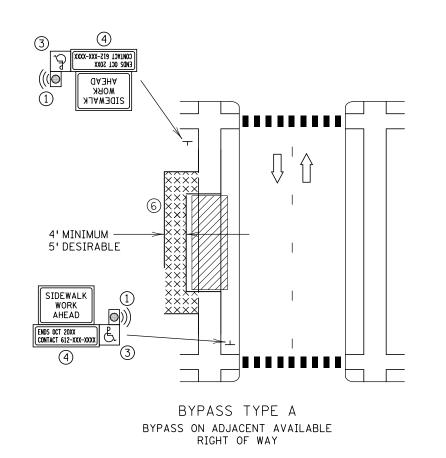
L						PAY ITEM AND	STANDARD SIGN T	AB SHEET
	DRAWN BY:	CHECKED BY:	CERTIFIED BY	_ LIC. NO	DATE <u>\$IDATEI</u> \$	STATE PROJ. NO.	SHEET NO. TC2	OF TC9 SHEETS

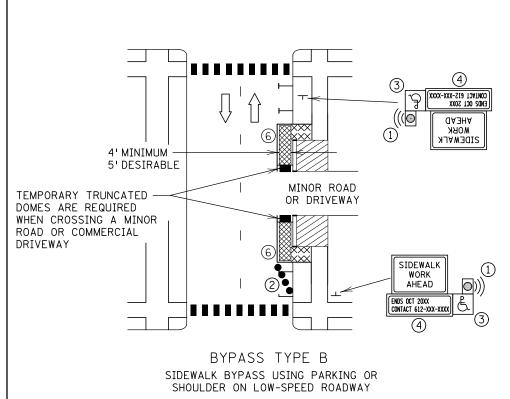


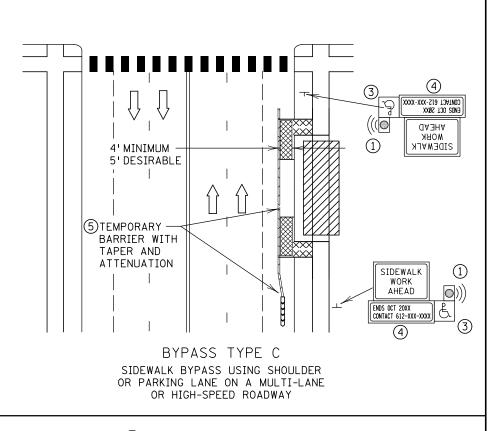












WHEN CLOSING OR RELOCATING CROSSWALKS OR SIDEWALKS, PROVIDE DETECTABLE TEMPORARY FACILITIES AND INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH EXISTING PEDESTRIAN FACILITIES. THE ALTERNATE PEDESTRIAN ROUTE (APR) MUST REMAIN OPEN AT ALL TIMES.

TEMPORARY TRAFFIC CONTROL DEVICES FOR PEDESTRIANS ARE SHOWN. OTHER DEVICES MAY BE NECESSARY TO CONTROL VEHICULAR TRAFFIC. STAGE WORK AS NECESSARY TO PROVIDE AN APR AT ALL TIMES FOR ROADWAYS WITH NO AVAILABLE DETOURS. PROVIDE A SMOOTH, CONTINUOUS, HARD SURFACE THROUGH THE LENGTH OF THE APR.

PROVIDE A FIRM, STABLE, FREE-DRAINING, NON-SLIP, TEMPORARY WALKWAY SURFACE REGARDLESS OF WEATHER CONDITIONS. SUPPORT THE TEMPORARY WALKWAY SURFACE WITH A SOLID BASE TO COVER SHORT SEGMENTS OF ROUGH, SOFT, OR UNEVEN GROUND, THE TEMPORARY WALKWAY SURFACE WILL ALLOW NORMAL USAGE OF WHEELCHAIRS, WALKERS, STROLLERS, AND OTHER MOBILITY DEVICES. CONCRETE, BITUMINOUS, STEEL, RUBBER, WOOD (3/4" OR THICKER), AND PLASTIC ARE ACCEPTABLE SURFACE MATERIALS FOR THE TEMPORARY WALKWAY SURFACE. GRAVEL, MILLINGS, AND OTHER UNEVEN SURFACES ARE NOT ACCEPTABLE SURFACE MATERIALS. IF NEEDED, PROVIDE SOIL STABILIZATION TO PREVENT EROSION AROUND TEMPORARY SURFACES. IF NEEDED, PROVIDE SOIL STABILIZATION TO PREVENT EROSION AROUND TEMPORARY SURFACES.

IF A 60" PEDESTRIAN WALKWAY WIDTH ISN'T PROVIDED FOR THE ROUTE, THEN A 60" BY 60" PASSING SPACE IS REQUIRED EVERY 200'. THE MINIMUM WIDTH OF THE WALKWAY IS 48".

COVER OR DEACTIVATE ANY PEDESTRIAN TRAFFIC SIGNALS CONTROLLING CLOSED CROSSWALKS.

POST-MOUNTED SIGNS LOCATED ADJACENT TO A SIDEWALK SHALL HAVE A 7'MINIMUM CLEARANCE FROM THE BOTTOM OF THE LOWEST SIGN TO THE SIDEWALK SURFACE. SHARED-USE PATH SHALL HAVE 8' MINIMUM CLEARANCE FROM THE BOTTOM OF THE LOWEST SIGN TO THE SHARED USE PATH SURFACE.

APR SHOULD BE KEPT FREE OF TRASH, SEDIMENT, AND DEBRIS.

ANY PORTABLE SIGN OR BARRICADE PLACED OR STORED IN A PEDESTRIAN WALKWAY THAT COULD POSE A HAZARD TO A VISUALLY-IMPAIRED PEDESTRIAN SHALL HAVE A DETECTABLE EDGE TO GUIDE THE PEDESTRIAN AROUND THE HAZARD, FOR ADDITIONAL GUIDANCE, SEE THE "DETECTABLE EDGE FOR SIGN ON PORTABLE STAND" DETAIL ON STADARD PLAN 5-297.813.

MINIMIZE DISRUPTION TO PEDESTRIANS TO THE MAXIMUM EXTENT FEASIBLE BY PROVIDING AN APR IN THE FOLLOWING ORDER OF PREFERENCE:

- PROVIDE THE APR ON THE SAME SIDE OF THE ROADWAY AS THE DISRUPTED ROUTE UTILIZING 2. BYPASSES.
- WHERE NOT FEASIBLE TO PROVIDE A SAME-SIDE APR, PROVIDE AN APR DETOUR ON THE OTHER SIDE 3. OF THE ROADWAY.
 - WHERE NOT FEASIBLE TO PROVIDE AN APR ON EITHER SIDE OF THE ROADWAY, PROVIDE AN APR DETOUR WITH TRAILBLAZING SIGNS.
- (1) CONSIDER PROVIDING AN APPROVED AUDIBLE MESSAGE DEVICE OR TACTILE MESSAGE FOR PEDESTRIANS WITH VISUAL DISABILITIES.
- (2) RECOMMENDED TAPER WHEN THE CLOSED AREA WAS PREVIOUSLY USED AS AN INTERMITTENT TRAFFIC LANE OR BYPASS LANE IS 25'LONG USING FIVE EQUALLY-SPACED CHANNELIZING DEVICES.
- (3) FOR FULLY-ACCESSIBLE WALKWAYS THROUGH WORKZONES, CONSIDER DISPLAYING THE INTERNATIONAL SYMBOL OF ACCESSIBILITY.
- (4) INCLUDE INFORMATION SUCH AS THE DURATION OF THE WALKWAY RESTRICTIONS (BEGINNING AND/OR END DATES) AND A PROJECT CONTACT NUMBER FOR 24/7 QUESTIONS OR REPORTING HAZARDS ON SIGNS FOR TEMPORARY PEDESTRIAN DETOURS.
- (5) SEE THE MOST CURRENT EDTION OF THE MNDOT TEMPORARY BARRIER GUIDANCE MANUAL FOR GUIDANCE ON PLACEMENT AND USAGE OF TEMPORARY BARRIER.

(6) PROVIDE SOIL STABILIZATION AROUND TEMPORARY SURFACES TO PREVENT EROSION, IF NEEDED.

LEGEND

SIGN

WORK AREA

PEDESTRIAN CHANNELIZATION DEVICE

TEMPORARY BARRIER

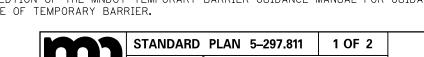
DIRECTION OF TRAFFIC

CHANNELIZER

AUDIBLE MESSAGE DEVICE (AMD)

TEMPORARY CURB RAMP WITH DETECTABLE EDGES

TEMPORARY WALKWAY SURFACE







(TH SHEET NO. 0F

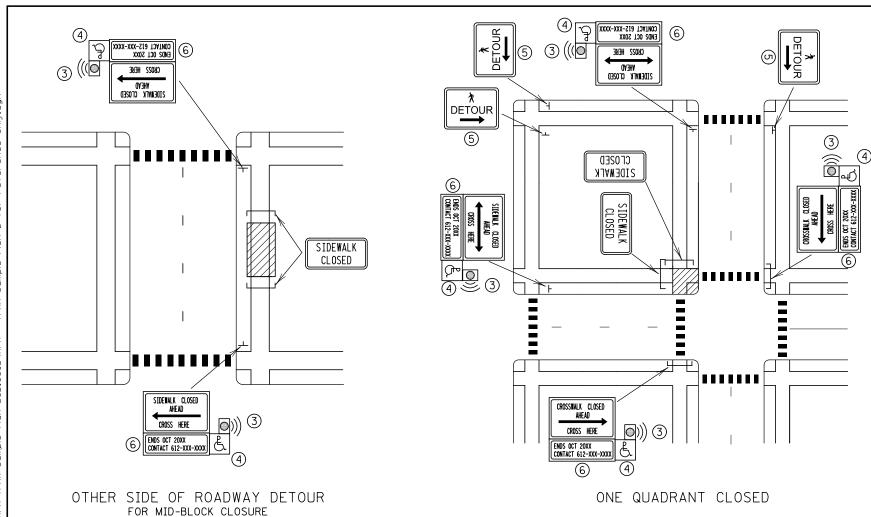
STATE TRAFFIC ENGINEER

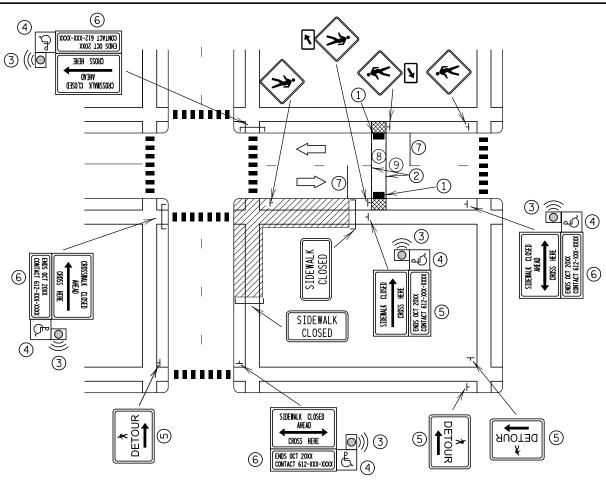
APPROVED: 03-18-2021

REVISION:

OF TRANSPORTATION

SHEETS





OTHER SIDE OF STREET DETOUR OR DETOUR WITH TRAILBLAZING SIGNS FOR CORNER SIDEWALK CLOSURE WITH OPTIONAL TEMPORARY CROSSWALK

WHEN CLOSING OR RELOCATING CROSSWALKS OR SIDEWALKS, PROVIDE DETECTABLE TEMPORARY FACILITIES AND INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH EXISTING PEDESTRIAN FACILITIES. THE MINIMUM TEMPORARY WALKWAY WIDTH SHOULD BE THE WIDTH OF THE EXISTING FACILITY. IF THE EXISTING FACILITY HAS A WIDTH GREATER THAN 60". THE WIDTH OF THE TEMPORARY FACILITY MAY BE 60". IF THE WIDTH OF THE DETOUR IS LESS THAN 60", A 60"-WIDE PASSING SPACE IS REQUIRED EVERY 200'.

TEMPORARY TRAFFIC CONTROL DEVICES FOR PEDESTRIANS ARE SHOWN. OTHER TRAILBLAZING SIGNS OR DEVICES MAY BE NEEDED FOR ADEQUATE ROUTING. STAGE WORK AS NECESSARY TO PROVIDE AN ALTERNATE PEDESTRIAN ROUTE (APR) AT ALL TIMES.

PROVIDE A SMOOTH, CONTINUOUS, HARD SURFACE THROUGH THE LENGTH OF THE APR. PROVIDE A FIRM, STABLE, FREE-DRAINING, NON-SLIP, TEMPORARY WALKWAY SURFACE REGARDLESS OF WEATHER CONDITIONS. SUPPORT THE TEMPORARY WALKWAY SURFACE WITH A SOLID BASE TO COVER SHORT SEGMENTS OF ROUGH, SOFT, OR UNEVEN GROUND. THE TEMPORARY WALKWAY SURFACE WILL ALLOW NORMAL USAGE OF WHEELCHAIRS, WALKERS, STROLLERS, AND OTHER MOBILITY DEVICES. CONCRETE, BITUMINOUS, STEEL RUBBER, WOOD (3/4" OR THICKER), AND PLASTIC ARE ACCEPTABLE SURFACE MATERIALS FOR THE TEMPORARY WALKWAY SURFACE. GRAVEL, MILLINGS, OR OTHER UNEVEN SURFACES ARE NOT ACCEPTABLE SURFACE MATERIALS. IF NEEDED, PROVIDE SOIL STABILIZATION TO PREVENT EROSION AROUND TEMPORARY SURFACES.

COVER OR DEACTIVATE ANY PEDESTRIAN TRAFFIC SIGNALS CONTROLLING CLOSED CROSSWALKS.

APR SHOULD BE KEPT FREE OF TRASH, SEDIMENT, AND DEBRIS.

REVISION: APPROVED: 03-18-2021 STATE TRAFFIC ENGINEER POST-MOUNTED SIGNS ADJACENT TO SIDEWALKS SHALL HAVE 7'MINIMUM CLEARANCE FROM THE BOTTOM OF THE LOWEST SIGN TO THE SIDEWALK SURFACE, SHARED-USE PATHS SHALL HAVE 8'MINIMUM CLEARANCE FROM THE BOTTOM OF THE LOWEST SIGN TO THE SHARED-USE PATH SURFACE.

ANY PORTABLE SIGN OR BARRICADE PLACED OR STORED IN A PEDESTRIAN WALKWAY THAT COULD BE A HAZARD TO A VISUALLY-IMPAIRED PEDESTRIAN SHALL HAVE A DETECTABLE EDGE TO GUIDE THE PEDESTRIAN AROUND THE HAZARD. FOR ADDITIONAL GUIDANCE SEE THE "TEMPORARY PEDESTRIAN ACCESS ROUTE (TPAR) DEVICES" STANDARD PLAN, "DETECTABLE EDGE FOR SIGN ON PORTABLE STAND" DETAIL.

MINIMIZE DISRUPTION TO PEDESTRIANS TO THE MAXIMUM EXTENT FEASIBLE BY PROVIDING AN APR IN THE FOLLOWING ORDER OF PREFERENCE:

- 1. PROVIDE THE APR ON THE SAME SIDE OF THE ROADWAY AS THE DISRUPTED ROUTE UTILIZING BYPASSES.
- 2. WHERE IT IS NOT FEASIBLE TO PROVIDE A SAME-SIDE APR, PROVIDE AN APR DETOUR ON THE OTHER SIDE OF THE ROADWAY.
- 3. WHERE IT IS NOT FEASIBLE TO PROVIDE AN APR ON EITHER SIDE OF THE ROADWAY, PROVIDE AN APR DETOUR WITH TRAILBLAZING SIGNS.
- (1) TEMPORARY CURB RAMPS WITH DETECTABLE WARNINGS.
- (2) TEMPORARY PAVEMENT MARKINGS FOR CROSSWALKS MAY USE CROSSWALK BLOCKS, TWO TRANSVERSE LINES OR TWO STRIPS OF 18" PREFORMED MARKING MATERIAL TO FORM 36" WIDE CROSSWALK BLOCKS.
- (3) PROVIDE AN APPROVED AUDIBLE MESSAGE DEVICE OR TACTILE MESSAGE FOR PEDESTRIANS WITH VISUAL DISABILITIES.

- (4) FOR FULLY ACCESSIBLE WALKWAYS THROUGH WORKZONES, CONSIDER DISPLAYING THE INTERNATIONAL SYMBOL OF ACCESSIBILITY.
- (5) USE PEDESTRIAN DETOUR TRAILBLAZING SIGNS IF THE PEDESTRIAN DETOUR IS NOT LOCATED ACROSS THE ROADWAY FROM THE SIDEWALK CLOSURE.
- TYPICAL SIGN MESSAGE FOR AN ALTERNATE PEDESTRIAN ROUTE SHOULD INCLUDE INFORMATION SUCH AS THE DURATION OF THE WALKWAY RESTRICTIONS (BEGINNING AND/OR END DATES) AND A PROJECT CONTACT NUMBER FOR 24/7 QUESTIONS OR REPORTING HAZARDS. TYPICAL INFORMATION INCLUDED IN AN AUDIBLE MESSAGE CAN BE FOUND IN "TPAR - AUDIBLE MESSAGE CONTENT GUIDELINES" AVAILABLE ON THE MNDOT TRAFFIC ENGINEERING WEBSITE ON THE PEDESTRIAN ACCOMMODATIONS THROUGH WORK ZONES WEB PAGE. ADDITIONALLY, A SUMMARY OF THE MESSAGE CONTENT GUIDELINES CAN BE FOUND WITHIN THE PEDESTRIAN ACCOMMODATIONS THROUGH WORK ZONES DESIGN GUIDANCE DOCUMENT.
- (7) LOCATE STOP BAR 20'TO 50'BEFORE THE CROSSWALK, RESTRICT PARKING BETWEEN THE STOP BAR AND THE CROSSWALK, ON TWO-WAY ROADWAYS. RESTRICT PARKING BOTH BEFORE AND AFTER THE CROSSWALK FOR BOTH DIRECTIONS.
- CONSIDER LIGHTING AT MID-BLOCK CROSSINGS IN ORDER TO ILLUMINATE PEDESTRIANS, IF NOT ALREADY LIT.
- (9) CONSIDER THE ADDITION OF R1-6a SIGNS AS MOTORISTS ARE NOT EXPECTING MID-BLOCK CROSSING.

LEGEND

SIGN

WORK AREA

SIDEWALK BARRICADE

□ DIRECTION OF TRAFFICE



AUDIBLE MESSAGE DEVICE (AMD)

TEMPORARY CURB RAMP WITH DETECTABLE EDGES

PEOR CHOSSIVAL

R1-6a

SHEETS



STANDARD PLAN 5-297.811 2 OF 2 APPROVED: 03-18-2021 REVISED:

THOMAS STYRBICKI STATE DESIGN ENGINEER

ALTERNATE PEDESTRIAN ROUTE (APR) LAYOUTS

STATE PROJ. NO. (TH SHEET NO. OF

TPAR SHOULD BE KEPT FREE OF TRASH, SEDIMENT, AND DEBRIS.

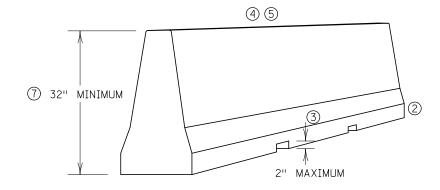
RAILINGS OR OTHER OBJECTS MAY PROTRUDE A MAXIMUM OF 4" INTO THE WALKWAY CLEAR SPACE WHEN LOCATED A MINIMUM OF 27" ABOVE THE WALKWAY SURFACE.

USE CRASHWORTHY TEMPORARY BARRIERS WHEN USED AS A PEDESTRIAN CHANNELIZERS.

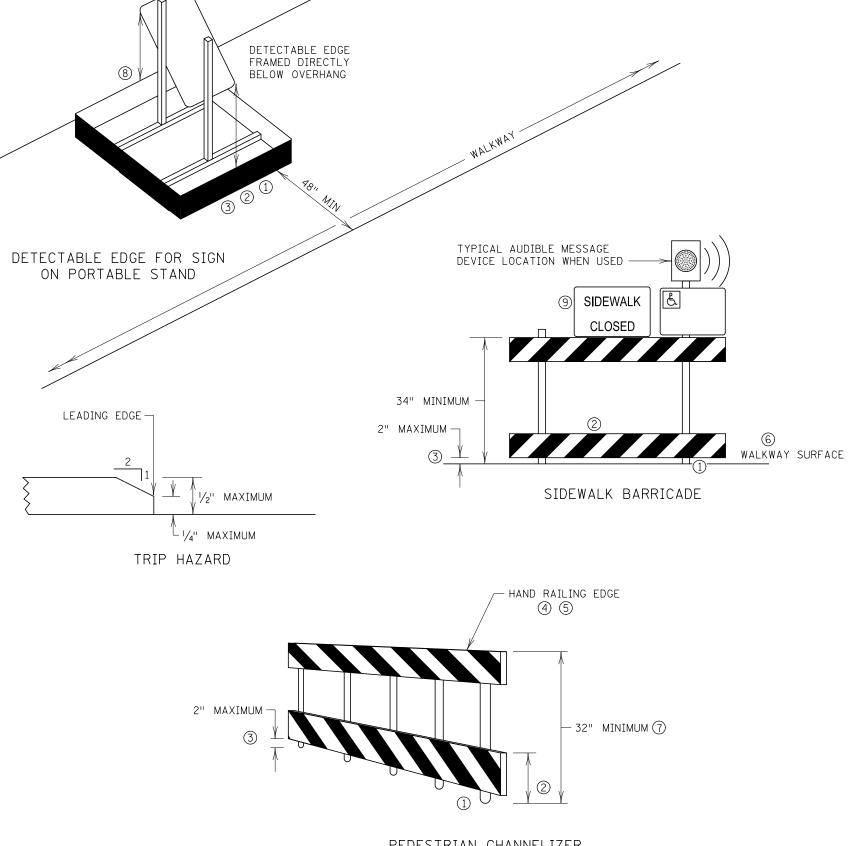
PLACE SIDEWALK BARRICADES ACROSS THE ENTIRE WIDTH OF THE WALKWAYSURFACE. WHEN USED.

USE INTERLOCKING DEVICES TO CHANNELIZE PEDESTRIAN FLOW TO PREVENT GAPS THAT COULD ALLOW PEDESTRIANS TO STRAY FROM THE CHANNELIZED PATH.

- PROVIDE DETECTABLE EDGE TO ANY TRIPPING HAZARD IN THE WALKWAY. LOCATE BALLAST BEHIND THE DETECTABLE EDGE OR INTEGRAL TO THE DEVICE, ANY SUPPORT ON THE FRONT OF THE DEVICE SHOULD NOT EXTEND INTO THE 48" MINIMUM WALKWAY CLEAR SPACE, ANY SUPPORT THAT EXTENDS INTO THE WALKWAY SHALL NOT EXCEED $\frac{1}{2}$ " HEIGHT ABOVE THE WALKWAY SURFACE; IF GREATER THAN $\frac{1}{4}$ ", BEVEL AS SHOWN IN THE TRIP HAZARD DETAIL.
- PROVIDE CONTINUOUS DETECTABLE EDGES EXTENDING AT LEAST 6" ABOVE THE WALKWAY SURFACE. MARK DETECTABLE EDGES WITH A COLOR THAT CONTRASTS WITH THE WALKWAY SURFACE, PLACE THE DETECTABLE EDGE AROUND ANY PORTABLE SIGN STAND IN THE WALKWAY AREA WHERE THE SIGN POSES A HAZARD TO A VISUALLY-IMPAIRED PEDESTRIAN.
- DEVICES AND DETECTABLE EDGES SHALL NOT BLOCK WATER DRAINAGE FROM THE WALKWAY. A GAP HEIGHT OR OPENING FROM THE WALKWAY SURFACE UP TO A MAXIMUM OF 2" IS ALLOWED FOR DRAINAGE PURPOSES.
- USE HAND AND GUIDE RAILS WHEN REQUIRED. INSTALL TOP RAIL OR TOP SURFACE IN A VERTICAL PLANE PERPENDICULAR TO THE WALKWAY, ABOVE THE DETECTABLE EDGE. PROVIDE CONTINUOUS RAIL AT A HEIGHT OF 34" TO 38" ABOVE THE WALKWAY SURFACE. USE RAIL SUPPORTS THAT MINIMIZE CONTACT WITH PEDESTRIAN'S HANDS AND FINGERS. SEE "PUBLIC RIGHTS OF WAY ACCESSIBILITY GUIDELINES (PROWAG) 2005" FOR ADDITIONAL GUIDANCE ON USE OF HAND AND GUIDE RAILS.
- USE DEVICES FREE OF SHARP OR ROUGH EDGES, AND USE ROUNDED FASTENERS (BOLTS) TO PREVENT HARM TO A PEDESTRIAN'S HANDS, ARMS, AND CLOTHING.
- REGARDLESS OF WEATHER CONDITIONS PROVIDE FIRM, STABLE, FREE-DRAINING, AND NON-SLIP TEMPORARY WALKWAY SURFACES, TEMPORARY WALKWAY SURFACES SHALL ALLOW NORMAL USAGE OF WHEELCHAIRS, WALKERS, STROLLERS, OR OTHER MOBILITY DEVICES. CONCRETE, BITUMINOUS, STEEL, RUBBER, WOOD (3/4" OR THICKER), AND PLASTIC ARE ACCEPTABLE SURFACE MATERIALS FOR A TEMPORARY WALKWAY SURFACE, GRAVEL, MILLINGS, AND OTHER UNEVEN SURFACES ARE NOT ACCEPTABLE SURFACE MATERIALS.
- (7) PROVIDE 32" HIGH OR GREATER LONGITUDINAL CHANNELIZING DEVICES FOR PEDESTRIANS.
- AN EDGE OF THE FRAMING MAY BE REMOVED IF IT IS NOT NEEDED FOR PEDESTRIAN GUIDANCE. STABILITY OF THE DETECTABLE EDGE SHOULD BE MAINTAINED.
- TYPICAL. SEE SIGNING PLAN FOR DETAILS.



PEDESTRIAN CHANNELIZER DEVICE USING A PORTABLE CONCRETE BARRIER



PEDESTRIAN CHANNELIZER

(TH



STANDARD PLAN 5-297.813 1 OF 2 APPROVED: 03-18-2021 REVISED: STATE PROJ. NO. THOMAS STYRBICKI STATE DESIGN ENGINEER

TEMPORARY PEDESTRIAN ACCESS **ROUTE (TPAR) DEVICES**

CHANNELIZERS, SIDEWALK BARRICADES, AND PORTABLE STANDS

RIAN SORENSON STATE TRAFFIC ENGINEER

APPROVED: 03-18-2021

REVISION:

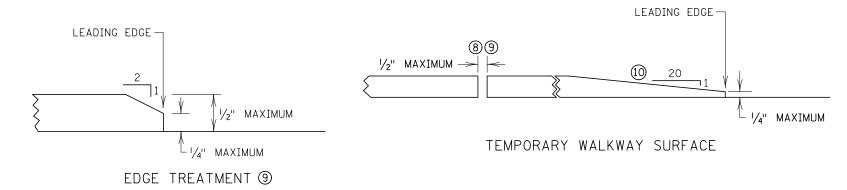
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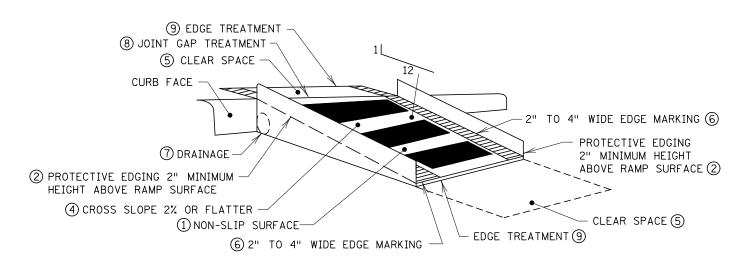
SHEETS

CONSTRUCT SLOPES AS INDICATED OR FLATTER, BUT NOT STEEPER.

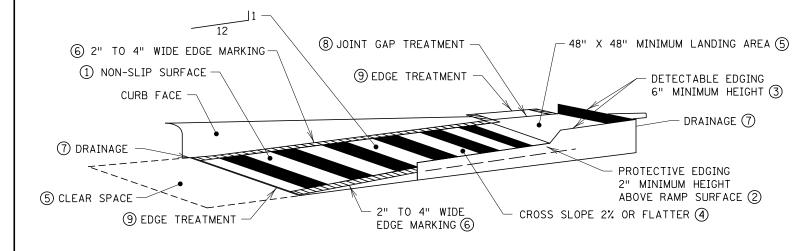
TPAR SHOULD BE KEPT FREE OF TRASH, SEDIMENT, AND DEBRIS,

- (1) CONSTRUCT CURB RAMPS AT LEAST 48" WIDE WITH A FIRM, STABLE, AND SLIP-RESISTANT SURFACE.
- 2 PLACE PROTECTIVE EDGING WITH A 2" MINIMUM HEIGHT WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1V:3H. CONSIDER PROTECTIVE EDGING WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- 3 PLACE DETECTABLE EDGING WITH 6" MINIMUM HEIGHT AND CONTRASTING COLOR ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION.
- ④ CONSTRUCT CURB RAMPS AND LANDINGS WITH A 2% OR FLATTER CROSS SLOPE.
- (5) PROVIDE A CLEAR SPACE OF AT LEAST 48" X 48" ABOVE AND BELOW THE CURB RAMP.
- (6) MARK THE CURB RAMP WALKWAY EDGE WITH A 2" TO 4" WIDE MARKING OF CONTRASTING COLOR. THE MARKING IS OPTIONAL WHERE COLOR-CONTRASTING EDGING IS USED.
- (7) DO NOT IMPEDE WATER FLOW IN THE GUTTER SYSTEM.
- (8) NO LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL EXCEED 1/2" WIDTH.
- (9) CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED $\frac{1}{2}$ ". USE VERTICAL LATERAL EDGES UP TO $\frac{1}{4}$ " HIGH, AND BEVELED AT 1V:2H FOR LATERAL EDGES BETWEEN $\frac{1}{4}$ " AND $\frac{1}{2}$ " HEIGHT.
- BEVEL THE EDGE OF TEMPORARY WALKWAY SURFACES 1/2" OR THINNER AT 1V:2H. FOR THICKER WALKWAY SURFACE BEVEL EDGE 1V:20H OR FLATTER.

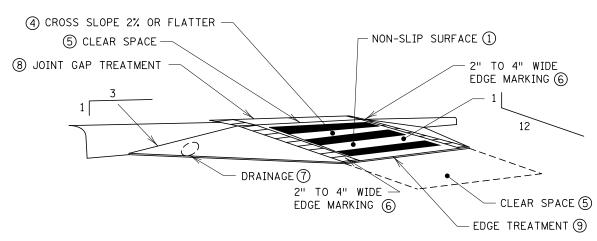




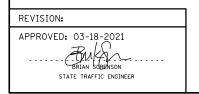
TEMPORARY CURB RAMP PERPENDICULAR TO CURB SHOWN WITH PROTECTIVE EDGE



TEMPORARY CURB RAMP
PARALLEL TO CURB



TEMPORARY CURB RAMP PERPENDICULAR TO CURB SHOWN WITH SIDE APRON





TEMPORARY PEDESTRIAN ACCESS
ROUTE (TPAR) DEVICES
TEMPORARY CURB RAMPS AND WALKWAY SURFACES

STATE PROJ. NO. (TH) SHEET NO. OF SHEETS