



(G.W.P. 6046-10-00)

**BAUDETTE/RAINY RIVER INTERNATIONAL BRIDGE  
REPLACEMENT ON HIGHWAY 11**

**TOWN OF RAINY RIVER**

CLASS ENVIRONMENTAL ASSESSMENT FOR PROVINCIAL  
TRANSPORTATION FACILITIES (2000)

GROUP 'B' PROJECT

**DESIGN AND CONSTRUCTION REPORT**

DECEMBER 2017

---

---



# DESIGN AND CONSTRUCTION REPORT

## Baudette/Rainy River International Bridge Replacement on Highway 11

### Town of Rainy River

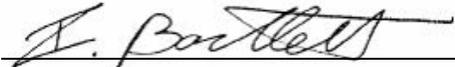
G.W.P. 6046-10-00

PREPARED BY:



---

Nevena Gazibara, B.Sc., MREM, ENV SP  
Environmental Planner  
Stantec Consulting Ltd.



---

Isaac Bartlett, P. Eng.  
Consultant Project Manager  
Stantec Consulting Ltd.

December 2017





**Ministry of Transportation**

Baudette/Rainy River International Bridge Replacement on Highway 11

G.W.P. 6046-10-00

**DESIGN AND CONSTRUCTION REPORT (DCR)**

This Design and Construction Report has been prepared under the Ontario Ministry of Transportation's *Class Environmental Assessment (EA) for Provincial Transportation Facilities, 2000*; a process that has been accepted and approved under Ontario's *Environmental Assessment Act*. This project was approved following the preparation and filing of a *Transportation Environmental Study Report (TESR)* in February 2017.

A CD copy of the DCR has been submitted to the Ontario Ministry of Environment and Climate Change to fulfill the requirements of the Ontario Ministry of Transportation *Class Environmental Assessment for Provincial Transportation Facilities (2000)*.

The Design and Construction Report is also available for review during regular business hours at the following locations:

Rainy River Public Library  
334 4th Street  
Rainy River ON  
Tuesday and Thursday:  
2 PM to 7 PM  
Wednesday, Friday and  
Saturday:  
11 AM to 4 PM

Town of Rainy River  
Clerk's Office  
201 Atwood Ave  
Rainy River ON  
Monday to Friday:  
9 AM to 4:30 PM

The report is being filed for information purposes and will be available for a 30-day review period from **December 6, 2017 to January 12, 2018**. Comments may be made to the MTO within this period. There is no provision during the review period for requests to the Minister of the Environment and Climate Change for a Part II Order requiring MTO to conduct an Individual Environmental Assessment under the Ontario *Environmental Assessment Act*, since this opportunity was provided during the TESR review period.

If you wish to obtain additional information about the project or provide comments please contact:

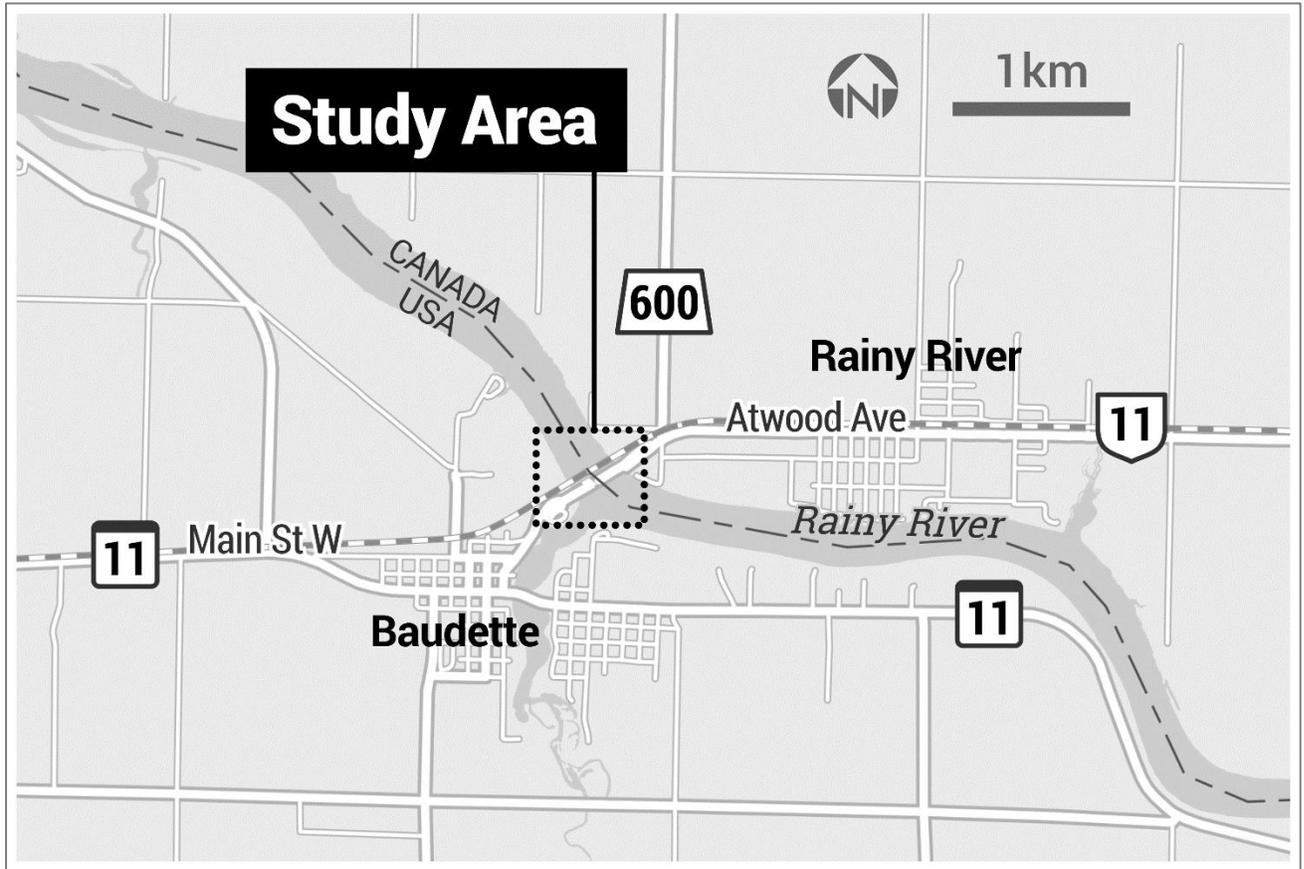
Isaac Bartlett, P. Eng.  
Consultant Project Manager  
Stantec Consulting Ltd.  
600-171 Queens Avenue  
London ON N6A 5J7  
Tel: 519-675-6643  
Call collect: 519-645-2007  
Fax: 519-645-6575  
Email: isaac.bartlett@stantec.com

Kevin Saunders  
MTO Senior Project Manager  
Ministry of Transportation, Northwestern Region  
615 James Street South  
Thunder Bay ON P7E 6P6  
Tel: 807-473-2109  
Toll free: 1-800-465-5034  
Fax: 807-473-2168  
Email: kevin.saunders@ontario.ca

Ce document hautement spécialisé n'est disponible qu'en anglais en vertu du règlement 671/92, qui en exempte l'application de la Loi sur les services en français. Pour de l'aide en français, veuillez communiquer avec le ministère des Transports, Bureau de services en français au : 905-704-2045 ou 905-701-2046.



# PROJECT KEY PLAN



Baudette/Rainy River International Bridge Replacement Study Area



# Table of Contents

<b>EXECUTIVE SUMMARY</b>	<b>1</b>
<b>1.0 OVERVIEW</b>	<b>5</b>
1.1 Project Background and Location	5
1.2 Purpose and Need	6
1.3 Site Context	6
1.4 Summary Description of the Undertaking	7
1.5 Environmental Assessment Background and Requirements	7
1.5.1 Ontario Environmental Assessment Act	7
1.5.2 Canadian Environmental Assessment Act	10
1.5.3 U.S. Environmental Assessment Process	10
1.6 U.S. and Canadian Coordination	11
<b>2.0 CONSULTATION PROCESS</b>	<b>13</b>
2.1 Government Agencies and Municipalities	13
2.2 Agency Meetings	14
2.2.1 Border Agencies	14
2.2.2 Fisheries and Oceans Canada	16
2.2.3 Ontario Ministry of Natural Resources and Forestry	16
2.3 Indigenous Consultation and Engagement	17
2.4 Public Engagement	17
2.4.1 Notice of Study Commencement	18
2.4.2 Public Meeting	18
2.4.3 Notice of Submission of the Design and Construction Report	19
<b>3.0 DETAILED DESCRIPTION OF THE RECOMMENDED DESIGN</b>	<b>21</b>
3.1 General Description of the Proposed Development	25
3.2 Drainage and Stormwater Management	25
3.3 Utilities	25
3.4 Construction Methodology and Staging	26
3.4.1 Bridge and Roadway Construction	26
3.4.2 Decommissioning of Existing Bridge	27
3.4.3 Construction Staging Areas on Land	27
3.4.4 Traffic Management – Highway	29
3.4.5 Traffic Management – Pedestrians and Cyclists	30
3.4.6 Boat Traffic Management – Navigation	30
<b>4.0 ENVIRONMENTAL ISSUES AND COMMITMENTS</b>	<b>31</b>
4.1 Natural Environment	31
4.1.1 Fisheries and Aquatic Resources	31

4.1.2	Terrestrial Resources	36
4.1.3	Groundwater and Surface Water Resources	39
4.1.4	Stormwater Management	39
4.1.5	Erosion and Sediment Control	40
4.2	Socio-Economic Environment	40
4.2.1	Land Use	40
4.2.2	Waste Management	41
4.2.3	Highway and Construction Noise	41
4.2.4	Construction Air Quality	42
4.2.5	Traffic Impacts	42
4.2.6	Navigation	42
4.2.7	Border Security and Access	42
4.2.8	Emergency Services	43
4.3	Cultural Environment	43
4.3.1	Built Heritage and Cultural Landscape Features	43
4.3.2	Archaeological Resources	43
4.4	Environmental Exemptions and Permits	43
4.4.1	United States and International Approvals	44
4.5	Monitoring	45
4.6	Summary of Environmental Effects, Proposed Mitigation and Commitments to Future Work	45
<b>FIGURES</b>		
Figure 1:	Study Area	6
Figure 2:	Proposed Improvements	23
<b>TABLES</b>		
Table 1:	External Agencies and Stakeholders Contacted	13
Table 2:	Conceptual Construction Staging	28
Table 3:	TESR Commitments Follow-Up	46
Table 4:	Summary of Environmental Effects, Proposed Mitigation and Commitments to Further Work	48
<b>APPENDICES</b>		
Appendix A:	Recommended Design	
Appendix B:	Construction Staging Plan	
Appendix C:	Notification	
Appendix D:	Selected Correspondence	
Appendix E:	Public Meeting Materials	

## Executive Summary

### GENERAL DESCRIPTION OF PROJECT

The purpose of this project is to replace the Baudette/Rainy River International Bridge on Highway 11 in the Town of Rainy River. The bridge provides access between the United States and Canada with 24-hour, full-service Port of Entry facilities in the City of Baudette, Minnesota and the Town of Rainy River, Ontario. The project will address the deteriorating condition of the existing bridge structure in order to maintain the international exchange of vehicular, freight and pedestrian traffic across the Rainy River at this location as follows:

- To provide a level of service that is consistent with the transportation needs of area residents, businesses/industries and farms
- To improve public safety by providing a safe roadway that conforms to the current safety and geometric standards
- To ensure the continued serviceability of the route

The Ontario Ministry of Transportation (MTO), in cooperation with the Minnesota Department of Transportation, has retained Stantec Consulting Ltd. and Parsons Corp. to undertake the Detail Design and Class Environmental Assessment (Class EA) Study for the replacement of the Baudette/ Rainy River International Bridge.

The Recommended Plan for the replacement of the Baudette/Rainy River International Bridge includes a new bridge on a new alignment approximately 1 m south and directly adjacent to the existing bridge. The Plan also includes the decommissioning of the existing bridge once the replacement bridge is complete. The new bridge will tie into the Canadian and U.S. Port of Entry facilities located on either end of the existing bridge.

Construction of the project is scheduled to commence in Spring 2018, subject to funding and approvals, and will occur over three construction seasons. There will be general winter shutdown periods between the 2018, 2019 and 2020 construction seasons.

The proposed construction staging and traffic management plan works to minimize disruptions to the existing Ports of Entry at the Canadian and United States customs plaza. Roadway traffic will continue to operate on the existing bridge during

construction of the replacement bridge through to its completion. Temporary closure of the pedestrian sidewalk on the existing bridge will be necessary to allow for safe construction as the new bridge is in close proximity to the existing bridge. Additionally, some brief traffic closures (maximum of 15 minutes at a time) may be needed at various stages of construction, with timing restrictions to be in accordance with the contract documents and border crossing requirements.

The Town of Rainy River does not have a Noise By-law in place. Construction will be limited to daytime hours as much as possible; however, night construction may be required. Specific equipment and operation constraints (e.g., drilling operations shall be limited to the minimum extent required during daytime hours) will be provided to the Contractor to adhere to during construction.

The consultation process for the Detail Design phase was initiated based on the list of contacts that were involved in the Preliminary Design and Environmental Assessment study. As the study progressed, contacts on the mailing list were removed, added and updated as relevant to the Detail Design study area.

The Detail Design Notice of Study Commencement was published on June 14, 2017 in the *Fort Francis Times* and the *Rainy River Westend Weekly* newspapers.

A Public Meeting was held on October 4, 2017 to provide information related to the Recommended Design, study updates, and Traffic and Construction Staging plans.

Information was presented using display boards, and project team members were on hand to answer any questions. Individuals were provided with an opportunity to fill out a comment sheet at the Public Meeting, or following the meeting by e-mail or fax to members of the project team.

The Notice of Submission has been published on November 29, 2017 in the *Fort Francis Times*, and the *Rainy River Westend Weekly* newspapers.

In accordance with the requirements of MTO's Class EA for a Group 'B' project; the purpose of the Design and Construction Report (DCR) is to summarize the following:

- The transportation engineering and environmental issues that were encountered during Preliminary Design, and how they were mitigated in the Detail Design phase
- A full description of the Detail Design consultation program

- Identification of all project approvals, licenses, and permits that have been, or must be obtained
- An update on environmental conditions and details on how environmental impacts will be mitigated
- A description of how the commitments made at the end of Preliminary Design, as documented in the Transportation Environmental Study Report (TESR) (February 2017), are being fulfilled
- Contract-specific measures and provisions to avoid or mitigate adverse impacts

This document addresses design-specific details and issues for the construction of the Baudette/ Rainy River International Bridge.

Coordination of the U.S. EA process and international and U.S. permits with Canadian permits occurred throughout Preliminary Design and continued throughout Detail Design. International, U.S. and Canadian permit applications and submissions were tracked by the project team and information was coordinated and verified for consistency in applications.

For environmental issues of broader concern, reference should be made to the TESR that was completed by Stantec in February 2017. The TESR is available on the project website at: <http://www.dot.state.mn.us/d2/projects/baudette-bridge/>

**This page intentionally left blank.**

## 1.0 Overview

### 1.1 PROJECT BACKGROUND AND LOCATION

The Baudette/Rainy River International Bridge spans the Rainy River from Baudette, Minnesota to Rainy River, Ontario. As part of a joint ownership agreement, the Minnesota Department of Transportation (MnDOT) and the Ontario Ministry of Transportation (MTO) maintain and operate the bridge. Minnesota Trunk Highway (TH) 72 and Highway 11 carry traffic over the bridge between the United States of America (U.S.) and Canada. Full service Port of Entry (Customs) facilities are located immediately after exiting on either end of the bridge and are operated by the Canada Border Services Agency (CBSA) and the U.S. Customs and Border Patrol (CBP).

The bridge carries approximately 1,300 vehicles per day and provides access between the U.S. and Canadian full-service, 24-hour Ports of Entry (POE) for the movement of international traffic and commerce. The bridge is a six-span truss structure with six steel beam approach spans that carry two lanes of traffic over the Rainy River. It is approximately 391 m long and has a 7.3 m two-lane roadway with an open steel grate deck. There is also a 1.8 m sidewalk cantilevered on the south side of the bridge.

The Ontario Ministry of Transportation (MTO), in cooperation with the Minnesota Department of Transportation, has retained Stantec Consulting Ltd. and Parsons Transportation Group to undertake the Detail Design and Class Environmental Assessment (Class EA) Study for the replacement of the Baudette/ Rainy River International Bridge. The study area is shown on Figure 1.

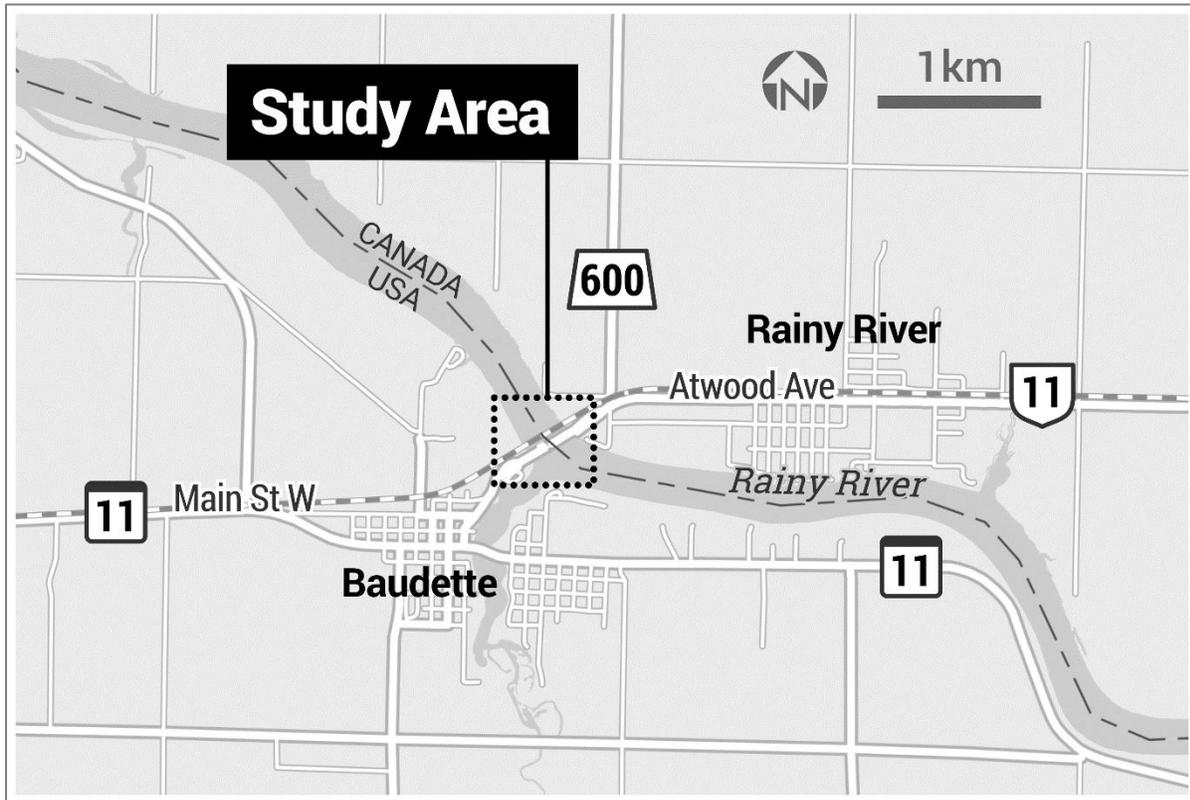


Figure 1: Study Area

## 1.2 PURPOSE AND NEED

The purpose of the Baudette/Rainy River International Bridge Replacement Project is to replace the existing bridge across the Rainy River to maintain access between the Town of Rainy River, Ontario and the City of Baudette, Minnesota. The project addresses the deteriorating condition of the existing bridge structure in order to maintain the international exchange of vehicular, freight and pedestrian traffic across the Rainy River at this location.

## 1.3 SITE CONTEXT

The Project is located in the Town of Rainy River, Ontario, Canada, and in the City of Baudette, Minnesota, U.S. The towns are located to the east and west, respectively, from the bridge. The Baudette Regional Airport is also located approximately 980 m northwest of the existing bridge. The existing City of Baudette wastewater facility is located approximately 550 m northwest of the existing bridge and directly south of the

Baudette Airport. The Town of Rainy River is located approximately one km east from the existing bridge.

## 1.4 SUMMARY DESCRIPTION OF THE UNDERTAKING

The new bridge will be a five-span structure with four in-water piers and two abutments on land. The new bridge will be located approximately one metre south of the existing bridge and will tie into the existing Port of Entry facilities in Canada and the U.S. The proposed replacement structure will have an approximate span arrangement of 67 m – 91 m – 91 m – 91 m – 67 m. The bridge will have no skew. The roadway width across the bridge is proposed to be 11.9 m with two 3.5 m lanes and two 2.4 m shoulders. Beyond the shoulder, the south side of the bridge also provides a 1.8 m sidewalk. The new bridge will tie into the existing U.S. and Canadian Port of Entry facilities and only minor changes are proposed to the U.S. facilities. No design changes or improvements are planned to the Canadian Port of Entry facilities as part of this Project.

The work proposed under G.W.P. 6046-10-00 includes the following:

- Construction of replacement bridge
- Construction of roadway approaches
- Connection of new approaches to U.S. and Canadian Port of Entry facilities
- Decommissioning of the existing bridge

## 1.5 ENVIRONMENTAL ASSESSMENT BACKGROUND AND REQUIREMENTS

### 1.5.1 ONTARIO ENVIRONMENTAL ASSESSMENT ACT

The *Ontario Environmental Assessment Act* (EAA) governs the conduct of Planning, Preliminary Design and Detail Design studies in Ontario. The purpose of the EAA is to make sure that:

- A reasonable and traceable planning process is followed
- The need for the project is demonstrated
- The public has had opportunities to provide input into the process and investigations
- The study includes a review of a range of alternatives
- The selected alternative minimizes any environmental impacts or provides mitigation strategies to minimize impacts resulting from the improvements

The Ontario Ministry of Transportation's Class Environmental Assessment (EA) was approved under the *Ontario Environmental Assessment Act* (EAA) in 1997, and was amended in 2000. This planning document defines groups of projects and activities, and the environmental assessment processes that the Ontario Ministry of Transportation (MTO) has committed to follow for these projects. Provided this process is followed, projects and activities included under the Class EA do not require formal review and approval under the *Ontario Environmental Assessment Act*.

This project falls within the scope of a 'Group B' undertaking, which includes major improvements to existing transportation facilities including highway improvements over land or water, that provide a significant increase in traffic capacity, or cause a significant widening of the "footprint" beyond the roadbed of an existing highway.

In compliance with the Class EA, a *Transportation Environmental Study Report* ("TESR") addressing the Baudette/Rainy River International Bridge Project (G.W.P. 6046-10-00) was prepared and filed in the Public Record for a 30-day public review in February 2017. The *Transportation Environmental Study Report* (TESR) prepared during Preliminary Design identified a Recommended Plan of a new replacement bridge to be located directly south (upstream) of the existing Baudette/Rainy River Bridge that will tie into the existing U.S. and Canadian Port of Entry.

No significant issues were raised and the project was approved to proceed to Detail Design in spring of 2017. The consultation program for Preliminary Design also included three Public Meetings which provided stakeholders and the public opportunities for input throughout the project.

This Detail Design study includes confirming the Recommended Design, confirming environmental protection and mitigation measures, developing construction staging and traffic management plans, and obtaining all applicable permits.

The Class EA also requires the preparation of additional documentation for the Detail Design phase of the undertaking in the form of a *Design and Construction Report* (DCR).

This DCR fulfills the Detail Design documentation requirements of the *Class Environmental Assessment for Provincial Transportation Facilities (2000)* for Group B projects.

### 1.5.1.1 Purpose of the Design and Construction Report

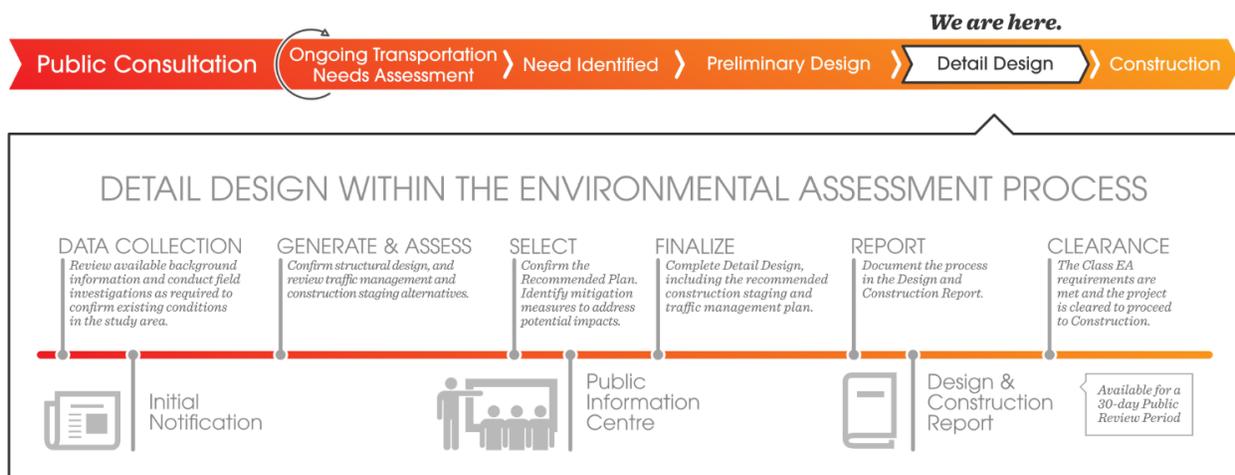
The purpose of the *Design and Construction Report* (DCR) is to document the following:

- The transportation engineering and environmental issues that were encountered during Preliminary Design, and how they were mitigated in the Detail Design phase
- A full description of the project's Detail Design consultation program
- Identification of all project approvals, licenses, and permits that have been, or must be obtained
- An update on environmental conditions and details on how environmental impacts will be mitigated
- A description of how the commitments made at the end of Preliminary Design, as documented in the TESR (February 2017) are being fulfilled
- Contract-specific measures and provisions to avoid or mitigate adverse impacts

This document addresses design-specific details and issues for the project. For environmental issues of broader concern, reference should be made to the TESR that was completed by Stantec in February 2017.

The 30-day public review period provides for public comment on the document. The DCR is not subject to a Part II Order (i.e. 'bump-up' opportunity to an Individual Environmental Assessment). Following the 30-day review, construction may commence in accordance with the description provided in this document, contingent upon the receipt of any other required approvals, permits and authorizations.

## THE CLASS ENVIRONMENTAL ASSESSMENT PROCESS for GROUP 'B' PROJECTS



### 1.5.2 Canadian Environmental Assessment Act

The Canadian *Environmental Assessment Act* (CEAA, 2012) is legislation that identifies which projects will be subject to federal environmental assessments (EAs). The *Canadian Environmental Assessment Act* (2012) applies to projects described in the Regulations Designating Physical Activities and to projects designated by the Minister of the Environment and Climate Change. The proposed bridge replacement is considered a 'new' bridge that crosses an international boundary and therefore is classified as a Designated Project under CEAA (2012). Section 28 of the *Regulations Designating Physical Activities* includes construction, operation, decommissioning and abandonment of a new international bridge.

Accordingly, the Project Description was submitted on March 3, 2017 to fulfill the initial requirement for a designated project to enable the Canadian Environmental Assessment Agency (CEA Agency) to conduct a screening to determine if the designated project requires an environmental assessment under CEAA 2012. The Project Description CEAA review process is a 45 – day period during which the CEA Agency reviews the document and makes it available for public review for 20 days. Following the 45-day review period the CEA Agency makes a determination as to whether a Federal EA is required for the project.

On April 27, 2017, following the 20-day public review, the CEA Agency determined that a Federal Environmental Assessment is not required for this project.

The CEA Agency remained on the mailing list through the Detail Design phase of this project and was provided with project updates throughout the phase.

### 1.5.3 U.S. Environmental Assessment Process

The Minnesota Department of Transportation's (MnDOT's) environmental assessment process has followed Minnesota's environmental review process set by the *National Environmental Policy Act* (NEPA), and the *Minnesota Environmental Policy Act* (MEPA), to fulfill requirements at both the state and federal level. A combined Environmental Assessment/Environmental Assessment Worksheet (EA/EAW) was prepared. At the federal level, the EA is used to provide sufficient environmental documentation to determine the need for an Environmental Impact Statement (EIS) or that a Finding of No Significant Impacts (FONSI) is appropriate. At the state level, the EAW is used to provide sufficient environmental documentation to determine the need for an EIS or that a Negative Declaration is appropriate. The Draft EA/EAW was distributed for public comment over a 45-day public review period from February 6, 2017 to March 24, 2017. Following the comment period, a FONSI was issued and signed in October 2017.

## 1.6 U.S. AND CANADIAN COORDINATION

The study process for this project, including the consultation program for Preliminary Design was developed with coordinated efforts from the Minnesota Department of Transportation (MnDOT) and the MTO to effectively coordinate the U.S. and Canadian study processes. A joint U.S. and Canadian Public Involvement Plan (PIP) was developed for the Preliminary Design stage that included details of planned Public Meetings, agency meetings, online resources including a project website and a Project Advisory Committee (PAC). The PIP also included detailed engagement plans for local, municipal, provincial, federal and international agencies and stakeholders throughout the duration of the Project through various consultation methods (i.e., correspondence, conference calls, and meetings). The Public Meetings during Preliminary Design were scheduled concurrently to ensure the same information was shared with the public and local stakeholders on both sides of the border at the same time.

The U.S. EA consultation requirements were completed during Preliminary Design. Following the completion of Preliminary Design and Environmental Assessment, the Ontario project team developed a consultation plan for the MTO Detail Design and EA process in order to fulfill Ontario requirements. The consultation plan included a Notice of Study Commencement, Notice of Public Meeting, and a Notice of the Design and Construction Report (DCR) public review period. Additional information on the Detail Design consultation program is discussed in Section 2.0.

**This page intentionally left blank.**

## 2.0 Consultation Process

The planning/preliminary design phase of the project involved an extensive consultation program that included a broad range of participants with diverse interests to assist in the planning and impact assessment for the project. Numerous opportunities for input from these stakeholders were provided as outlined in the *Transportation Environmental Study Report (TESR)*, 2017. Consultation opportunities during Preliminary Design included a Notice of Study Commencement, three Public Meetings, the TESR public review period and online project interfaces. A project website was established during Preliminary Design (<http://www.dot.state.mn.us/d2/projects/baudette-bridge/>) that was maintained during Detail Design and will continue to be maintained during construction of the project.

The subsequent sections describe the Detail Design consultation plan, and the results of the process to address government agency/municipal interests and public concerns. A copy of the notification materials is provided in Appendix C.

### 2.1 GOVERNMENT AGENCIES AND MUNICIPALITIES

The consultation process for the Detail Design phase was initiated based on the list of contacts that were involved in the broader Preliminary Design study, as outlined in Table 1. As the study progressed, contacts on the mailing list were removed, added and updated as relevant to the Detail Design study area.

**Table 1: External Agencies and Stakeholders Contacted**

<b>Federal Government</b>	
	Canadian Environmental Assessment Agency
	Fisheries and Oceans Canada
	Transport Canada
	Environment and Climate Change Canada
	Global Affairs Canada
	International Joint Commission
	Canada Border Services Agency
	Citizenship and Immigration Canada
	Canadian Transportation Agency
	Royal Canadian Mounted Police
	Canada Revenue Agency
	Canadian National Railway

<b>Provincial Government</b>	Ministry of Tourism, Culture and Sport Ministry of Natural Resources and Forestry Ministry of Northern Development and Mines Ministry of the Environment and Climate Change Ontario Provincial Police Member of Provincial Parliament – Kenora Rainy River
<b>Municipal Government</b>	Town of Rainy River Township of Dawson Rainy River Chamber of Commerce Rainy River District Social Services Administration Board (including EMS) Rainy River Health Centre
<b>Conservation Authorities</b>	International Rainy-Lake of the Woods Watershed Board
<b>Local Landowners, Occupants and Residents</b>	Residents of the Town of Rainy River and the City of Baudette, local businesses, and other stakeholders (Federation of Anglers and Hunters, Trans Canada Trails, Rainy River Future Development Corporation)

Correspondence was sent to each agency and stakeholder/interest group to advise them of the commencement of the Detail Design phase of the project, to invite them to preview sessions in conjunction with the Detail Design Public Meeting, and to allow them to express any concerns or identify issues that they may have with the undertaking. Key U.S. agencies were contacted by the U.S. Stantec staff as part of the U.S. design and EA and permitting requirements. U.S. consultation was documented in the U.S. EA which is available on the project website at: <http://www.dot.state.mn.us/d2/projects/baudette-bridge/>.

Commitments and concerns identified by external agencies during Preliminary Design and documented in the TESR were addressed during Detail Design. A summary of agency correspondence is provided in Appendix D.

## 2.2 AGENCY MEETINGS

Throughout the Detail Design phase, several meetings were held with key federal and provincial agencies to discuss the project, permitting requirements, and protection and mitigation measures.

### 2.2.1 Border Agencies

The U.S. POE facility is located approximately 292 m west from the centre of the bridge in the City of Baudette, Minnesota to the west and the Canadian Port of Entry facility is

approximately 300 m east from the centre of the bridge to the POE in the Town of Rainy River, Ontario. On the U.S. side of the Project, the lands located directly west of the bridge that house the U.S. POE facilities are owned by the General Services Administration (GSA) and operated by Customs and Border Protection (CBP) and the lands located directly south of the existing bridge are owned by the City of Baudette (Peace Park). The Canada Border Services Agency (CBSA) owns and operates the Port of Entry facility (POE) located on the east end of the bridge and owns lands located south of the Port of Entry building.

The CBSA, CBP, City of Baudette and the GSA were actively involved from the start of the Project in 2015. The CBSA and the CBP are key members of the Technical Advisory Committee (TAC) for the Project and participated in the monthly TAC meetings. At the TAC meetings, the CBP and CBSA were provided with project updates and were asked to provide input and information on the POE and security agency protocols and design requirements. The two agencies were also included on the Project Advisory Committee (PAC) and four meetings were held for the PAC. The CBSA and CBP were also included on the project mailing list and sent notices and correspondence for the project. At the conclusion of the TESR, future consultation was recommended with CBSA to:

- Confirm Recommended Plan during Detail Design
- Discussions and agreement regarding temporary use of CBSA land for staging areas
- Ongoing consultation regarding construction methods, access required

The project team held a meeting on May 9, 2017 with CBSA and CBP to provide an overview of the proposed bridge and approach reconstruction, and to review construction operations during the construction period. Items discussed included the need for future security cameras, fencing (temporary and permanent), daily communication with the contractor, vetting construction employees, and equipment monitoring. The CBSA and CBP were also in attendance at the Contractor Workshop to explain their security protocols and will also be in attendance at the future Contract Pre-Bid Meeting during tendering.

An additional meeting was held with CBSA and CBP on September 14, 2017 in Baudette, Minnesota. The purpose of the meeting was to provide a project and schedule update and discuss security during construction, laydown areas, and contractor communication requirements.

### 2.2.2 Fisheries and Oceans Canada

Fisheries and Oceans Canada has been actively consulted throughout Preliminary Design and Detail Design. A conference call was held with DFO and members of the Stantec project team including the Fisheries Specialist on August 15, 2017. A discussion was held regarding construction methodology, construction timing, jurisdictional authority and potential impacts to fish and fish habitat. Stantec provided an overview of the general fish habitat in the study area and noted that the study area only has general habitat (no specialized fish habitat) and the river is considered a migratory route for Lake Sturgeon.

The DFO noted that they would prefer that the in-water timing restrictions were the same on both sides of the border for the construction of the project but that they do not have any authority over activities that take place in the U.S. DFO also noted that extra consideration should be given for protection and mitigation measures for barge activity to prevent and avoid spills within the Rainy River. Stantec noted that they will review and where necessary, include appropriate protection measures during the development of contract specifications.

The DFO concluded that they do not foresee any issues or authorization requirements with the information provided to-date and that potential impacts to fish and fish habitat as a result of this project appear to be minor/have been minimized. The DFO noted that they would like to continue to be updated as the project continues and once construction details and the construction schedule is finalized. The DFO will continue to be consulted as the project proceeds into construction.

### 2.2.3 Ontario Ministry of Natural Resources and Forestry

The Ontario Ministry of Natural Resources and Forestry (MNRF) has been actively consulted throughout Preliminary Design and Detail Design. During Detail Design, a conference call was held with the Ontario Ministry of Natural Resources – Fort Frances District Office on September 22, 2017. The purpose of the conference call was to provide a project update and construction methodology information and confirm whether an *Endangered Species Act (ESA)* permit would be required. The MNRF noted that Lake Sturgeon (listed as Threatened in Ontario) migrate through the study area to spawning areas and the main concern is for timing to not disturb this migration to spawning areas. The MNRF noted that abiding by the in-water timing window is an appropriate protection and avoidance measure, as per the ESA. The MNRF provided a summary of the requirements of the ESA and the avoidance and harm and harassment of individuals (listed and protected species).

The MNRF noted that given the information provided during the call, an ESA permit is not required. Stantec will continue to update the MNRF as the project proceeds

forward and environmental specifications are confirmed (comparison and documentation of Minnesota and Ontario specifications).

A summary of selected correspondence with external agencies is provided in Appendix D.

### 2.3 INDIGENOUS CONSULTATION AND ENGAGEMENT

The following Indigenous (First Nations/Métis) communities were contacted by the MTO in advance of Detail Design study commencement and the Public Meeting to invite their input through correspondence with the study team:

Indigenous Communities	
Anishinabe of Wauzhushk Onigum	Nigigoonsiminikaaning First Nation
Anishnaabeg of Naongashiing	Northwest Angle No. 33
Big Grassy River First Nation	Animakee Wa Zhing No. 37
Couchiching First Nation	Ojibways of Onigaming First Nation
Grand Council Treaty No. 3	Pwi-Di-Goo-Zing-Ne-Yaa-Shing Advisory Services
Mitaanjigamiing First Nation	Rainy River First Nations
Naicatchewenin First Nation	MNO Sunset Country Métis Council
Métis Nation of Ontario – Métis Consultation Unit	Buffalo Point First Nation

Copies of the Public Meeting displays were provided directly on a CD and mailed to the above-noted Indigenous communities. Notification materials to the Indigenous communities listed above are provided in Appendix C.

### 2.4 PUBLIC ENGAGEMENT

Public participants included property owners, business owners, stakeholders/interest groups, and bridge users from within and outside the immediate project limits, the general public and elected representatives. These groups were provided with notification at study milestones and in advance of the Detail Design Public Meeting. A Canada Post unaddressed airmail for the entire community of Rainy River

(approximately 450 mailings) during study milestones. In addition, notices were posted on the project website.

#### **2.4.1 Notice of Study Commencement**

The Detail Design Notice of Study Commencement was published on June 14, 2017 in the *Fort Francis Times* and in the *Rainy River Westend Weekly* newspapers. The notice included the project limits, described the project including the Recommended Plan, and the Class EA process. The notice and notification letters were sent to external agencies, stakeholders, and property owners in the study area, as well as members of the public who expressed an interest in the study via regular mail and email. The notice was posted on the project website on June 8, 2017.

#### **2.4.2 Public Meeting**

A Notice of the Detail Design Public Meeting was published on September 27, 2017 in the *Fort Francis Times* and in the *Rainy River Westend Weekly* newspapers. The Notice of Public Meeting provided details of the Public Meeting including the date, venue, location, and purpose of the meeting. The notice also requested public involvement, and listed the contact information of Project Team personnel from whom additional information could be requested. The notice was posted on the project website on September 27, 2017.

Direct notification letters were sent to external agencies, stakeholders, and property owners in the study area, as well as members of the public who expressed an interest in the study via regular mail and email.

The Public Meeting was held on Wednesday, October 4, 2017 at the Rainy River Community Recreation Centre in the Town of Rainy River, Ontario. External agencies were invited to attend the Public Meeting from 3:00 PM to 4:00 PM, one hour in advance of the general public session from 4:00 PM to 7:00 PM.

Information materials presented at the Public Meeting were related to the overall scope of work for the project, as provided in Appendix E.

The Public Meeting forms part of the overall consultation plan for the project that was designed to involve stakeholders early and throughout the study to identify concerns and provide opportunities for input. In general, the purpose of the Public Meeting was to:

- Present the Recommended Plan
- Display and seek input on the construction staging and traffic management plan

- Present protection and mitigation measures
- Answer questions about the study

The following information was displayed at the Public Meeting:

- Welcome
- Introduction/Objectives
- Background
- Study Process
- Existing Conditions Map
- Investigations
- Existing Conditions, Mitigation, and Commitments
- Recommended Plan
- Construction – What to Expect during Construction/ Construction Phases
- Construction – Traffic Staging
- Thank You
- Historic Bridge Construction and Proposed Bridge Video (from visualizations model)

The text panels and displays were obtainable for review and the Project Team was present to answer questions and discuss the study. Comment sheets were provided, and attendees were encouraged to return them either in the comment sheet box at the meeting, or by mail, fax, or e-mail until the November 6, 2017 deadline.

Nine members of the general public attended the Public Meeting and eight agency representatives attended the Public Meeting.

An overview of comments received at, or following the Public Meeting and responses provided is outlined in Appendix D. All names and addresses from the comment sheets and visitor register were added or updated on the project mailing list.

### **2.4.3 Notice of Submission of the Design and Construction Report**

The Notice of Submission was published on November 29, 2017 at the time of filing of the *Design and Construction Report*. The notice includes a description of the study process, the proposed construction work, commencement and termination dates for the review process and MTO and consultant contacts for further information.

**This page intentionally left blank.**

## 3.0 Detailed Description of the Recommended Design

Detail Design develops a transportation plan to the design implementation level of detail. The Recommended Design is considered to be the most technically, environmentally, and economically suitable option for addressing the project objectives. This section of the report describes the Recommended Design for the Baudette/Rainy River International Bridge Project, as illustrated on the following Figure 2.

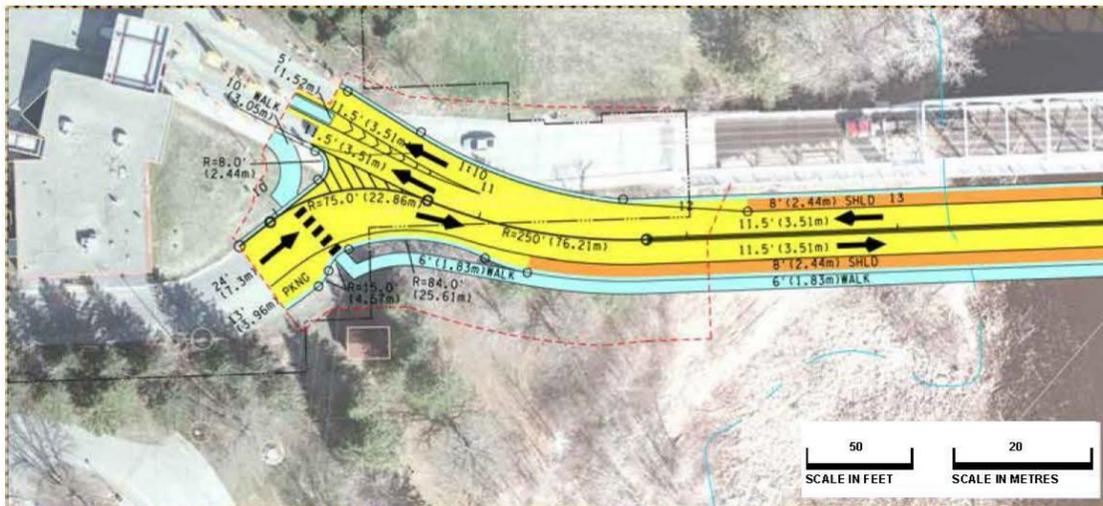
The current project schedule is for construction to occur over three construction seasons (i.e., 2018/2019/2020), subject to funding and approvals.

The Recommended Design is provided in Appendix A.

**This page intentionally left blank.**



 US PORT OF ENTRY TIE-IN



 CANADIAN PORT OF ENTRY TIE-IN

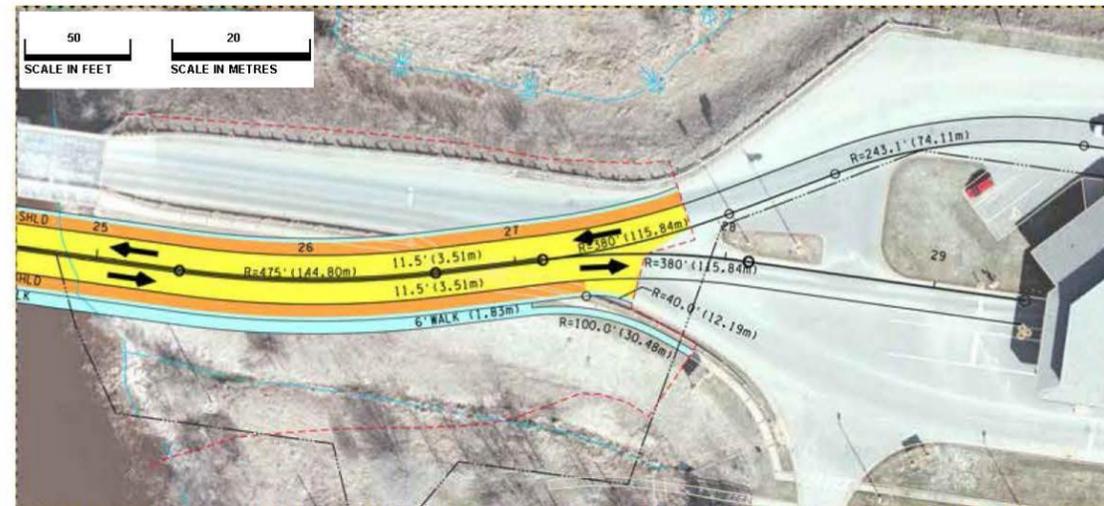


Figure 2: Proposed Improvements



**This page intentionally left blank.**

### 3.1 GENERAL DESCRIPTION OF THE PROPOSED DEVELOPMENT

The new bridge will be a five-span structure with four in-water piers and two abutments on land. The new bridge will be located approximately one m south of the existing bridge and will tie-into the existing Port of Entry facilities in Canada and the U.S., as shown on Figure 2.

The roadway width across the bridge is proposed to be 11.9 m with two 3.5 m lanes and two 2.4 m shoulders. Cyclists will be able to use the shoulders to cross the bridge. Beyond the shoulder, the south side of the bridge also provides a sidewalk. The bridge will have no skew. Minor changes are proposed to the U.S. facilities while no design changes or improvements are planned to the Canadian Port of Entry facilities as part of this Project.

### 3.2 DRAINAGE AND STORMWATER MANAGEMENT

The new bridge deck and bridge approach will add approximately 1.5 acres (0.61 hectares) of impervious surface to the study area. Stormwater runoff for the project will be captured at the bridge approach panels by catch basins and directed via storm sewer away from the roadway low points. Drainage system improvements on the Canadian side of the bridge will tie into the existing outlet for the parking lot at the Canadian border crossing facility. The preferred stormwater management approach identified for the bridge on the Canadian side includes maintaining the outlet to the north wetland and to the realigned ditch on the south. The proposed system includes stormwater management Best Management Practices (BMPs) designed to meet the project needs and permit requirements.

### 3.3 UTILITIES

Bell Canada owns and operates a fiber cable on the south side of the bridge on the Canadian side to the U.S./Canada border. The MTO owns, operates and maintains the lighting and conduit on the Canadian side of the bridge. MnDOT owns and maintains conduit and lighting on the U.S. side of the bridge.

Utility relocations will be required to accommodate the Recommended Plan in advance of construction. No interruptions to utilities are anticipated during construction.

Advance work will include:

- A temporary line will be installed prior to construction to resolve a conflict with the existing Bell line and the proposed bridge abutment. Upon completion of the new bridge, a new line will be run through the new bridge and connected.

- Hydro One will upgrade their existing plant and run a new line to a new power supply on the north side of the Port of Entry. Power will be maintained through this new power supply during construction. The existing power supply on the south side will be removed. This work will be done in advance of construction.

### 3.4 CONSTRUCTION METHODOLOGY AND STAGING

#### 3.4.1 Bridge and Roadway Construction

The new bridge is proposed have two 2.43 m diameter drilled shafts with permanent steel casing and precast cofferdams for each of the 4 river piers. The abutment piles will be steel H-Piles. The piers and abutments will be concrete. Work is planned to be accomplished with cranes placed on barges, and construction of temporary supports within the river that would be used to facilitate construction of the foundations, erection of the steel beams, and construction of the concrete bridge deck. Use of precast cofferdams will eliminate the need for a lower concrete seal and footing, which reduces the amount of below mudline excavation and below water construction substantially. Barge operations are expected to work progressively across the river at individual locations while temporarily anchored to the river bottom by spud piles or mooring lines.

Construction of the drilled shaft at Piers 2, 3 and 4 will generally consist of two barge mounted cranes and tender barges to install the shafts. Typical operation includes the installation of a 12.2 m long steel casing through the water and into the river bottom. The casing is then drilled out with the drilling continuing to a depth of approximately 1.5 m below the bottom of the casing. The casing will then be filled with concrete. Shaft construction at Pier 1 will be land based via access from the U.S. temporary causeway utilizing the same construction methods as the other in-water piers. Once the shafts are installed at a pier location, the concrete cap and pier construction will begin utilizing a separate crane and barge.

Once the substructures are completed, the steel beam spans will be erected utilizing a crane on a barge. Temporary shoring in the water to support the girder pieces during erection will be required at four locations. Typical temporary shoring is expected to consist of piles driven into the river bottom supporting a platform and support tower above the waterline. Temporary shoring will remain in place over the duration of the girder erection. Girders will then be erected on the piers by crane. Once the girders are erected the crane and barge operations will continue to support the construction of the concrete bridge deck.

A temporary causeway has been planned for on the U.S. embankment for the construction of the first pier and is included within the U.S. Environmental Assessment

Documents (EA). Access on the Canadian side of the river during construction would be provided by a temporary access road either south or north of the existing bridge. General and approximate construction staging areas include a staging area of approximately 1811 m<sup>2</sup>, a roadway area of approximately 548 m<sup>2</sup>, and a temporary dock area/causeway of approximately 102 m<sup>2</sup>. The construction staging areas will be determined and confirmed by the Contractor.

### 3.4.2 Decommissioning of Existing Bridge

The Contractor will be required to submit a Bridge Removal Plan to MnDOT and MTO for review and approval to ensure removal operations are in accordance with Contract and permit requirements.

The probable demolition technique will consist of deconstruction of the structure in the opposite method of original construction, utilizing temporary shoring to support the individual truss spans during deconstruction. Use of cutting torches, rope saws, cranes, and temporary supports would be permitted to allow the bridge deck and larger truss spans to be cut into smaller pieces and placed within barges. Containment of bridge debris will be required by use of containment mats on approach spans and positioning of barges spudded in beneath the removal areas within the river. Dropping the bridge superstructure or bridge components directly into the river will be prohibited. Removals are expected to be near the bottom of the cased foundation at each pier at a minimum of 0.6 m below the river bottom. Use of explosives for bridge removal operations will not be permitted. All temporary fill would be removed at the conclusion of the project and disposed of appropriately.

Demolition of the bridge may also require a temporary causeway and temporary access road on the north side of the existing bridge on the Canadian side, depending on the method of removal and access restrictions (see Appendix A). The details of these temporary structures will be confirmed by the contractor and documented in a Decommissioning Plan.

### 3.4.3 Construction Staging Areas on Land

The Project will likely also temporarily require additional land to facilitate construction. Two preliminary construction laydown and staging areas have been identified as part of the construction methodology assessment. These potential areas are located approximately 40 m southeast and approximately 52 m east of the existing bridge on the Canadian side of the crossing. Use of this land will be subject to discussions and an agreement between the contractor and CBSA.

At the conclusion of construction/decommissioning, all temporary staging areas/laydown areas will be removed and the areas will be returned to their previous states.

For more information and plans, drawings and specifications, please see Appendix A (Recommended Plan) and Appendix B (Construction Staging Plan).

For the purposes of construction staging and traffic management, construction has been broken into the following stages:

- Substructure- Construct piers and abutments, use barges and temporary causeway
- Superstructure- Construct bridge deck and superstructure using barges and temporary causeway
- Approaches- Construct bridge approaches concurrent with bridge construction
- Removal of existing bridge using temporary causeway and barges

Construction staging plans are shown in Appendix B. The anticipated construction schedule is outlined in Table 2. The final staging remains subject to change.

**Table 2: Conceptual Construction Staging**

<b>Preliminary Construction Staging</b>	
<b>BRIDGE CONSTRUCTION</b>	
<b>Activity</b>	<b>Season</b>
Substructure (Abutment)	Summer 2018
<b>Pier 1</b>	Summer-Fall 2018
Foundations (cofferdams & drilled shafts)	
Construction of Substructure	
<b>Pier 2</b>	Summer -Fall 2018
Foundations (drilled shafts)	
Construction of Substructure	

Activity	Season
Pier 3 (Canada)	Summer-Fall 2018
Foundations (drilled shafts)	
Substructure	
Pier 4 (Canada)	Fall 2018-Spring 2019
Foundations	
Substructure	
East Abutment	Summer-Fall 2018
Foundations	
Substructure	
Structural Steel / Aesthetic Arch	Summer 2018-Summer 2019
Falsework	Summer 2018-Summer 2019
Steel Erection	Spring 2019-Summer 2019
Superstructure Deck	Summer 2019-Fall 2019
Deck Concrete	
Finish Work (painting, aesthetic arches, railing)	
Civil Construction	
Canada (Roadway & tie-in)	Summer 2018-Fall 2019
USA (Roadway & tie-in)	Summer 2019-Fall 2019
Demolition	Spring 2020-Summer 2020

### 3.4.4 Traffic Management – Highway

During the construction of the new bridge, traffic will be maintained on the existing bridge. Once the new bridge is complete, there will be short duration lane closures in order to build the tie-ins to the U.S. and Canadian ports of entry. In addition, temporary road closures (duration and timing to follow CBSA and CBP requirements) will be allowed to complete the tie-in of the new bridge with the Port of Entry facilities and roadway. Once the tie-ins are complete then access will be provided on the new

bridge while the existing bridge is decommissioned. Additionally, some brief traffic closures may be needed at various stages of construction, with timing restrictions to be in accordance with border crossing requirements. Advanced signage will advise motorists of traffic delays and alternate routing will be utilized to reduce any inconvenience and impacts during construction. Emergency vehicle access through the work zone will be maintained throughout the duration of construction.

#### **3.4.5 Traffic Management – Pedestrians and Cyclists**

Closure of the pedestrian sidewalk on the existing bridge will be necessary for safety during construction as the new bridge is in close proximity to the existing bridge. Once the new bridge is complete, access and traffic will be open to pedestrians on the new bridge. Cyclists will be able to use the existing bridge until construction is complete and then will be able to use the shoulders on the new bridge to cross.

#### **3.4.6 Boat Traffic Management – Navigation**

The proposed construction staging will minimize overall navigation impacts to users of the Rainy River as it is an identified Navigable Waterway (List of Scheduled Waters) under the *Navigation Protection Act*.

Navigation will be maintained through all Project phases. There may be minor delays for boaters due to construction barge activity, however this will not result in significant effects (socio-economic or Indigenous activities). Temporary cofferdams and stabilization tower impacts will be minor, and those temporary works will be removed following the completion of project. Navigation aids (channel markers) will be provided during construction.

## 4.0 Environmental Issues and Commitments

This section focuses on the direct and indirect environmental effects associated with the design, construction and maintenance of the international bridge crossing. In general, effects to the natural, social and cultural environment were minimized during the study by following three principles:

1. Avoidance.
2. Identification of bridge and roadway design elements to minimize environmental impacts.
3. Development of site specific and generic environmental protection plan guidelines for consideration during and following construction.

The design, construction and operation/maintenance phases of this project involve construction activities for which potential environmental impacts are predictable and proven environmental protection measures are applied. All commitments made during the Preliminary Design Study have been addressed, as applicable to this project.

Details on how environmental impacts will be mitigated, either through the use of environmental design or through environmental constraints during construction are provided in the following sections.

### 4.1 NATURAL ENVIRONMENT

The existing environmental features described in the TESR, 2017 were taken into consideration during the impact assessment for this study. There are no provincially significant natural features (i.e., Provincially Significant Wetlands, Areas of Natural or Scientific Interest, or Provincial Parks) within the study area.

#### 4.1.1 Fisheries and Aquatic Resources

The Rainy River provides a migratory corridor to many resident fish species. Within the study area, nearshore areas provide potential rearing habitat for a variety of species and potential spawning habitat for Northern Pike (although Pike spawning habitat is abundant outside of the study area as well). In recent correspondence regarding habitat sensitivity, the MNRF (2016) indicated that it is unlikely that the study area provides spawning habitat for Walleye, White Sucker and Lake Sturgeon. While overwintering likely occurs in the study area, Lake of the Woods is 18 km downstream and likely provides more suitable overwintering habitat than the Rainy River and/or the

Baudette River. Within the study area, the Baudette River provides a diversity of habitats with shallow water, slower velocity, and abundant emergent vegetation to support a variety of fish species. The aquatic habitat identified in the study area is not classified as sensitive or specialized habitat.

#### 4.1.1.1 Impacts to Fish and Fish Habitat

Based on the bridge design and construction, potential impacts to fish and fish habitat and residual negative effects are minimal. Potential impacts were identified and are documented below. Overall, impacts to fish and fish habitat as a result of the project are considered minimal.

A Fish and Fish Habitat Preliminary Impact Assessment (Stantec 2016) was completed for the Preferred Plan following the 2013 *MTO/DFO/OMNR Protocol for the Protection of Fish Habitat on Provincial Undertakings* (the Protocol). The 2013 Protocol is a streamlined method of the regulatory review process for highway activities. The 2013 Protocol includes the DFO Self-Assessment process and DFO's Risk Management Framework (RMF). The RMF was used to determine the risk of the project to fish and fish habitat. Using the RMF, the impact assessment determined that the Project was Low Risk and therefore will not cause serious harm to fish. An updated Impact Assessment was completed in 2017 during Detail Design once construction methods were identified. The updated Impact Assessment determined that the Project was Low Risk. As per the Protocol and the RMF, DFO review and *Fisheries Act* Authorizations are not necessary for Low Risk projects; therefore, an Authorization is not anticipated for the project.

Background data and habitat assessments indicated that the Rainy River is a migratory corridor for Lake Sturgeon (Stantec 2016). The Lake Sturgeon population in Northwestern Ontario is Threatened and as such, the species and its habitats are protected by Ontario's *Endangered Species Act, 2007* (ESA 2007). The bridge design has considered Lake Sturgeon and their habitat, and the project will avoid negative effects to the species. Permitting under the ESA 2007 is not anticipated based upon design details and has been confirmed by the MNRF (see Appendix D).

Changes in habitat structure (disturbance to river bottom) are due to the physical footprint of the two new bridge piers in Ontario waters. Approximately 68 m<sup>2</sup> of natural substrate on the river bottom will be overprinted by the new bridge piers. Aquatic vegetation and bottom substrate may be disturbed during construction; however, the disturbance will be temporary. Other areas of the river bottom will be disturbed due to temporary work areas for construction (barges and temporary towers).

The footprint area of the bridge piers will occupy a relatively small area of the Rainy River. Although the area overprinted by the piers will not be available for use by fish, the areas around the piers will remain unchanged and the area currently occupied by

piers of the existing bridge will provide habitat once the existing bridge is removed. Temporary work areas and shoreline areas will be rehabilitated following construction.

Low Risk projects do not require DFO review or *Fisheries Act* authorization; therefore, a Fisheries Contracts Specialist is not required for construction monitoring.

Noise and vibrational impacts of construction activities were also considered for the project. The fish species of most concern are the Lake Sturgeon (species at risk) and Walleye as these species are both sensitive to noise and are valued in sport, commercial, and indigenous fisheries and Lake Sturgeon is a protected species. However, Lake Sturgeon and Walleye are only expected to be in the project area during the spring when they migrate to their respective spawning areas (i.e., not when in-water pile driving is expected to take place).

The negative effects to fish from the pile driving noise associated with this Project will be minimal. The pile driving will be restricted to the abutments (apart from minor driving required for temporary towers during construction of piers) which will be located at the shoreline on the east and west banks of the river, respectively. If impacts to fish are to occur from pile driving they will most likely be very close to the piles. Being primarily on or partially on land, much of the sound-energy generated from the pile strikes will be absorbed by the ground, which greatly diminishes the noise entering the water. Although neither Lake Sturgeon nor Walleye are expected to be in the Project area at the time of pile driving, fish can do unexpected things. Both of these species are photosensitive and most active at night. If they do happen to be near the Project area when pile driving is occurring, they will be in the deepest parts of the channel to avoid daylight, so relatively far away from the sound source.

General construction noises may startle fish and have them leave the Project area temporarily, but for the short duration that the Project will occur this will not have any serious or lasting consequences on the fishes in the river. On the contrary, the construction activities will likely function as a fish deterrent by keeping fish away from the immediate Project area and the potentially harmful sound source of the pile drivers. Further, by not using explosives for the demolition of the existing bridge, impacts from the Project overall are greatly reduced.

The negative effects to fish from pile driving activities is minimal for the following reasons:

- Pile driving activities are minimal and are occurring primarily on or near the shoreline where noise will be readily absorbed;
- Piles used are small in diameter and few in number relative to other projects where minimal impacts were observed;

- Water in the project area is shallow, shallow water attenuates low frequency energy quickly;
- The substrate of the riverbed is not hard, and will absorb rather than reflect sound waves;
- No explosives are being used;
- Noise from general construction activities (e.g. excavating abutment areas) are likely to scare fish away from the immediate area of pile driving, further minimizing risk;
- The band of weeds along each shoreline will further attenuate sound energy;
- Fish species of concern (Lake Sturgeon and Walleye) will not likely be in the Project area during time of construction;
- Impacts would likely be restricted to behavioural effects, which could be beneficial in terms of keeping fish away from the Project area, and;
- Although unlikely, if fish are in immediate vicinity of the pile driving and are impacted by the noise, any injuries would most likely be mild and not effect survival.

### Protection and Mitigation Measures

All fisheries impacts will be mitigated in accordance with the 2013 *MTO/DFO/OMNR Fisheries Protocol* or current *MTO/DFO/MNRF* fisheries regulations and appropriate U.S. and Minnesota fisheries protection regulations, as documented in the U.S. EA (on file with MnDOT and available on the project website). Protection and mitigation measures include in-water timing restrictions, erosion and sediment control measures, restoration of areas disturbed by construction, and appropriate management of excess materials.

The following mitigation measures described below are applicable to protect fish and fish habitat in the study area and are consistent with DFO's *Measures to Avoid Causing Harm to Fish and Fish Habitat*. The following measures will also protect Lake Sturgeon and their habitat.

### Design

Design considerations to minimize potential impacts to fish and fish habitat included:

- Reduced number of in-water piers, when compared to the existing bridge
- Construction method options consider temporary structures such as false towers required for construction

- The Recommended Plan alignment doesn't require a temporary modular bridge during construction
- Wherever possible, the footprint of in-water piers was minimized

## Construction

### **Timing of In-Water Work**

Works adjacent to aquatic resources that provide fish habitat, or have the potential to support fish habitat, are often restricted to certain periods to avoid construction-related impacts to fish species during their most sensitive / vulnerable life cycles (i.e., during reproduction and early development stages of off-spring). Therefore, construction activities are often not permitted close to, or within fish habitat during these periods.

The thermal regime of the Rainy River is 'coolwater' as per information provided by the MNRF; therefore, in-water construction for the project is permitted from July 16 to March 31 in Ontario. In Minnesota in-water work is allowed from June 16 to March 14. The timing window protects the sensitive life stages of resident fish species including Lake Sturgeon migration.

### **Flow Diversion/Dewatering**

As required, the following mitigation measures will be applied to isolate construction activities from the Rainy River during construction:

- Design and implement isolation plan to isolate temporary in-water work zones
- Fish rescue plan - Transfer fish in the work area away from the construction area using capture, handling, and release techniques to minimize harm and stress. Ensure the intakes of pumping hoses are equipped with an appropriate device to avoid entraining and impinging fish
- Manage water from dewatering operations to reduce erosion to the extent possible and reduce the risk of the release of sediment laden or contaminated water to the waterbody
- As a contingency, materials (e.g., pump, hoses, silt fencing, sand bags) shall be on site at all times in the event that water re-enters the work area once construction begins

### **Drilling/Pile Driving**

The following measures will reduce the risk of the effects of noise and vibrations on fish due to pile driving in the Rainy River during construction:

- In-water pile driving will occur during the in-water construction timing window for the project
- Reduce the overall duration of noise to the extent possible
- Construction will be limited to daytime hours as much as possible; however, night construction may be required. Specific equipment and operation constraints (e.g., drilling operations shall be limited to the minimum extent required during daytime hours) will be provided to the Contractor to adhere to during construction.

#### 4.1.2 Terrestrial Resources

Field investigations were undertaken during Preliminary Design by Stantec in August 2015 to confirm existing conditions as documented in the *Terrestrial Ecosystems Report* (Stantec, 2016). Given the ecological resources within the study area, low impacts to terrestrial ecosystems are anticipated from the Recommended Design.

A variety of environmental protection and mitigation measures will be adopted to guide the construction, operation, and decommissioning of the project. These include:

- Avoid sensitive areas such as wetlands, sensitive fish habitat, where possible. Where unavoidable, minimize the size and number of natural features that may be affected
- Regulated standards for air and water emissions, for storage and disposal of solid wastes, and for handling and disposal of hazardous materials
- Implement Best Management Practices for erosion and sediment control
- Minimize the footprint of Project facilities and activities to minimize the amount of disturbed land, wetlands, fish habitat and water resources

##### 4.1.2.1 Vegetation

The study area consists of mixed deciduous forest and coniferous forest. Surrounding lands include pasture and major river segments, including the Rainy River.

#### Wetlands

Mapped wetlands include wetlands identified by the MNRF, and Ecological Land Classification (ELC) wetland communities as determined through field investigations and air photo interpretation. Wetland communities identified in the study area during the terrestrial field investigations are located north of the existing bridge and directly south of the existing bridge along the shore of the Rainy River. There are no Provincially

Significant Wetlands (PSW) within the study area. Minimal and temporary construction impacts to wetlands are anticipated during construction.

Potential impacts to wetlands include:

- Direct alteration of wetland habitat through vegetation removal, grading, and other work associated with construction of the bridge
- Disturbance from vehicle use and construction activities and potential inadvertent releases of deleterious substances into the wetlands
- Alteration of wetland hydrology

Wetland Candidate Significant Wildlife Habitat (SWH) on the Canadian side is well-represented in the study area and adjacent lands, and negligible long-term implications to wildlife that use these areas are anticipated. Protection from indirect impacts to habitat such as sedimentation and erosion will be addressed through standard environmental protection measures discussed in the section above.

### Rare Vegetation

Wild licorice, a provincially rare plant (S3), was observed in the study area. Wild licorice was not delineated or mapped through the MNR's database and for the purposes of this project, have been identified using MNR's SWH technical guidance documents. This species was observed within the study area, south of the Canadian Port of Entry facility. No other rare or highly sensitive plant species were encountered during field surveys.

The Recommended Design and potential construction areas impact provincially rare species habitat for wild licorice. Woodland edges and other suitable habitat for wild licorice is well-represented in other parts of the study area, and negligible long-term implications to the local population are anticipated as result of the loss of 0.10 ha of Cultural Woodland where wild licorice was observed.

During construction adjacent to vegetated areas, heavy equipment could damage peripheral vegetation through contact, excavation and/or soil compaction. Where natural features occur adjacent to construction activities, barrier fencing should be erected to protect vegetation that is to be retained, including fencing to protect the retained portions of the Cultural Woodland. Areas to be cleared of existing vegetation should be clearly marked to prevent unnecessary clearing. Barrier fencing may coincide with sediment fencing used to control erosion and sediment transport at the site.

#### 4.1.2.2 Wildlife

##### Species of Conservation Concern

No terrestrial Species at Risk (SAR) have been observed or recorded in the area during field investigations or from background information. Impacts to any potential SAR or their habitats are not anticipated to result from the proposed improvements.

The entire Baudette/Rainy River International Bridge was searched by canoe during field investigations for the presence of Barn Swallow nests and other bird nests. No nests were observed on the bridge. A barn swallow was observed flying near the bridge. The configuration of the bridge is such that it is not expected to provide the necessary protection to attract birds such as Barn Swallows. The bridge is a metal grate structure that does not offer protection from the elements, and the piers do not provide sufficient overhang to protect a bird nesting underneath. Although nesting activity by Barn Swallow on the replacement structure during construction is highly unlikely, the Contractor should monitor the replacement bridge during construction for nesting activity. If Barn Swallow or other protected species initiate nesting on the structures prior to or during construction, construction must be suspended until the nest is no longer active.

Vegetation in the construction areas may support nests of migratory birds (as per the *Migratory Bird Convention Act – MBCA*). The Primary Nesting Period (PNP; the period when the percent of total nesting species is greater than 10%) for the study area is between April 21 and August 14. If vegetation clearing is scheduled to occur outside of the PNP, no mitigation is required.

In order to avoid contravention of the MBCA, vegetation clearing is not recommended between April 21 and August 14. If vegetation clearing is required during this period, an avian biologist should be retained to search suitable areas prior to work. The nest searches should identify nests of protected species and implement an appropriate buffer so that they are not disturbed. The work setback distance is species specific and must be determined by a qualified professional (e.g. ecologist/biologist).

Nest searches should be completed within 7 days of the proposed works. If work is not completed within 7 days following the nest search, the search should be repeated to make sure that no birds have established new nests during that period.

The MNRF also noted that milkweed plants are known to occur in the study area which are a food source for the Monarch butterfly. The Monarch butterfly is provincially ranked as S4 (apparently secure) and is designated as Special Concern federally and provincially. The Monarch in the study area had an abundance of Common Milkweed,

the larval host plant for this species. However, meadow habitat is not a limiting factor for Monarch in Ontario, and is not considered SWH. Suitable habitat for milkweed is well-represented in the study area and adjacent lands, and negligible long-term implications to milkweed populations are anticipated.

### **Wildlife Protection during Construction**

Wildlife habitat, including candidate SWH, will be protected during construction using measures that are proposed to reduce potential impacts on wetlands and other vegetation communities (described above). Assuming the above noted measures are implemented, negligible impacts to significant wildlife habitat are anticipated during construction. Generally, sediment and construction fencing will serve the dual purpose of preventing access of wildlife; however, there is some potential that individuals will enter the limits of construction. Contractors should be aware of the potential to encounter other wildlife and avoid them to the extent possible.

#### **4.1.3 Groundwater and Surface Water Resources**

The construction of the replacement bridge will involve the temporary dewatering of surface water from work areas to facilitate construction including cofferdams to isolate the piers and abutments during construction and provide a dry working environment. Dewatering operations will follow standards according to provincial regulations to prevent discharge of sediment laden water such as erosion and sediment controls to prevent scouring at discharge points and sediment release.

To protect water resources during construction, standard erosion and sedimentation measures and controls will be implemented to minimize potential suspended solids in runoff and other related environmental effects. These measures will include a requirement that waste oils, fuels, and hazardous wastes (if any) shall be handled in a safe manner. Contractors will be required to transport, store and handle all such substances as recommended by the suppliers/manufacturers, and in compliance with all applicable international, federal, provincial or municipal regulations.

There are two wells located within the vicinity of the work area on the Canadian side of Rainy River; however, these wells will not be affected during construction and no short and long term impacts are expected.

#### **4.1.4 Stormwater Management**

The stormwater management approach for the Canadian side of the new bridge includes capturing the runoff of the new bridge deck into catch basins at the bridge approach and carrying it through storm sewers to outlet down the new embankment. The existing ditch along the south side of the bridge will be realigned to match the new

bridge embankment, and includes rip rap and other erosion control measures. The existing outlet to the Rainy River will be generally maintained, though it will be located a few metres to the south.

The proposed system includes stormwater management Best Management Practices (BMPs) designed to meet the project needs and permit requirements.

#### **4.1.5 Erosion and Sediment Control**

Bridge construction may create instability in slopes and increase the rate of erosion along the highway from the removal of vegetation or other construction activities on steep slopes. These potential effects are expected to be minor in sections of the study area that are relatively flat. Any increased erosion will be temporary and limited to the construction period.

Mitigation measures for sedimentation, erosion, and dust control will be implemented during construction to prevent sediment and dust from entering sensitive areas, including the Rainy River.

The primary principles associated with sedimentation and erosion protection measures are to: (1) minimize the duration of soil exposure; (2) retain existing vegetation, where feasible; (3) encourage re-vegetation; (4) divert runoff away from exposed soils; (5) keep runoff velocities low; and (6) trap sediment as close to the source as possible.

Erosion and sediment control measures include: restricting equipment from entering sensitive natural areas beyond the construction site boundaries; stabilizing all stockpiled materials and exposed soil areas; application of erosion control blankets to disturbed sites; refuelling of equipment carried out away from sensitive natural features; and provisions for additional sediment fencing on-site as a contingency supply. Any natural areas that are temporarily disturbed for access or construction will be restored to natural self-sustaining conditions. Environmental controls will be monitored to ensure controls are functioning as intended and maintained accordingly.

Erosion and sediment control measures will be applied according to MTO standards and specifications or equivalent Minnesota Department of Transportation erosion and sediment control measures and standards.

## **4.2 SOCIO-ECONOMIC ENVIRONMENT**

### **4.2.1 Land Use**

Land use designations in the study area are not expected to change as a result of the Recommended Plan.

The Recommended Plan supports Regional and Provincial Growth Planning policies by providing the transportation infrastructure required to maintain a high level of service on the provincial highway system for the movement of people and goods.

It is not expected that residents in the study area will experience delay or disruption during construction. The existing bridge will be operational while the replacement bridge is being built.

#### 4.2.2 Waste Management

All regulated solid waste generated by construction of the proposed project will be handled in a safe manner and disposed of properly in a permitted, licensed solid waste facility or a similarly regulated facility elsewhere. Project demolition of concrete, asphalt and other potentially recyclable construction materials will be directed to the appropriate storage, crushing or renovation facility for recycling or reuse. All activities, including maintenance procedures, will be controlled to prevent the entry of petroleum products, debris rubble, concrete or other deleterious substances into the water.

Opportunities for reducing, reusing and recycling excess materials generated during construction will be considered.

#### 4.2.3 Highway and Construction Noise

Changes greater than 5 dB and absolute sound levels greater than 65 dBA are not predicted for residences within the study area. Therefore, noise mitigation is not required according to the *MTO Environmental Guide for Noise* requirements.

The Environmental Noise Assessment that was completed as part of the Preliminary Design recommended that a detailed construction noise evaluation be undertaken during Detail Design and documented in a separate report that also makes recommendations for a Code of Practice to minimize construction noise impacts. Following the development of a detailed construction methodology during Detail Design, it was determined that potential noise impacts would be temporary in nature and standard noise restrictions would be acceptable noise protection and mitigation measures.

The contractor will be required to adhere to standard noise restrictions (i.e., proper maintenance of equipment, no unnecessary idling). Standard dust suppressants (i.e., water, calcium chloride) will be used to minimize dust.

#### 4.2.4 Construction Air Quality

Dust and local air quality impacts to properties adjacent to the study area during construction will be minimized by General Conditions in the contract. These include: minimizing operation and idling of gas powered equipment and vehicles, minimizing vehicular traffic on exposed soils and stabilizing high traffic areas with suitable cover material, restoring disturbed areas as soon as possible to minimize the duration of soil exposure, and controlling dust emissions by the application of suitable MOECC approved dust suppressants.

#### 4.2.5 Traffic Impacts

Roadway traffic will continue to operate on the existing bridge during construction of the replacement bridge and access across the international border will be maintained. Some brief traffic closures (duration in accordance with border crossing agency requirements) may be needed at various stages of construction. The public will be advised in advance of any temporary road closures by means of traffic closure signs.

#### 4.2.6 Navigation

Navigational clearance will be maintained for boaters during construction. The proposed bridge will provide permanent navigational clearance and channels on both sides of the border.

Navigation will be maintained during construction and required clearances for watercraft will be provided. Signage or lighting required for canoeists or boaters on the watercourse during construction will be required, as per the U.S. Coast Guard lighting standards and the Canadian Coast Guard's *Canadian Aids to Navigation System* (2011).

#### 4.2.7 Border Security and Access

The Baudette/ Rainy River International Bridge Replacement project has been designed to maintain existing service at the border crossing during construction. The new bridge construction will occur alongside the existing structure, with each Port of Entry inspection facility operational for the duration of the construction phases. Security checkpoints for this project will remain unchanged. This will facilitate that no security breaches occur as commercial and recreational vehicles will continue to use the crossing.

The CBSA and CBP also have access requirements between buildings and the main port, and must meet standard access wait-times. Timing restrictions will be in place to minimize impacts when traffic volumes typically increase (i.e., lane closures not

permitted on holidays or after noon on Fridays and days preceding holidays). Temporary pavement markings will also be used to direct traffic through ports.

#### 4.2.8 Emergency Services

EMS providers in Rainy River cross the bridge in order to transport patients to the Baudette Airport to be air-lifted to the nearest hospital. Access will be maintained across the bridge with 24-hour service across the bridge and the international border. EMS providers will not be impacted by the project and they will be notified at least two weeks prior to the start of construction.

### 4.3 CULTURAL ENVIRONMENT

#### 4.3.1 Built Heritage and Cultural Landscape Features

The existing bridge is not listed or eligible for inclusion on the *Ontario Heritage Bridge List*. The bridge is eligible for listing in the U.S. National Register of Historic Places however, the U.S. parties with jurisdiction over this resource have agreed that adequate measures were taken to minimize harm to the resource to the extent possible.

#### 4.3.2 Archaeological Resources

An archaeological assessment was carried out to assess the potential impact of the proposed replacement of the bridge on archeological resources. No archaeological materials or resources were identified. If unexpected archaeological resources are identified during construction, appropriate agencies will be contacted and work suspended until further direction from authorities is provided.

### 4.4 ENVIRONMENTAL EXEMPTIONS AND PERMITS

The following Canadian environmental permits will be in effect during construction:

- Transport Canada International Bridges and Tunnels Act permit
- Transport Canada Navigation Protection Act permit
- Category 3 Permit to Take Water from the Ministry of the Environment and Climate Change -A draft Category 3 PTTW application has been submitted to the MOECC for review and approval. A final PTTW application will be submitted to the MOECC by the contractor prior to construction.

A separate *Navigation Protection Act* permit will be submitted to Transport Canada for the decommissioning of the existing bridge once the bridge removal plan has been developed and confirmed. No other provincial approvals are anticipated for the Project. No impacts to air quality or water quality are anticipated and therefore, permits

are not anticipated. The Town of Rainy River does not currently have a noise bylaw listed and therefore a noise bylaw permit for construction activities is not required.

#### 4.4.1 United States and International Approvals

In addition to the Canadian federal and provincial approval requirements, the following international and U.S. federal and state permits and approvals are required for this Project.

Permit	Approval Agency
<b>Section 4(f) de minimis determination</b>	Federal Highway Administration (FHWA)
<b>Endangered Species Act Section 7 Consultation</b>	U.S Fish and Wildlife Services (USFWS)
<b>Section 106 (Historic/Archeological) Approval</b>	State Historic Preservation Office (SHPO)/FHWA
<b>Section 404 Permit</b>	U.S Army Corps of Engineers (USACE)
<b>Section 9 Permit</b>	U.S. Coast Guard (USCG)
<b>Revocable License for Non-Federal Use of Federal Property</b>	General Services Administration (GSA)
<b>FHWA Environmental Assessment (FONSI)</b>	FHWA
<b>7460-1 Notice of Proposed Construction or Alteration</b>	Federal Aviation Administration (FAA)
<b>MnDOT/MnDNR Public Waters Work General Permit</b>	Minnesota Department of Natural Resources (MnDNR)
<b>Section 401 (Water Quality Certification)</b>	Minnesota Pollution Control Agency (MPCA)
<b>National Pollutant Discharge Elimination System – Construction Stormwater</b>	MPCA
<b>Notification to Manage Dredged Materials without a Permit</b>	MPCA

<b>Wetland Conservation Act Permit (WCA)</b>	Lake of the Woods County and/or MnDOT
<b>Works within 3 m (10 ft.) of International Boundary</b>	International Boundary Commission
<b>International Boundary Waters Treaty Act – International Joint Commission</b>	Global Affairs Canada and the U.S. State Department

Throughout Preliminary and Detail Design phases all international, U.S. and Canadian permits including the timing of permits and consistency of information provided in permits were coordinated by the U.S. and Canadian project team. Although the Design and Construction Report (DCR) is an Ontario-specific required document as part of the MTO Class EA process, all permit approvals are required for the project to proceed as a whole.

#### 4.5 MONITORING

Monitoring will be conducted by on-site construction supervisory staff to make sure that environmental protection measures, as outlined in this report and in the contract package, are being implemented and are effective.

In the event that the impacts of construction are different than anticipated, or that the method of construction is such that there are greater than anticipated impacts, the Contractor's method of operation will be modified to reduce those impacts. Any changes proposed by the Contractor should be thoroughly evaluated to make sure that the intent of the mitigation measures and provisions is maintained.

#### 4.6 SUMMARY OF ENVIRONMENTAL EFFECTS, PROPOSED MITIGATION AND COMMITMENTS TO FUTURE WORK

The proposed improvements can be completed without significant adverse effects to the natural, social and cultural environment in the study area. Potential impacts can be avoided or mitigated by the measures and provisions included in the construction contract.

Following the completion of the Preliminary Design phase of this Project, the following commitments (see Table 3) documented in the Transportation Environmental Study Report (TESR) were addressed:

Table 3: TESR Commitments Follow-Up

External Agency	Subject of Consultation
<b>International Joint Commission &amp; Global Affairs Canada</b>	<ul style="list-style-type: none"> <li>Confirmed that application for the <i>International Boundary Waters Treaty Act</i> and special agreement will not be required for the project</li> </ul>
<b>International Boundary Commission</b>	<ul style="list-style-type: none"> <li>Confirmed requirements of application for working near the border as per the <i>International Boundary Commission Act</i> and submitted application</li> </ul>
<b>Environment and Climate Change Canada</b>	<ul style="list-style-type: none"> <li>Confirmed stormwater management requirements with ECCC and U.S. agencies and applied requirements to Recommended Plan</li> </ul>
<b>Fisheries and Oceans Canada</b>	<ul style="list-style-type: none"> <li>Confirmed potential fisheries impacts and held meeting with DFO to confirm that a fisheries authorization would not be required</li> <li>Provided timing restrictions and other fisheries mitigation in contract package</li> </ul>
<b>Transport Canada</b>	<ul style="list-style-type: none"> <li>Confirmed requirements of <i>Navigation Protection Act</i> permit and approval and submitted application</li> <li><i>International Bridges and Tunnels Act</i> permit application submitted</li> </ul>
<b>Canadian Environmental Assessment Agency</b>	<ul style="list-style-type: none"> <li>CEAA determined that a federal Environmental Assessment is not required</li> </ul>
<b>Ministry of Natural Resources and Forestry</b>	<ul style="list-style-type: none"> <li>Confirmed that habitat for Species-at-Risk are not negatively impacted by the Recommended Plan</li> <li>Confirmed with the MNRF that an <i>Endangered Species Act</i> permit is not required</li> <li>Confirmed potential mitigation measures for natural features and confirmed Significant Wildlife Habitat</li> <li>Provided timing restrictions and other fisheries mitigation in contract package</li> </ul>
<b>Ministry of the Environment and Climate Change</b>	<ul style="list-style-type: none"> <li>Prepared a <i>Design and Construction Report</i> for each Detail Design contract to be placed on the public record</li> </ul>
<b>Town of Rainy River</b>	<ul style="list-style-type: none"> <li>Confirmed Recommended Plan during Detail Design</li> <li>Continued participation in advisory committee</li> </ul>
<b>Railways</b>	<ul style="list-style-type: none"> <li>Notified of start of Detail Design and construction phases</li> </ul>

External Agency	Subject of Consultation
<b>Snowmobile Clubs</b>	<ul style="list-style-type: none"> <li>Notified of start of Detail Design and construction phases</li> </ul>
<b>Canada Border Services Agency</b>	<ul style="list-style-type: none"> <li>Confirmed Recommended Plan during Detail Design</li> <li>Held meetings with the CBSA and CBP to discuss construction, security requirements, and access and traffic management</li> </ul>
<b>Emergency service agencies (i.e., OPP, Fire, ambulance, etc.)</b>	<ul style="list-style-type: none"> <li>Notified of start of Detail Design (i.e., staging etc.) to minimize impacts to emergency response times during and after construction</li> </ul>
<b>All other agencies/groups involved in planning and preliminary design study (i.e., Rainy River Chamber of Commerce)</b>	<ul style="list-style-type: none"> <li>Notified of start of Detail Design</li> </ul>
<b>Utility companies</b>	<ul style="list-style-type: none"> <li>Notified of start of Detail Design to confirm that potential conflict areas are properly identified and resolved</li> <li>Held meetings with utility companies to confirm conflicts and plan for future utilities on replacement bridge</li> </ul>

A summary of Detail Design environmental commitments is summarized in Table 4.

**Table 4: Summary of Environmental Effects, Proposed Mitigation and Commitments to Further Work**

**Legend**

MTO: Ontario Ministry of Transportation

MOECC: Ministry of the Environment and Climate Change

DFO: Fisheries and Oceans Canada

UTIL: Utilities

MNRF: Ministry of Natural Resources and Forestry

MTCS: Ministry of Tourism, Culture and Sport

EMS: Emergency Management Services

MCCC: Ministry of Environment and Climate Change Canada

CBSA: Canada Border Services Agency

I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
<b>1.0</b>	<b>Erosion and Sediment Control</b>			
1.1	Excavation and grading may result in erosion of exposed soils that may be carried to the watercourse during storm events	MTO MOECC MNRF DFO	1.1.1	<ul style="list-style-type: none"> <li>Erosion and sediment control measures will be implemented prior to and maintained throughout the construction phases</li> <li>Site access and staging will minimize disturbance to all watercourses and natural areas. Any material stockpiled on site will be kept a safe distance from any sensitive natural features and contained such that sediment does not enter the watercourse (either flowing or dewatered)</li> <li>Any areas which are disturbed and are not part of active construction operations shall be stabilized (with vegetation or erosion control matting)</li> <li>Materials removed or stockpiled during construction (e.g., excavated soil, backfill material) must be deposited, stored, and contained in a manner to ensure sediment does not enter a water body and will be appropriately stored</li> <li>Areas containing exposed soils or stockpiled materials will be isolated using appropriate sediment control devices to prevent the entry of sediment into the watercourse</li> </ul>

I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
<ul style="list-style-type: none"> <li>All activities, including maintenance procedures, will be controlled to prevent the entry of petroleum products, debris rubble, concrete or other deleterious substances into the water</li> <li>Vehicular refueling and maintenance will be conducted a minimum of 30 m away from any aquatic resource to avoid potential impacts in the event that an accidental spill occurs. If a gas pump is used for flow diversion and it is not possible to achieve the 30 m distance from aquatic resources, a containment system should be implemented to prevent accidental spills or leaks from entering the waterbody</li> <li>Trapped sediments and controls are to be removed only after the soils of the construction area have been stabilized and adequately re-vegetated</li> <li>Erosion and sediment control measures will be in accordance with MTO requirements and standards or equivalent MnDOT erosion and sediment control measures and standards</li> </ul>				
<b>2.0 Management of Waste and Excess Materials</b>				
2.1	Excess materials may be encountered during construction and require proper management/disposal	MTO MOECC	2.1.1	<ul style="list-style-type: none"> <li>All materials and debris will be removed upon completion of the work</li> <li>Excess materials generated during construction will be managed by the Contractor in accordance with standard MTO specifications</li> <li>Opportunities for reducing, reusing and recycling excess materials generated during construction will be considered</li> <li>Where an excess material is identified as a subject waste in form OPSF 180-4, Subject Waste Classification, or through Toxicity Characteristic Leaching Procedure (TCLP) analysis, the Contractor shall obtain a valid Generator Number issued by the Ministry of the Environment and Climate Change</li> </ul>
2.2	Improper handling and disposal of operating fluids from equipment and machinery may result in spills, which may impact the environment	MTO MOECC	2.2.1	<ul style="list-style-type: none"> <li>All spills will be immediately controlled and reported, and contact will be made with the MOECC Spills Action Centre. The Spills Action Centre Hotline number (1-800-268-6060) shall be posted at the site</li> </ul>

I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
2.3	Designated substances such as asbestos and lead may be present in the existing structure	MTO MOECC	2.3.1	<ul style="list-style-type: none"> <li>Designated Substance Surveys were undertaken for the structure. In accordance with the <i>Occupational Health and Safety Act</i>, R.S.O. 1990. 0.1, the Contractor is advised of the potential for designated substances to be encountered during the structure rehabilitation/replacements</li> </ul>
2.4	Waste and excess materials from the demolition of the structure may be harmful to the natural environment, groundwater and surface water, if not properly handled and disposed of	MTO MOECC	2.4.1	<ul style="list-style-type: none"> <li>Contract documentation includes general specification for management of excess material (i.e. disposal, re-use, stockpiling, etc.)</li> <li>Contractor to confirm staging areas</li> </ul>
2.5	Soil, groundwater and surface water that is contaminated through the release of chemicals may be classified as contaminants	MTO MNRF MOECC	2.5.1	<ul style="list-style-type: none"> <li>Inspection staff will be notified immediately if contaminated soils (soils that exhibit visual or olfactory evidence of petroleum or other contamination) are encountered. The contract will include standard wording to address contaminated soil should it be encountered during construction</li> <li>Any effluent or cleaning materials generated during the work program will be considered waste and will need to be managed as such</li> <li>They should be contained in appropriate storage containers and sampled prior to off-site disposal to determine the quality and the appropriate location for disposal</li> <li>Effluent and cleaning materials cannot be discharged over land or to the Rainy River</li> </ul>
2.6	Impacts to surface water quality and quantity. Secondary effects via impacts to groundwater and surface water quality in relation to watercourse and drainage ditches	MTO MOECC MNRF	2.6.1	<ul style="list-style-type: none"> <li>Where dewatering is required, effluent will be discharged to prevent entry of sediment into watercourses</li> <li>All surface water taking activities will be undertaken in accordance with the Permit to Take Water (PTTW)</li> </ul>

I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
<b>3.0</b>	<b>Vegetation / Migratory Birds</b>			
3.1	Potential for localized impacts due to disturbance of common vegetation within the Highway right-of-way	MTO MNRF	3.1.1	<ul style="list-style-type: none"> <li>Minimize encroachment and vegetation disturbance to that necessary for construction, including standard measures to protect adjacent vegetation and associated habitat</li> </ul>
3.2	Potential impacts to rare plant-wild licorice during land disturbance for construction	MTO MNRF	3.2.1	<ul style="list-style-type: none"> <li>Barrier fencing should be erected to protect vegetation that is to be retained, including fencing to protect the retained portions of the Cultural Woodland</li> <li>Barrier fencing may coincide with sediment fencing used to control erosion and sediment transport at the site</li> </ul>
3.3	Some tree/vegetation clearing required	MTO MNRF MCCC	3.3.1	<ul style="list-style-type: none"> <li>Areas to be cleared of existing vegetation should be clearly marked to prevent any unnecessary clearing</li> <li>Pre-clearing of common vegetation and trees should be undertaken in advance of April 21</li> <li>In the event that any additional trees or vegetated areas are identified for removal, by the Contractor, removals must occur outside of the April 21 to August 14 Migratory Bird timing window</li> </ul>
<b>4.0</b>	<b>Wildlife and Wildlife Habitat / SAR</b>			
4.1	Although no terrestrial Species at Risk (SAR) were recorded in study area during field observations there is habitat close to the study area	MTO MNRF	4.1.1	<ul style="list-style-type: none"> <li>Any species encountered during construction will be neither harassed nor harmed</li> <li>The animal will be permitted to leave the worksite on their own accord, and all removal activities will come to a halt until the animal has cleared the area</li> <li>The areas where tree clearing occurred should also be enclosed with a barrier to mitigate any potential for animals entering the worksite</li> <li>The structure was inspected for active Barn Swallow and migratory bird nests - none were found</li> </ul>

I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
				<ul style="list-style-type: none"> <li>Contractors should be aware of potential encounters with wildlife and avoid them to the extent possible</li> <li>Wildlife habitat, including candidate Significant Wildlife Habitat will be protected during construction using measures that are proposed to reduce potential impacts on wetlands and other vegetation communities</li> </ul>
<b>5.0</b>	<b>Fisheries</b>			
5.1	Construction of replacement bridge and decommissioning of existing bridge may temporarily impede fish passage and fish habitat	MTO MNRF DFO	5.1.1	<ul style="list-style-type: none"> <li>Low Risk Project Notification Form 1 will be maintained on file with MTO</li> <li>In-water construction and decommissioning operations in the Rainy River must be completed between July 16 and March 31<sup>st</sup> in Ontario</li> <li>Fisheries mitigation measures as outlined in the contract documents must be implemented during flow diversion/dewatering</li> <li>Environmental mitigation measures should be inspected during critical in-water construction periods to make sure that measures have been installed and are functioning properly for the protection of fish and fish habitat</li> </ul>
5.2	Potential impacts to fish and fish habitat during construction from dredging, pile driving, and structure removal	MTO MNRF DFO	5.2.1	<ul style="list-style-type: none"> <li>Where de-watering is required and fish habitat is present, apply DFO's <i>Freshwater Intake End-of-Pipe Fish Screen Guideline</i></li> <li>Any displaced fish will be captured and released outside of the work area prior to the start of construction</li> <li>Refuelling of equipment will be carried out away from any sensitive natural features to avoid potential impacts, in the event that an accidental spill occurs</li> <li>Materials and equipment used for site preparation and project completion shall be operated and stored in a manner that prevents any deleterious substance from entering the water</li> <li>In-water work areas will be isolated so that clean flow is maintained downstream/around the work area</li> </ul>
5.3	Potential vibration impacts to fish during construction (specifically pile driving)	MNRF DFO	5.3.1	<ul style="list-style-type: none"> <li>In-water construction and decommissioning operations in the Rainy River must be completed between July 16 and March 31<sup>st</sup> in Ontario</li> <li>Reduce the overall duration of noise to the extent possible</li> </ul>

I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
5.4	Dust and visible emissions may enter the watercourse during operations around water	MTO DFO MOECC MNRF	5.4.1	<ul style="list-style-type: none"> <li>Construction will be limited to daytime hours as much as possible; however, night construction may be required, and specific equipment and operation constraints (e.g., drilling operations shall be limited to the minimum extent required during daytime hours) will be provided to the Contractor to adhere to during construction. The timing corresponds to the period when fish are most active</li> <li>Design and implement isolation plan to isolate temporary in-water work zones</li> <li>Fish rescue plan - transfer fish in the work area away from the construction area using capture, handling, and release techniques to minimize harm and stress. Ensure the intakes of pumping hoses are equipped with an appropriate device to avoid entraining and impinging fish (see DFO's <i>Freshwater Intake End-of-Pipe Fish Screen Guideline</i> (1995))</li> <li>Manage water from dewatering operations to reduce erosion to the extent possible and reduce the risk of the release of sediment laden or contaminated water to the waterbody</li> <li>As a contingency, materials (e.g., pump, hoses, silt fencing, sand bags) shall be on site at all times in the event that water re-enters the work area once construction begins</li> </ul>
<b>6.0 Groundwater, Surface Water and Drainage</b>				
6.1	Uncontrolled runoff during construction could result in contamination of groundwater resources in the study area through construction and facility maintenance	MTO MOECC	6.1.1	<ul style="list-style-type: none"> <li>Groundwater protection will include the application of proper fuel or other containment spills management during construction, proper dewatering and sediment control, and proper facility management during operation and maintenance in order to avoid potential impacts to groundwater</li> </ul>

I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
6.2	Temporary and permanent construction impacts from clearing and grubbing, grading, road surfacing and bridge work can affect groundwater quality and quantity	MTO MOECC	6.2.1	<ul style="list-style-type: none"> <li>• Erosion and sediment control measures implemented as in 1.0 above</li> <li>• To minimize sediment transport, the following mitigation measures are proposed as deemed necessary:               <ul style="list-style-type: none"> <li>– All groundwater discharge water will be directed through a filter bag or straw bale/filter fabric device or equivalent to reduce suspended solids</li> <li>– The number and size of the sediment control bags or equivalent filter will be dependent on the extent and location of the required dewatering</li> </ul> </li> </ul>
6.3	The construction of the abutments requires groundwater dewatering	MTO MOECC	6.3.1	<ul style="list-style-type: none"> <li>• The groundwater dewatering program can be completed without causing any long-term impact to groundwater and surface water conditions</li> <li>• Water from dewatering and unwatering operations shall be directed to a sediment control measure and/or a vegetated discharge area 30 m away from waterbodies or as far away as practicable from the top of the bank of any waterbody, prior to discharge to the natural environment</li> <li>• Equipment and materials shall not be used or stored in vegetated discharge areas</li> <li>• The discharge of water to the natural environment shall not be directed across pavements, sidewalks, curb and gutter or similar hard surfaces except through appurtenances as specified in the Contract Documents.</li> <li>• Potential short-term impacts can be effectively mitigated with the contingency plans</li> <li>• Construction discharge water can be directed through a filter bag or straw bale/filter fabric device or equivalent to reduce suspended solids and discharge flow velocity. The number and size of the sediment control bags</li> </ul>

I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
				<p>or equivalent filter will be dependent on the extent and location of the required dewatering</p> <ul style="list-style-type: none"> <li>• Discharge may be directed to the ground surface provided there is no visible sheen or olfactory indicators of possible contamination, and onsite mitigation measures are taken</li> <li>• The Contractor will be responsible for determining the appropriate mitigation measures based on conditions encountered during dewatering</li> <li>• Category 3 Permit to Take Water has been submitted to MOECC – Contractor will be responsible for obtaining final PTTW</li> </ul>
6.4	The replacement bridge will result in additional impervious (paved) area (existing bridge was an open-grate deck)	MTO MOECC	6.4.1	<ul style="list-style-type: none"> <li>• Stormwater runoff for the project will be captured at the bridge approach panels by catch basins and directed via storm sewer away from the roadway low points</li> <li>• Drainage system improvements on the Canadian side of the bridge will be designed to tie in to the existing drainage system for the parking lot at the Canadian border crossing facility</li> </ul>
6.5	Construction of bridge piers will require dewatering of cofferdams	MTO MOECC	6.5.1	<ul style="list-style-type: none"> <li>• Surface water will be pumped out of water tight cofferdams surrounding the pier and abutment construction sites to generate a dry working environment</li> <li>• All surface water pumped out of the cofferdams shall be treated in the same manner as groundwater discharge. At no point shall surface water pumped out of the work area be discharged directly to a surface water feature without settling/filtration and energy dissipation measures at the discharge location to minimize the potential for scour and erosion, and for sediment transport to the Rainy River</li> </ul>
<b>7.0</b>	<b>Adjacent Land Use / Property</b>			
7.1	No private property required for project. Property and ROW transference between MTO and	MTO MNRF	7.1.1	<ul style="list-style-type: none"> <li>• No private property impacts identified during Detail Design</li> </ul>

I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
	MNRF is underway for the riverbed			
7.2	Potential changes to CBSA property as a result of the Recommended Design, including new lighting	MTO CBSA	7.2.1	<ul style="list-style-type: none"> <li>A formal agreement will be made between the MTO and CBSA regarding the maintenance of MTO-owned lighting on CBSA property</li> </ul>
<b>8.0</b>	<b>Construction Noise</b>			
8.1	Potential for change in noise level during construction to adversely affect noise sensitive receivers in the vicinity of the project	MTO MOECC PUBLIC Town of Rainy River	8.1.1	<ul style="list-style-type: none"> <li>The contractor will be required to adhere to standard noise restrictions (i.e., proper maintenance of equipment, no unnecessary idling)</li> <li>Contract limits designated as a Noise Sensitive Area requiring that equipment be maintained in an operating condition that prevents unnecessary noise, including but not limited to non-defective muffler systems, properly secured components, and the lubrication of moving parts</li> <li>Idling of equipment shall be restricted to the minimum necessary to perform the specified work</li> <li>Operation of equipment shall be limited to the minimum extent required during daytime hours due to proximity to residential dwellings/Noise Sensitive Receptors south of the existing bridge on the Canadian side</li> </ul>
<b>9.0</b>	<b>Air Quality</b>			
9.1	Construction activities associated with the project will have the potential to generate dust and increase total suspended particulates in the vicinity of the project	MTO MOECC	9.1.1	<ul style="list-style-type: none"> <li>Dust control measures for the construction phase will be undertaken in accordance with standard MTO procedure</li> <li>MOECC approved dust suppressants (i.e., water, calcium chloride) will be used to minimize dust</li> <li>The Contractor will take measures to ensure that no visible dust from his operations escapes to watercourses or beyond the right-of-way limits, or causes any reduction in visibility on the highway</li> <li>Visible dust will be considered to include airborne dust, and dust that reaches watercourses, ground surfaces and other surfaces as result of</li> </ul>

I.D. #	Issues/Concerns Potential Effects	Concerned Agencies	I.D. #	Mitigation/Protection/Monitoring
				concrete /construction operations, and the management of materials from these operations
<b>10.0</b>	<b>Archaeology</b>			
10.1	Previously unknown/deeply buried artifacts/remains could be uncovered during construction	MTO MTCS	10.1.1	<ul style="list-style-type: none"> <li>If the Contractor's operations expose any archaeological resources or human remains, work in the area must be suspended immediately and the MTCS will be contacted</li> </ul>
<b>11.0</b>	<b>Construction Staging / Traffic</b>			
11.1	Temporary disruption due to the short duration single-lane traffic staging	MTO PUBLIC EMS	11.1.1	<ul style="list-style-type: none"> <li>Access to residential and commercial properties within the study area will be maintained at all times during construction</li> <li>Access for emergency services will be maintained for the duration of construction</li> </ul>
11.2	No impacts to EMS response times anticipated	MTO EMS	11.2.1	<ul style="list-style-type: none"> <li>Advance notice of construction start, the construction schedule and any changes to traffic flow will be provided in writing two weeks in advance of construction to emergency service providers and the Town of Rainy River</li> </ul>
<b>12.0</b>	<b>Utilities</b>			
12.1	Although impacts to existing/relocated utilities are not anticipated there is always potential for conflicts during construction	MTO UTIL	12.1.1	<ul style="list-style-type: none"> <li>Contractor to confirm location of all existing utilities to avoid conflicts during construction</li> <li>Hydro One will install a service upgrade prior to start of construction which will feed the existing bridge lighting and provide power to the new bridge lighting</li> <li>Bell will install a temporary line prior to construction to avoid conflict. A new line will be installed post construction and before the removal of the existing bridge. The temporary line will be abandoned</li> <li>Provisions for working in the vicinity of utility plants included in contract</li> </ul>

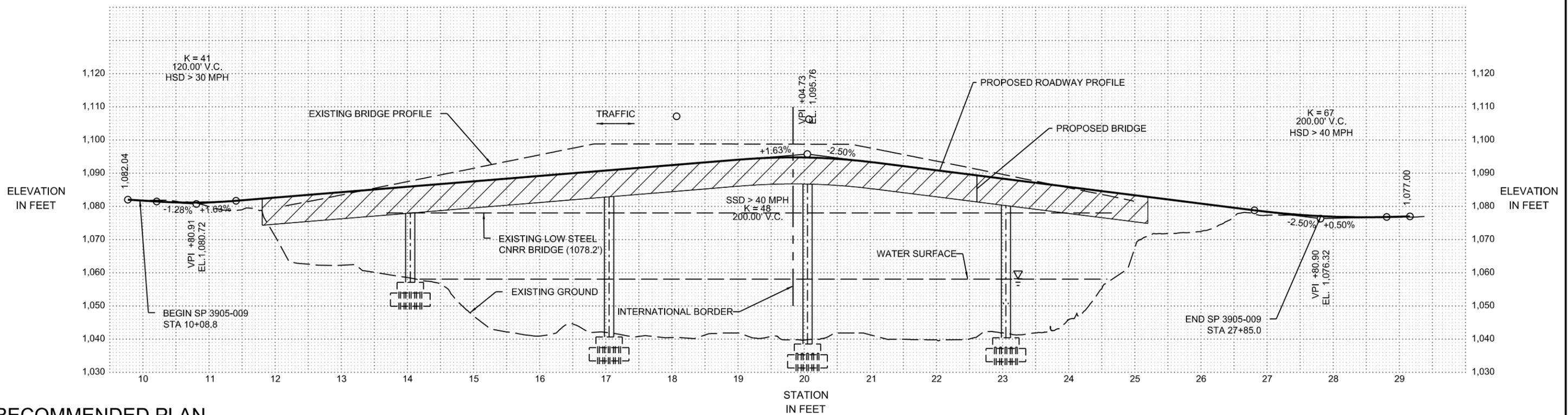
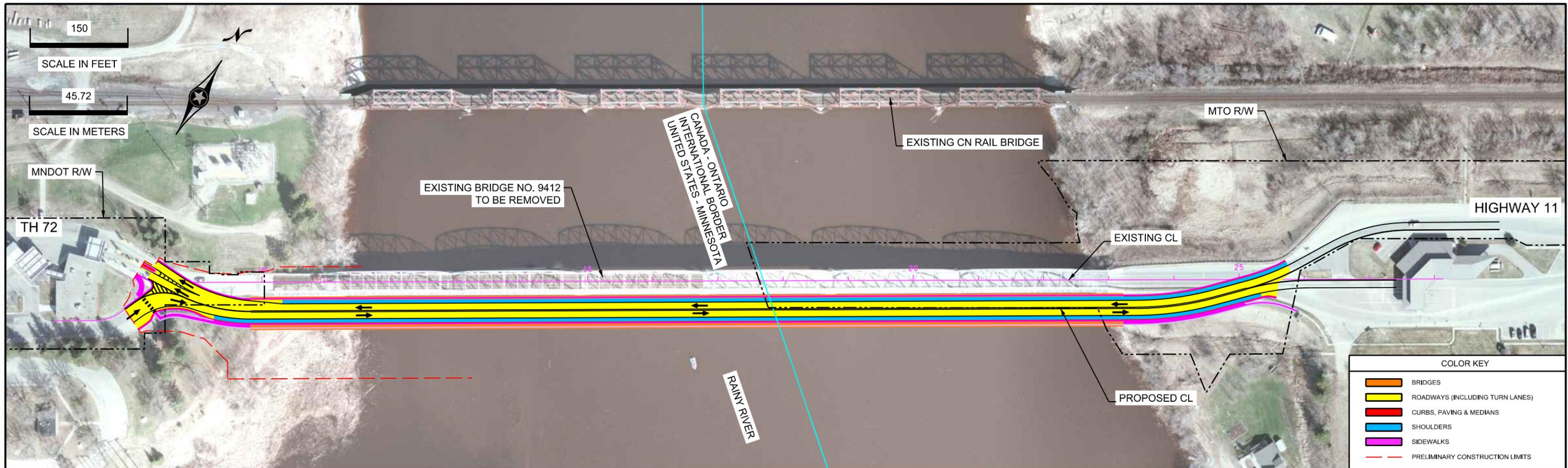


---

**APPENDIX A:  
RECOMMENDED DESIGN**

---





**RECOMMENDED PLAN**

MINNESOTA DEPARTMENT OF TRANSPORTATION AND MINISTRY OF TRANSPORTATION ONTARIO  
 BAUDETTE/RAINY RIVER INTERNATIONAL BRIDGE REPLACEMENT



View from Peace Park and Baudette River - looking north



View from sidewalk on bridge - looking northeast toward Canadian Port of Entry



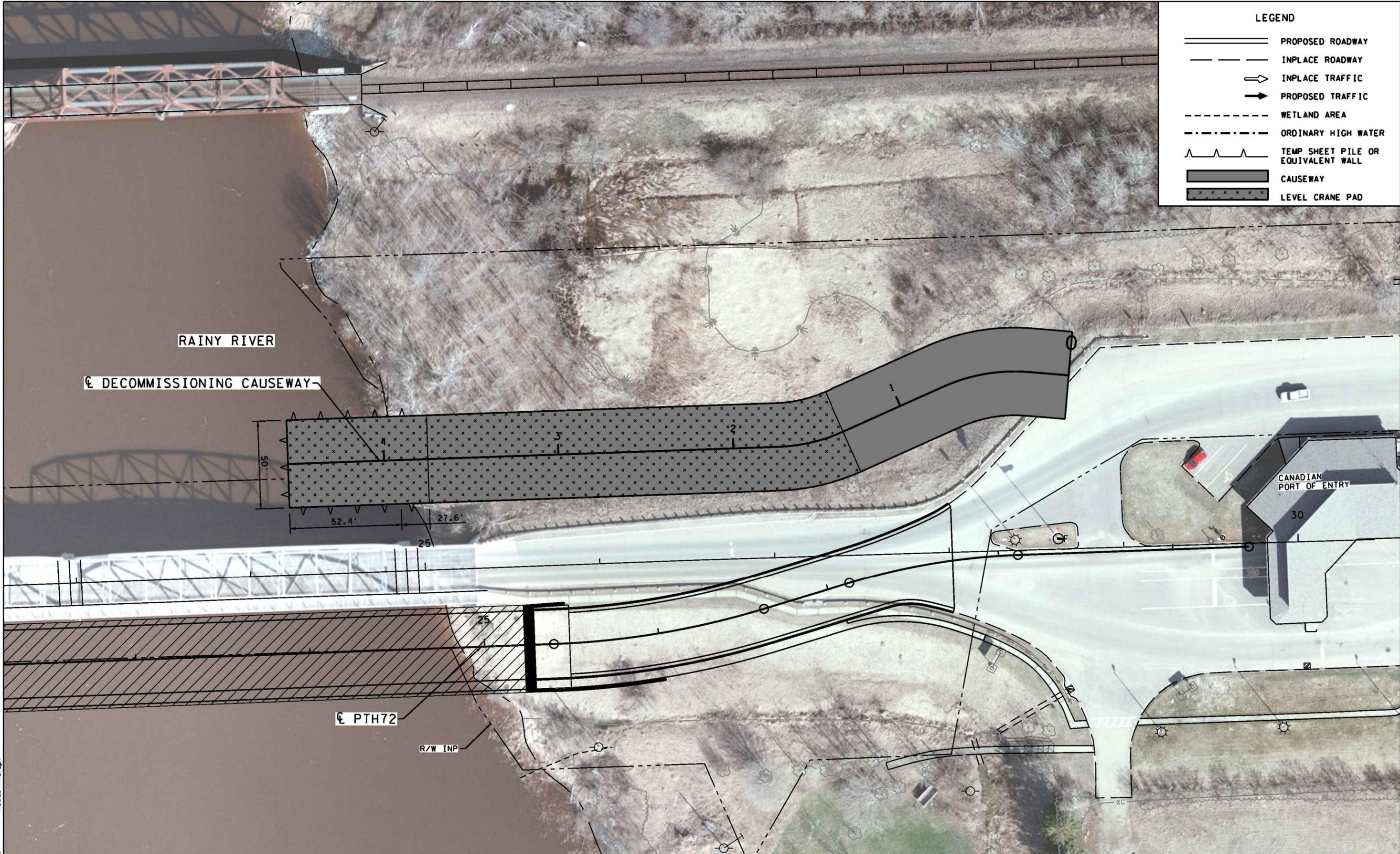
Birds-eye view of bridge showing both ports of entry



View of proposed arch at U.S./Canadian border from U.S. side



View of proposed arch at the U.S./Canadian border



**LEGEND**

	PROPOSED ROADWAY
	INPLACE ROADWAY
	INPLACE TRAFFIC
	PROPOSED TRAFFIC
	WETLAND AREA
	ORDINARY HIGH WATER
	TEMP SHEET PILE OR EQUIVALENT WALL
	CAUSEWAY
	LEVEL CRANE PAD

RAINY RIVER

☐ DECOMMISSIONING CAUSEWAY

50'  
52.4'  
27.6'

CANADIAN PORT OF ENTRY

30

☐ PTH72

R/W INP

MODEL NAME: cp1  
PLOT DATE: 9/1/2017

NO	DATE	APPR	REVISION

SURVEY	MINDOT
DRAWN	SNV
DESIGNED	RYF
APPROVED	RYF
DATE	9/1/2017
PROJ. NO.	193803921

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.  
PRINT NAME: R THOMAS FOLER  
SIGNATURE: **60% PLANS**  
DATE: 9/1/2017  
UC. NO. 26126



St. Paul Office  
2335 W. Highway 36  
St. Paul, MN 55113  
Phone: 651-636-4600  
Fax: 651-636-1311  
©STARTEC 2017

**ONTARIO DECOMMISSIONING CAUSEWAY OPTION**

MINNESOTA DEPT. OF TRANSP.  
T.H. 72  
SP 3905-009

SHEET



---

**APPENDIX B:  
CONSTRUCTION STAGING PLAN**

---



# CONSTRUCTION PHASES

2018



**SUBSTRUCTURE**  

- construct piers and abutments
- use barges and temporary causeway

2019



**APPROACHES**  

- construct bridge approaches concurrent with bridge construction. See traffic staging ▶

2020



**SUPERSTRUCTURE**  

- construct bridge deck and superstructure
- use barges and temporary causeway

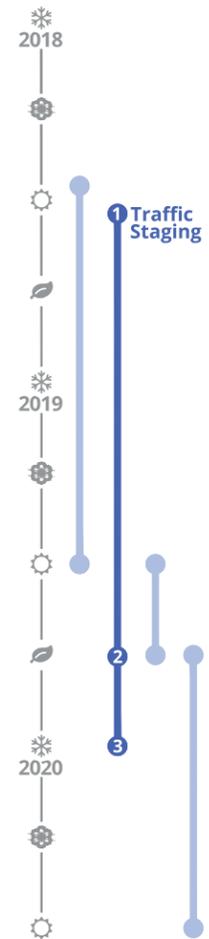


**REMOVAL**  

- remove old bridge
- use bridges and temporary causeway

# Traffic Staging

## CONSTRUCTION PHASES



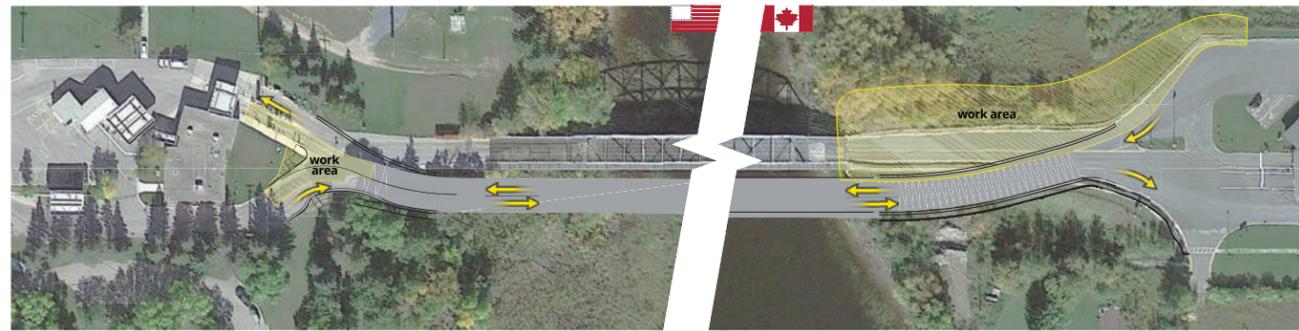
### Stage 1

**1**  
traffic on *old* bridge



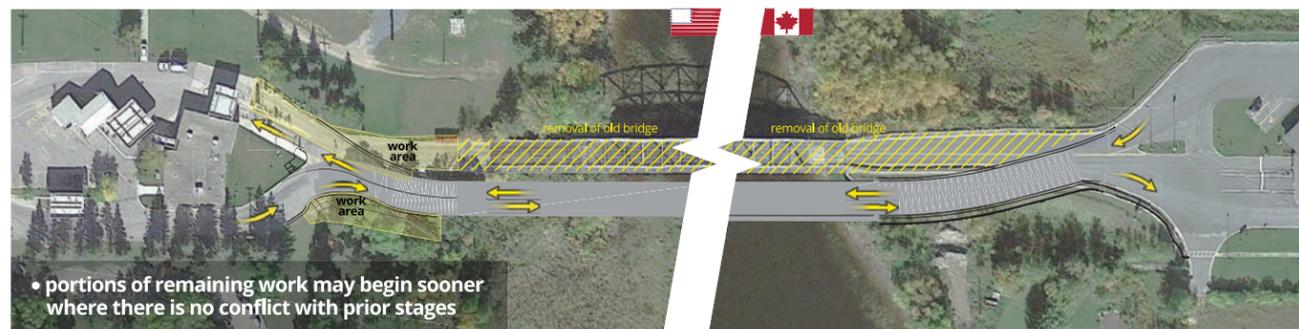
### Stage 2

**2**  
traffic on *new* bridge

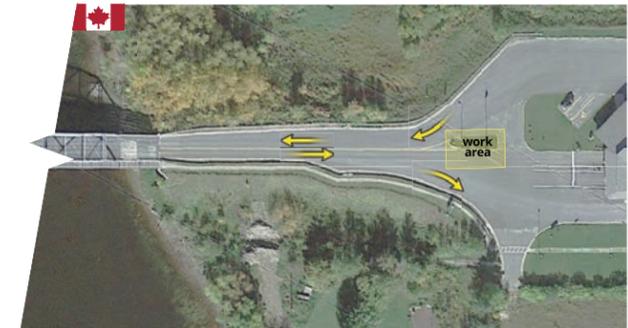


### Stage 3

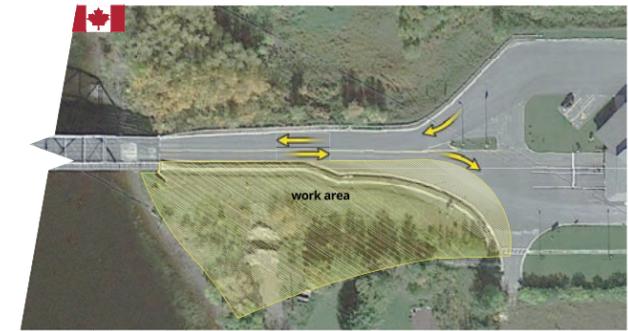
**3**  
remaining work



### Stage 1a



### Stage 1b



### Stage 1c



---

## **APPENDIX C: NOTIFICATIONS**

---



---

## **Notice of Study Commencement**

---



---

To: K. Saunders, S. Wall  
MTO Northwestern Region

From: Nevena Gazibara  
Stantec-Toronto Office

File: 193802870

Date: May 10, 2017

---

**Reference: Detail Design and Environmental Assessment - Baudette/Rainy River International Bridge on Highway 11, Town of Rainy River  
Notice of Study Commencement Timing Strategy**

Please find attached the Notice of Study Commencement timing strategy for the above-mentioned project for your records.

KEY STAGE	DATE
<b>MTO Final Advertisement Approval and Internal Notification</b>	<b>Monday, May 29, 2017</b>
Notice to MPP (sent by MTO)	Friday, June 2, 2017
External Agency Mailing (Letter, Flyer and Comment Sheet)	Thursday, June 8, 2017
Businesses, Property Owners and Stakeholder Mailing (Flyer)	Thursday, June 8, 2017
Mailing to First Nation and Aboriginal groups (on MTO Letterhead)	Thursday, June 8, 2017
Canada Post Mailing	Week of June 5, 2017
<b>Ontario Government Notice in:</b> <i>Rainy River Westend Weekly</i> (Published on Wednesdays)	Wednesday, June 14, 2017
<b>Ontario Government Notice in:</b> <i>Fort Frances Times</i> (Published on Wednesday)	Wednesday, June 14, 2017
<b>Project Website Updated with Notice of Commencement</b>	Thursday, June 8, 2017
External Agency Comments requested by:	Friday, July 28, 2017

**STANTEC CONSULTING LTD.**



May 10, 2017  
K. Saunders, S. Wall  
Page 2 of 2

**Reference: Detail Design and Environmental Assessment - Baudette/Rainy River International Bridge on  
Highway 11, Town of Rainy River  
Commencement Timing Strategy** **Notice of Study**

A handwritten signature in black ink, appearing to read "Nevena Gazibara".

Nevena Gazibara, B.Sc., MREM  
Environmental Planner  
Phone: 416-598-7663  
Fax: 416-596-6680  
nevena.gazibara@stantec.com

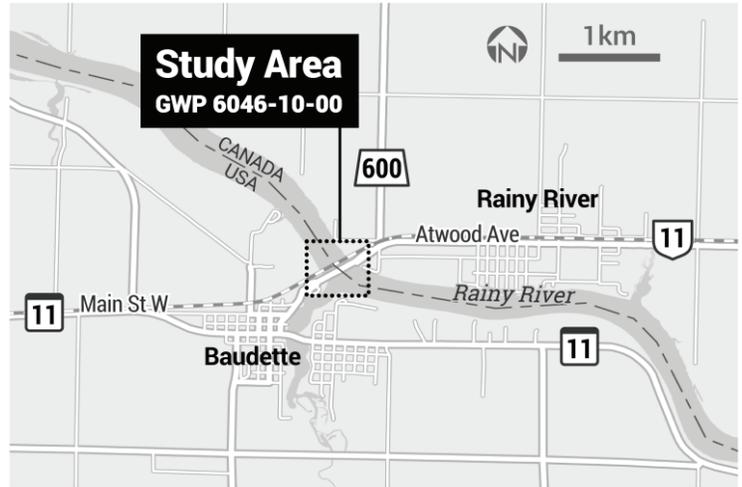
c. D. Grove, I. Bartlett – Stantec Consulting Ltd.  
J. McKinnon – Minnesota Department of Transportation

**THE STUDY**

The Ontario Ministry of Transportation (MTO), in cooperation with the Minnesota Department of Transportation, has retained Stantec Consulting Ltd. and Parsons Corp. to undertake the Detail Design and Class Environmental Assessment (Class EA) Study for the replacement of the Baudette/Rainy River International Bridge on Highway 11 in the Town of Rainy River. The bridge provides access between the United States and Canada with 24-hour, full-service Port of Entry facilities in the City of Baudette and the Town of Rainy River. The *Transportation Environmental Study Report* (TESR) prepared during Preliminary Design identified a Recommended Plan of a new replacement bridge to be located directly south (upstream) of the existing Baudette/Rainy River Bridge that will tie into the existing U.S. and Canadian Port of Entry facilities.

**THE PROCESS**

The study will follow the *Class Environmental Assessment* (Class EA) for *Provincial Transportation Facilities* (2000) process for a Group 'B' project. It includes confirmation of the Recommended Design, confirming environmental protection and mitigation measures, developing construction staging and traffic management plans, and obtaining all applicable permits. The TESR that documented the Recommended Plan was made available for a 30-day public review period during Preliminary Design in February 2017. No significant issues were raised and the project was approved to proceed to Detail Design. The consultation program for Preliminary Design also included three Public Meetings which provided stakeholders and the public opportunities for input throughout the project.



As part of the consultation process for Detail Design, one Public Meeting will be held. The Public Meeting is tentatively scheduled for Fall 2017 to provide an opportunity to review the details of the Recommended Design and to answer questions about the study.

Upon completion of Detail Design, a *Design and Construction Report (DCR)* will be prepared and made available for a 30-day public review period. The DCR will be prepared in accordance with the MTO *Class EA* requirements for a Group 'B' project and will document the design-specific details, applicable permits, and environmental commitments and mitigation measures to be carried forward to construction. Notices regarding the availability of the DCR and the times and location of the Public Meeting will be published in local newspapers and sent to persons on the mailing list.

**COMMENTS**

If you wish to comment on this project, have your name added to the project mailing list, or have any questions about this project, please contact one of the individuals identified below.

Mr. Isaac Bartlett, P.Eng.  
 Consultant Project Manager  
 Stantec Consulting Ltd.  
 600-171 Queens Avenue  
 London ON N6A 5J7  
 Tel.: (519) 675-6643  
 Call Collect: (519) 645-2007  
 Fax: (519) 645-6575  
 E-mail: Isaac.bartlett@stantec.com

Kevin Saunders  
 MTO Senior Project Manager  
 Ministry of Transportation  
 Northwestern Region  
 615 James Street South  
 Thunder Bay ON P7E 6P6  
 Tel: 807-473-2109  
 Toll free: 1-800-465-5034  
 Fax: 807-473-2168  
 Email: kevin.saunders@ontario.ca

You are encouraged to visit the project website, [www.dot.state.mn.us/d2/projects/baudette-bridge](http://www.dot.state.mn.us/d2/projects/baudette-bridge), to obtain current project information. If you have any accessibility requirements in order to participate in the project, please contact one of the Project Team members listed above. Comments and information are being collected to assist MTO in meeting the requirements of the *Ontario Environmental Assessment Act*. Information will be collected in accordance with the *Freedom of Information and Protection of Privacy Act*. All comments will be maintained on file for use during the study and, with the exception of personal information, may be included in study documentation and become part of the public record.

**From:** [Wall, Steven \(MTO\)](#)  
**To:** [Cooper, Doug \(MTO\)](#); [Coulter, Ken \(MNDM\)](#); [Galloway, Iain \(MTO\)](#); [Taylor, John P. \(MTO\)](#); [Piscopo, Annemarie \(MTO\)](#); [Kaszuba, Linda \(MTO\)](#); [Weiss, Gary \(MTO\)](#); [McNabb, Marvin \(MTO\)](#); [scmpp@ndp.on.ca](mailto:scmpp@ndp.on.ca)  
**Cc:** [Saunders, Kevin \(MTO\)](#); [DeCal, Patricia \(MTO\)](#); [Cooke, Gregg](#); [Gazibara, Nevena](#); [Furioso, Karen \(MTO\)](#)  
**Subject:** G.W.P. 6046-10-00, Rainy River/Baudette International Bridge, Hwy 11 - Ontario Government Notice - Study Commencement  
**Date:** Friday, June 02, 2017 2:26:19 PM  
**Attachments:** [adv\\_02870\\_nosc\\_MTO\\_detail\\_design\\_fnl.pdf](#)

---

Attached is a copy of a public advertisement to be placed in the following newspapers:

***Rainy River Westend Weekly*** Wednesday, June 14, 2017

***Fort Frances Times*** Wednesday, June 14, 2017

This is a routine notice for Study Commencement of the Detail Design stage of the project and is a requirement of the ministry's environmental assessment process. The ad has been prepared by our consultant Stantec Consulting Ltd.

Project specific comments and concerns may be directed to Kevin Saunders or Isaac Bartlett, as indicated in the advertisement.

Steve Wall  
Environmental Planner  
Ministry of Transportation – Northwest Region  
615 James Street South  
Thunder Bay, Ontario  
(807) 473-2126

## MINISTRY OF TRANSPORTATION



Provincial Highways Management  
Planning & Design Section  
Northwestern Region  
615 James Street South  
Thunder Bay, Ontario P7E 6P6  
Tel.: (807) 473-2002  
Fax.: (807) 473-2168

June 8, 2017  
File: 193803921

### **Letter to Indigenous Communities**

**Reference: NOTICE OF STUDY COMMENCEMENT  
Detail Design and Class Environmental Assessment  
Baudette/Rainy River International Bridge on Highway 11, Town of Rainy River**

The Ontario Ministry of Transportation (MTO), in cooperation with the Minnesota Department of Transportation, has retained Stantec Consulting Ltd. and Parsons Corp. to undertake the Detail Design and Class Environmental Assessment (Class EA) Study for the replacement of the Baudette/Rainy River International Bridge on Highway 11 in the Town of Rainy River. A copy of the Notice of Study Commencement is attached.

The purpose of the study is to confirm the Recommended Plan, develop and confirm environmental protection and mitigation measures, developing construction staging and traffic management plans, and obtaining all applicable permits.

This Detail Design study is following an approved planning process under the *Class Environmental Assessment for Provincial Transportation Facilities (2000)* for a Group "B" project and will document the existing conditions and sensitivities of the study area, examine potential impacts/changes, give recommendations for proposed mitigation, and outline any commitments for future environmental work.

A *Design and Construction Report (DCR)* will be prepared documenting the detailed Recommended Plan, protection and mitigation measures, potential impacts of the undertaking, protection and mitigation procedures, and commitments to future work.

As part of the study commencement, MTO is inviting you to participate in this study and assist us in identifying the environmental, social, and cultural values your community may have within the study area. If you are aware of any additional First Nation or Aboriginal communities that we should be contacting, please forward that information to me and I will include them on our agency contact list.

The study team will keep you up-to-date on the status of the study as the project progresses. We invite and encourage your input.

June 8, 2017

Page 2 of 2

**Reference: Detail Design and Class Environmental Assessment Baudette/Rainy River International Bridge on Highway 11, Town of Rainy River  
Notice of Study Commencement**

Should you have any questions, please contact me at (807) 473-2109 or 1-800-465-5034, or by email at kevin.saunders@ontario.ca. If you would prefer a meeting with ministry staff, that could also be arranged. Please contact me at your earliest convenience if you are interested in such a meeting.

Yours Truly,



Kevin Saunders  
MTO Senior Project Manager, Northwestern Region  
Tel: (807) 473-2109  
Fax: (807) 473-2168  
kevin.saunders@ontario.ca

c. D. Grove, I. Bartlett N. Gazibara – Stantec Consulting Ltd.  
J. McKinnon – Minnesota Department of Transportation

**Baudette/Rainy River International Bridge Replacement Project (GWP 6046-10-00) Indigenous Community Mailing List**

Title	First Name	Last Name	Position	Organization	Address	City	Province	Postal Code	Telephone	Fax	Email
Ms.	Ashley	Johnson	Senior Advisor	Ministry of Aboriginal Affairs; Consultation Unit	160 Bloor Street West, 9th Floor	Toronto	ON	M7A 2E6			maa.ea.review@ontario.ca
Chief	Carl	Tuesday	Chief	Big Grassy River First Nation	PO Box 414	Morson	ON	P0W 1J0	807-488-5457	807-488-5533	zigtuesday@hotmail.com
Chief	Jim	Leonard II	Chief	Rainy River First Nation	PO Box 450	Emo	ON	P0W 1E0	807-482-2479	807-482-2603	jleonard@bellnet.ca
Chief	Katherine	Kishiqueb	Chief	Ojibways of Onigaming First Nation	PO BOX 160	Nestor Falls	ON	P0X 1K0	807-484-2162	807-484-2737	angie.copenace@onigaming.ca
Chief	Wayne	Smith	Chief	Naicatchewenin First Nation	PO BOX 15, RR1	Devlin	ON	P0W 1C0	807-486-3407	807-486-3704	Wayne.smith@bellnet.ca
Chief	Brian	Perrault	Chief	Couchiching First Nation	RR 2; RMB 2027	Fort Frances	ON	P9A 3M3	807-274-3228	807-274-6458	chuckmcp@vianet.ca
Chief	Janice	Henderson	Chief	Mitaanjigamiing First Nation	PO BOX 609	Fort Frances	ON	P9A 3M9	807-274-2188		Janice@mitaanjigamiing.ca
Chief	Patricia	Big George	Chief	Anishnaabeg of Naongashiing	PO BOX 335	Morson	ON	P0W 1J0	807-488-5602	807-488-5942	lilgoldeneagle@tbaytel.net
Chief	Darlene	Ross Sandy	Chief	Northwest Angle No.33	BOX 1490	Kenora	ON	P9N 3X7	807-733-2200	807-733-3148	moosey@mts.net
Chief	Aileen	Oshie-White	Chief	Northwest Angle No.37	PO Box 267	Sioux Narrows	ON	P0X 1N0	807-226-5353	807-226-1164	chief@nwa37.ca
Chief	Chris	Skead	Chief	Anishnabe of Wauzhushk Onigum	PO Box 1850	Kenora	ON	P9N 3X8	807-548-5868	807-548-4877	ken@wonation.ca
Chief	William	Windigo	Chief	Nigigoonsiminikaaning First Nation	PO Box 68	Fort Frances	ON	P9A 3M5	807-481-2536	807-481-2511	zhaawinigaabo@vianet.ca
			Tribal Council	Pwi-Di-Goo-Zing Ne-Yaa-Zhing Advisory Services	PO Box 522; 1455 Idyllwild Drive	Fort Frances	ON	P9A 3M8	807-274-8531		
Grand Chief	Frances	Kavanagh	Grand Chief	Grand Council Treaty #3	PO Box 1720	Kenora	ON	P9N 3X7	807-548-4215		
President	Clint	Calder	President	MNO Sunset Country Métis Council	418 Third Street East	Fort Frances	ON	P9A 3M3	807-274-7076	807-274-9773	clint@calderlawoffice.ca
Mr.	Hank	Rowlinson	Métis Community Relations Manager	Métis Consultation Unit Métis Nation of Ontario	500 Old St. Patrick Street.; Unit D	Ottawa	ON	K1N 9G4	613-798-1488		hankr@metisnation.org
Ms.	Joanne	Meyer	Director of Intergovernmental Relations	Métis Consultation Unit Métis Nation of Ontario	75 Sherbourne Street, Suite 311	Toronto	ON	M5A 2P9			



**Stantec Consulting Ltd.**  
300-49 Bathurst Street, Toronto ON M5V 2P2

June 8, 2017  
File: 193803921

«Organization\_Name»  
«Region»  
«Address»  
«Address\_2»  
«City» «Prov» «PostCode»

**Attention: «First\_Name» «Last\_Name», «Position»**

**EXTERNAL AGENCY LETTER**

Dear «Title» «Last\_Name»:

**Reference: NOTICE OF STUDY COMMENCEMENT  
Detail Design and Class Environmental Assessment  
Baudette/Rainy River International Bridge Replacement on Highway 11  
Town of Rainy River**

The Ontario Ministry of Transportation (MTO), in cooperation with the Minnesota Department of Transportation, has retained Stantec Consulting Ltd. and Parsons Corp. to undertake the Detail Design and Class Environmental Assessment (Class EA) Study for the replacement of the Baudette/Rainy River International Bridge on Highway 11 in the Town of Rainy River. The bridge provides access between the United States and Canada with 24-hour, full-service Port of Entry facilities. The enclosed Notice of Study Commencement provides a key plan and additional details for your reference.

The purpose of this letter is to introduce the project, to seek your input on the existing conditions within the study area, and to identify any issues, concerns or approval requirements that your organization may have. We respectfully request that you complete and return the enclosed form by **July 28, 2017** so that your comments may be considered early in the design process.

As part of the consultation process for this project, one Public Meeting will be held. You will be notified in advance of the Public Meeting, currently scheduled for Fall 2017 to provide an opportunity to discuss the study with members of the project team and to provide comments. A separate External Agency Meeting will also be scheduled to coincide with the Public Meeting.

If you have any questions regarding the study, please contact the undersigned or one of the Project Managers named in the enclosed material.

Sincerely,

**STANTEC CONSULTING LTD.**

Nevena Gazibara, B.Sc., MREM, ENV SP  
Environmental Planner  
Phone: (416) 598-7663  
Fax: (416) 596-6680  
nevena.gazibara@stantec.com

Attachment: Notice of Study Commencement, Reply Form

c. K. Saunders – MTO Northwestern Region  
D. Grove, I. Bartlett – Stantec Consulting Ltd.  
J. McKinnon – Minnesota Department of Transportation

---

# EXTERNAL AGENCY COMMENT SHEET

## Detail Design and Class Environmental Assessment BAUDETTE/RAINY RIVER INTERNATIONAL BRIDGE REPLACEMENT ON HIGHWAY 11, TOWN OF RAINY RIVER

---

Please submit your comments before July 28, 2017. Use the other side if required.  
With the exception of personal information, all comments will become part of the public record.

Comments may be submitted by mail, fax or email to:

Nevena Gazibara, B.Sc., MREM, ENV SP Environmental Planner  
Stantec Consulting Ltd., 300-49 Bathurst Street, Toronto ON M5V 2P2  
Tel. (416) 598-7663, Fax (416) 596-6680  
Email: nevena.gazibara@stantec.com

Comments submitted by:

---

Name of Contact:

---

Organization:

---

Address (including Postal Code):

---

Telephone Number:

---

Email:

---

The contact name and mailing address for your organization/agency (please check one):

- Is correct; or
  - Should be changed to:
- 
- 

Your interests or concerns regarding the study area or undertaking are (continue on back if required):

---

---

---

Your organization/agency can provide the following environmental (i.e., natural, social, economic or cultural) information and requires the following permits/approvals:

---

---

I am providing a copy of background reports/existing information:

- With this comment sheet
- No background information available
- Via email / mail

The level of involvement you wish to have on this project is:

- Active participation
  - None, please remove from mailing list
  - Participation at key points
- 
-



Baudette/Rainy River International Bridge Replacement (GWP 6046-10-00) External Agencies Mailing List

Category	Title	First Name	Last Name	Position	Organization	Address	Address 2	City	Province	Postal Code
Federal	Mr.	Rob	Dobos	Manager, Environmental Assessment Section	Environment and Climate Change Canada	867 Lakeshore Road	Box 5050	Burlington	ON	L7R 4A6
Federal	Mr.	Rob	Read	Environmental Assessment Officer	Environment and Climate Change Canada	867 Lakeshore Road	Box 5050	Burlington	ON	L7R 4A6
Federal	Mr.	Aaron	Thompson	Senior Water Resources Engineer	Environment and Climate Change Canada	867 Lakeshore Road	Box 5050	Burlington	ON	L7R 4A6
Federal	Ms.	Sheelagh	Hysenaj	Environmental Assessment Officer	Environment and Climate Change Canada	4905 Dufferin Street		Toronto	ON	M3H 5T4
Federal	Mr.	John	Woodward	Senior Environmental Officer, Ontario Region	Canadian Transportation Agency	15 Eddy Street		Gatineau	QC	K1A 0N9
Federal	Mr.	Vince	Harper	Senior Fisheries Protection Biologist	Department of Fisheries and Oceans Canada	103-1800 11th Avenue		Regina	SK	S4P 0H8
Federal	Ms.	Jenny	Archibald	Environmental Assessment Officer	Canadian Environmental Assessment Agency	55 St. Clair Avenue East	Suite 907	Toronto	ON	M4T 1M2
Federal	Ms.	Justine	Knox	Senior Advisor, Indigenous Affairs	Canadian Environmental Assessment Agency	55 St. Clair Avenue East	Suite 907	Toronto	ON	M4T 1M2
Federal	Ms.	Darla	Cameron	Team Leader	Canadian Environmental Assessment Agency	55 St. Clair Avenue East	Suite 907	Toronto	ON	M4T 1M2
Federal	Mr.	David	Zeit	Senior Environmental Officer	Transport Canada	4900 Yonge Street		North York	ON	M2N 6A5
Federal	Ms	Sarah	O'Keefe	Senior Advisor - Environmental Assessment	Transport Canada	330 Sparks Street		Ottawa	ON	K1A 0N5
Federal	Ms.	Sarah	Bunting	Environmental Officer	Transport Canada	4900 Yonge Street		North York	ON	M2N 6A5
Federal	Ms.	Tania	Havelka	Navigable Waters Protection Officer	Transport Canada	100 Front Street South		Sarnia	ON	N7T 2M4
Federal	Ms.	Mary	Johnson	Policy Analyst	Transport Canada	330 Sparks Street		Ottawa	ON	K1A 0N5
Federal	Mr.	Sylvain	Cyr	Director General, Infrastructure and Environmental Operations	Canada Border Services Agency (CBSA)	2265 St. Laurent Boulevard	2nd Floor	Ottawa	ON	K1G 4K3
Federal	Mr.	Christopher	Lazette	Manager, Infrastructure Northern Ontario	Canada Border Services Agency (CBSA)	2265 St. Laurent Boulevard	2nd Floor	Ottawa	ON	K1G 4K3
Federal	Mr.	Paul	Loo	Director	Canada Border Services Agency (CBSA)	79 Bentley Avenue	2nd Floor	Ottawa	ON	K1A 0L8
Federal	Ms.	Tuula	Schuler	Director, Northwestern Ontario District	Canada Border Services Agency (CBSA)	22 Bay Street		Sault Ste. Marie	ON	P6A 5S2
Federal	Ms.	Debbie	Dundas	Project Officer- Infrastructure and Environmental	Canada Border Services Agency (CBSA)	5755 River Road		Niagara Falls	ON	L2G 3K9
Federal	Ms.	Lise	Nordhage-Sangster	Facilities Officer	Canada Border Services Agency (CBSA)	by email:lise.nordhage-sangster@cbsa-asfc.gc.ca				
Federal	Ms.	Kim	Beaudry	Chief of Operations	Canada Border Services Agency (CBSA)	301 Scott Street 2nd Floor		Fort Frances	ON	P9A 1H1
Federal	Ms.	Angela	MacNeil	Facilities Officer for Rainy River (Senior Program Officer)	CBSA - SNC Lavelan (property caretakers)	100 Metcalfe Street, 17th Floor		Ottawa	ON	K1A 0L8
Federal		Rejean	Cantlon	Communications Advisor	Citizenship and Immigration Canada (CIC)	25 St. Clair Avenue East	Suite 700	Toronto	ON	M4T 1M2
Federal	Mr.	Normand	Roy	Staff Sergeant	Royal Canadian Mounted Police (RCMP)	221 Archibald Street North		Thunder Bay	ON	P7C 3Y3
Federal	Ms.	Daniella	Fisher	Public Affairs Officer	Global Affairs Canada	Consulate General of Canada in Minneapolis;	Suite 900	Minneapolis	MN	55415-1899
Federal	Mr.	Kyle	Hipsley	Acting Commissioner International Boundary Commission US and Canada	International Boundary Commission (IBC) - US	2000L Street NW	Suite 615	Washington	DC	20036 USA
Federal	Mr.	Joe	Harrietha	Senior Surveyor	International Boundary Commission - (IBC) Canada	588 Booth Street	Room 210	Ottawa	ON	K1A 0Y7
Federal	Mr.	Daniel	Fortin	Senior Surveyor	International Boundary Commission - (IBC) Canada	588 Booth Street	Room 210	Ottawa	ON	K1A 0Y7
Federal	Dr.	Wayne	Jenkinson	Senior Engineering Advisor	International Joint Commission	234 Laurier Avenue West, 22nd Floor		Ottawa	ON	K1P 6K6
Federal	Ms.	Trish	Morris	Director	International Joint Commission	100 Ouellette Avenue, 8th Fl		Windsor	ON	N9A 6T3
Utility	Ms.	Jackie	Macewicz	Manager of Public Projects	Canadian National Railway (CN)	1625 Depot Street		Stevens Point	WI	54481
Utility	Mr.	John	McEvoy	Implementation Manager	Bell Canada	238 Church Street		Fort Frances	ON	P9A 1C8
Utility	Mr.	Stefan	Linder	Manager of Public Works	Canadian National Railway	4 Welding Way	PO Box 1000	Concord	ON	L4K 1B0
Provincial	Mr.	Ray	Boivin	Senior Environmental Officer-Kenora Area Office	Ministry of the Environment and Climate Change	808 Robertson Street		Kenora	ON	P9N 1X9

Baudette/Rainy River International Bridge Replacement (GWP 6046-10-00) External Agencies Mailing List

Category	Title	First Name	Last Name	Position	Organization	Address	Address 2	City	Province	Postal Code
Provincial	Ms.	Ellen	Cramm	Environmental Planner/EA Coordinator	Ministry of the Environment and Climate Change	435 James Street South	Suite 331	Thunder Bay	ON	P7E 6S7
Provincial	Ms.	Bonnie	McNulty	Regional Advisor-Thunder Bay Office	Ministry of Tourism, Culture and Sport	435 James Street South	Suite 334	Thunder Bay	ON	P7E 6S7
Provincial	Ms.	Paige	Campbell	Archaeology Review Officer, Archaeology Program Unit	Ministry of Tourism, Culture and Sport	435 James Street South	Suite 334	Thunder Bay	ON	P7E 6S7
Provincial	Mr.	Frank	Bastone	Northern Development Advisor-Tourism, Kenora and Area	Ministry of Northern Development and Mines	810 Robertson Street	Suite 104	Kenora	ON	P9N 4J2
Provincial	Ms.	Kim	Austen	Northern Development Officer, Kenora and Area	Ministry of Northern Development and Mines	810 Robertson Street	Suite 104	Kenora	ON	P9N 4J2
Provincial	Ms.	Jane	Gillon	Northern Development Officer, Kenora and Area	Ministry of Northern Development and Mines	922 Scott Street		Fort Frances	ON	P9A 1J4
Provincial	Mr.	Ken	Mantey	Traffic Staff Sergeant	Ontario Provincial Police, Northwest Region Headquarters	615 James Street South	2nd Floor	Thunder Bay	ON	P7E 6P6
Provincial	Mr.	Dereck	McLean	Administrative Sergeant	Ontario Provincial Police- Rainy River Detachment	320 Portage Avenue		Fort Frances	ON	P9A 3P9
Provincial	Mr.	Ron	Van Straalen	Superintendent-Northwest Region Headquarters	Ontario Provincial Police, Northwest Region Headquarters	615 James Street South	2nd Floor	Thunder Bay	ON	P7E 6P6
Municipal	Mrs.	Veldron	Vogan	Chief Administration Officer	Town of Rainy River	PO Box 488		Rainy River	ON	P0W 1L0
Municipal	Ms.	Deborah	Ewald	Mayor	Town of Rainy River	PO Box 488		Rainy River	ON	P0W 1L0
Municipal	Mr.	Dan	McCormick	Chief Administration Officer	Rainy River District Social Services Administration Board	450 Scott Street		Fort Frances	ON	P9A 1H2
Municipal	Mr.	Ed	Carlson	EMS Deputy Chief	EMS Rainy River District Social Services	801 Scott Street		Fort Frances	ON	P9A 1J3
Municipal	Ms.	Linda	Armstrong	Mayor	Dawson Township	PO Box 427		Rainy River	ON	P0W 1L0
Municipal	Mr.	Patrick	Giles	Clerk	Dawson Township	PO Box 427		Rainy River	ON	P0W 1L0



**Stantec**

**Stantec Consulting Ltd.**  
300-49 Bathurst Street, Toronto ON M5V 2P2

June 8, 2017  
File: 193803921

«First\_Name» «Last\_Name»  
«Address»  
«Address\_2»  
«City» «Prov» «PostCode»

**PROPERTY OWNERS & STAKEHOLDERS**

Dear «Title» «Last\_Name»:

**Reference: NOTICE OF STUDY COMMENCEMENT  
Detail Design and Class Environmental Assessment  
Baudette/Rainy River International Bridge Replacement on Highway 11  
Town of Rainy River**

The Ontario Ministry of Transportation (MTO), in cooperation with the Minnesota Department of Transportation, has retained Stantec Consulting Ltd. and Parsons Corp. to undertake the Detail Design and Class Environmental Assessment (Class EA) Study for the replacement of the Baudette/Rainy River International Bridge on Highway 11 in the Town of Rainy River. The bridge provides access between the United States and Canada with 24-hour, full-service Port of Entry facilities. The enclosed Notice of Study Commencement provides a key plan and additional details for your reference.

The purpose of this letter is to introduce the Detail Design phase of the project, to seek your input on the existing conditions within the study area, and to identify any issues that you may have.

As part of the consultation process for this project, one Public Meeting will be held. You will be notified in advance of the Public Meeting, tentatively scheduled for Fall 2017 to provide an opportunity to discuss the study with members of the project team and to provide comments.

If you have any questions regarding the study, please contact the undersigned or one of the Project Managers named in the enclosed material.

Sincerely,

**STANTEC CONSULTING LTD.**

Nevena Gazibara, B.Sc., MREM, ENV SP  
Environmental Planner  
Phone: (416) 598-7663  
Fax: (416) 596-6680  
nevena.gazibara@stantec.com

Attachment: Notice of Study Commencement

c. K. Saunders – MTO Northwestern Region  
D. Grove, I. Bartlett – Stantec Consulting Ltd.  
J. McKinnon – Minnesota Department of Transportation

# Seeds of hope



The Alzheimer's Society offers Seeds of Hope to any group interested. Last week Mary O'Connor and Lenora Gustafson planted the flowers, forget me nots, in the special plot prepared by the Rainy River Hospital. "I just bring the flowers. We plant them all together and every year they come up!" said Mary. If any individual or family needs information on dementia, Mary O'Connor is the person to talk to.

# Fatigue kills campaign launch

## ATU 1624 calls for Canadian Government action to stop fatigue-related fatalities

It only takes a second to fall asleep at the wheel, but driver fatigue begins long before a driver starts to nod off.

Fatigue is one of the leading causes of death on the roads. Roughly 43% of crash fatalities of all motor coach collisions are a direct result of exhaustion. It's an issue that impacts everyone and the consequences can be dire.

There are many factors that can lead to driver fatigue. The primary contributor is a lack of quality sleep. The risk of fatigue also increases when drivers are on the road at times when they would normally be sleeping, such as early

morning or late at night. Did you know a person's alertness is reduced after being awake for 17 hours and that this has the same effect as a blood alcohol concentration of 0.05?

Studies have proven that performance ability begins to decline after being awake for 13 hours. The reality is a motor coach operator can drive up to 13 hours in a 24-hour period with only eight consecutive hours of rest between shifts. How long could you uphold this schedule before suffering from fatigue?

In 2014, ATU (Automated Transit Union) 1624 launched a campaign to

draw awareness to this issue, hoping to prompt the Canadian government to change the Motor Vehicle Transport Act that governs drivers' hours of service.

However, the new world order Trump has blundered upon still resembles the old world order, with one major difference. Trump has made it clear that the United States is no longer willing to carry the leadership burden or foot the bill. Western leaders have begun to realize that this radical departure is not simply another late-night musing of The Donald. It represents the views of significant numbers of frustrated and angry Americans.

You can feel backs stiffening around the Western world. German Chancellor Angela Merkel stated the obvious: "The times in which we could completely depend on others are, to a certain extent, over... We Europeans truly have to take our fate into our own hands."

Canadian Foreign Minister Chrystia Freeland delivered Canada's most assertive foreign policy declaration in decades to the House of Commons. Canada will change its role and increase its military spending to help fill a Trump-induced vacuum in global leadership.

But what kind of world order are we trying to build?

# In Trump's wake, a new world order can inspire us again

By Robert McGarvey

In five short months, Donald Trump has turned the world upside down. So what kind of new world order can we expect?

Trump has attacked NATO (questioning its very existence), shredded trade agreements like the North American Free Trade Agreement (NAFTA) and the Trans-Pacific Partnership (TPP), and withdrawn the U.S. from the Paris climate change agreement. He's insulted allies, embraced brutal dictators and promised to put American interests first and foremost.

It's a far cry from the magnanimous worldview of former U.S. president John F. Kennedy in the 1960s and a radical departure from the post-Second World War norm.

However, the new world order Trump has blundered upon still resembles the old world order, with one major difference. Trump has made it clear that the United States is no longer willing to carry the leadership burden or foot the bill.

Western leaders have begun to realize that this radical departure is not simply another late-night musing of The Donald. It represents the views of significant numbers of frustrated and angry Americans.

You can feel backs stiffening around the Western world.

German Chancellor Angela Merkel stated the obvious: "The times in which we could completely depend on others are, to a certain extent, over... We Europeans truly have to take our fate into our own hands."

Canadian Foreign Minister Chrystia Freeland delivered Canada's most assertive foreign policy declaration in decades to the House of Commons. Canada will change its role and increase its military spending to help fill a Trump-induced vacuum in global leadership.

But what kind of world

order are we trying to build?

The 70-year post-war era was dominated by the United States. After the collapse of Nazi Germany in 1945, the U.S. and its Allies essentially reinvented the western world.

The subsequent world order was built on U.S. military, economic and ideological power. The U.S. was the driving force and de facto leader, not just of NATO, but of a host of post-war institutions like the World Bank, the International Monetary Fund and the World Trade Organization.

After the collapse of Soviet communism in the late 1980s, the 'free world' seemed to be ascendant. There was a strong belief in the 1990s that the final ideological battle had been won and the values and institutions that defined the West would naturally become the governing fundamentals of the entire planet.

That rosy assumption turned out to be almost entirely wrong.

Globalization of the world economy based on the Washington Consensus turned out to be a gift to multi-national corporations that could place their production in low-wage economies with no unions, no environmental restrictions and no messy human rights concerns. Trade agreements insured free entry of these goods to western markets.

This resulted in lost jobs and the hollowing out of the manufacturing base of many developed economies. And that turned many people against globalization. On the political front, many peoples around the world rejected western-inspired progressive initiatives like LGBTQ rights, Muslim fundamentalists, in particular, not only reject democracy and western values but have launched a holy war against them that has brought bloody terror to the streets of Paris, Lon-

don and other western cities.

On the military front, Russia and China are emerging as great powers. Russia never really accepted its second-class status after the fall of the Soviet Union. The Russian annexation of Crimea and its aggressive military intervention in Syria form part of a larger superpower role. President Vladimir Putin is shaping in defiance of the western world-view.

So what should the western response be?

Western leaders could do worse than restoring the West as it existed before the collapse of the Soviet Bloc.

Western values for westerners has a nice ring to it. And, although insular, it frees western powers from intervening around the world to promote values that don't have local support. Let the rest of the world make their own value choices. It sure beats fighting neo-colonial wars in distant lands.

Redrafting trade agreements so they serve the interests of western economies would also have growing support. Free-trade agreements and globalization have not served the interests of the western middle classes and many (quietly) support Trump's plan to blow them up.

The bottom line is that western civilization is not a universal culture. We need to accept our limitations and start taking care of our own.

Making the world over in our image is not only too great a burden for the United States, it's a fool's errand for the rest of us.

Robert McGarvey is chief strategist for Troy Media Digital Solutions Ltd., an economic historian and former managing director of Merlin Consulting, a London, U.K.-based consulting firm. Robert's most recent book is *Futurotics: A Guide to Thriving in Capitalism's Third Wave*.



**Thunderbird Lodge**  
ON BEAUTIFUL RAINY LAKE

**Our Special**  
**This Friday & Saturday is**

**Kentucky Bourbon Steak**  
10 oz. Sirloin Cap Steak  
marinated in bourbon sauce  
and topped with green and red  
peppers, onions, & mushrooms.  
Served with choice of potato.

Now Serving Breakfast, Lunch & Dinner!

10 miles east of In'l Falls on Hwy. 11  
**218-286-3151**  
Dine Daily - 7:00 a.m. - 9:00 p.m.  
[www.thunderbirdrainylake.com](http://www.thunderbirdrainylake.com)

**Boating season is here. Protect your investment.**

At Causeway we believe that insurance is not a commodity but rather a financial protection tool for an individual, family or business.

Boating is a lifestyle choice and one you have worked hard for. By insuring your watercraft you are protecting your investment and your lifestyle against the financial hardship caused by the unexpected, whether damage to your own watercraft, the property of others, or physical injury. With the right type and amount of insurance on your watercraft, you can be protected from the risks that are inherent in everyday life.

**Causeway Insurance, right there with you, offering advice, and providing insurance that matches your needs now and in the future.**



Home's Auto Commercial Trucking Farm  
[www.causewayinsurance.com](http://www.causewayinsurance.com)  
229 Scott St., Fort Frances 274-6688  
303 Fifth St., Rainy River 852-4237  
Serving the District Since 1956!

### NOTICE OF STUDY COMMENCEMENT

**Baudette/Rainy River International Bridge Replacement  
Highway 11, Town of Rainy River**

**THE STUDY**

The Ontario Ministry of Transportation (MTO), in cooperation with the Minnesota Department of Transportation, has retained Stantec Consulting Ltd. and Parsons Corp. to undertake the Detail Design and Class Environmental Assessment (Class EA) Study for the replacement of the Baudette/Rainy River International Bridge on Highway 11 in the Town of Rainy River. The bridge provides access between the United States and Canada with 24-hour, full-service Port of Entry facilities in the City of Baudette and the Town of Rainy River. The Transportation Environmental Study Report (TESR) prepared during Preliminary Design identified a Recommended Plan of a new replacement bridge to be located directly south (upstream) of the existing Baudette/Rainy River Bridge that will tie into the existing U.S. and Canadian Port of Entry facilities.

**THE PROCESS**

The study will follow the Class Environmental Assessment (Class EA) for Provincial Transportation Facilities (2000) process for a Group 'B' project. It includes confirmation of the Recommended Design, confirming environmental protection and mitigation measures, developing construction staging and traffic management plans, and obtaining all applicable permits. The TESR that documented the Recommended Plan was made available for a 30-day public review period during Preliminary Design in February 2017. No significant issues were raised and the project was approved to proceed to Detail Design. The consultation program for Preliminary Design also included three Public Meetings which provided stakeholders and the public opportunities for input throughout the project.

As part of the consultation process for Detail Design, one Public Meeting will be held. The Public Meeting is tentatively scheduled for Fall 2017 to provide an opportunity to review the details of the Recommended Design and to answer questions about the study.

Upon completion of Detail Design, a Design and Construction Report (DCR) will be prepared and made available for a 30-day public review period. The DCR will be prepared in accordance with the MTO Class EA requirements for a Group 'B' project and will document the design-specific details, applicable permits, and environmental commitments and mitigation measures to be carried forward to construction. Notices regarding the availability of the DCR and the times and location of the Public Meeting will be published in local newspapers and sent to persons on the mailing list.

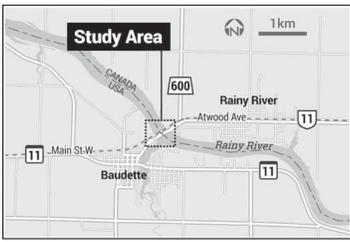
**COMMENTS**

If you wish to comment on this project, have your name added to the project mailing list, or have any questions about this project, please contact one of the individuals identified below.

**Mr. Isaac Bartlett, P.Eng.**  
Consultant Project Manager  
Stantec Consulting Ltd.  
600-171 Queens Avenue  
London, ON N6A 5J7  
tel: 519-675-6643  
call collect: 519-645-2007  
fax: 519-645-6575  
e-mail: isaac.bartlett@stantec.com

**Kevin Saunders**  
MTO Senior Project Manager  
Ministry of Transportation - Northwestern Region  
615 James Street South  
Thunder Bay, ON P7E 6P6  
tel: 807-473-2109  
toll-free: 1-800-465-5034  
fax: 807-473-1168  
e-mail: kevin.saunders@ontario.ca

You are encouraged to visit the project website, [www.dot.state.mn.us/d2/projects/baudette-bridge](http://www.dot.state.mn.us/d2/projects/baudette-bridge), to obtain current project information. If you have any accessibility requirements in order to participate in the project, please contact one of the Project Team members listed above. Comments and information are being collected to assist MTO in meeting the requirements of the Ontario Environmental Assessment Act. Information will be collected in accordance with the Freedom of Information and Protection of Privacy Act. All comments will be maintained on file for use during the study and, with the exception of personal information, may be included in study documentation and become part of the public record.




**DISCOVER WHAT FORT FRANCES HAS TO OFFER! GET CONNECTED TO FORT FRANCES, ONTARIO CANADA**

**EVENT CALENDAR**



Search Events

Fort Frances Event Listings - Find out what's happening in Fort Frances and the Rainy River District!

**DINING RESTAURANTS**



Search Food

Fort Frances dining options - From fine dining to fast food, and everything in between, Fort Frances has it all!

**HOTELS ACCOMMODATIONS**



Search Hotels

Fort Frances hotels and accommodations - check out rooms, rates, and availability of hotels in Fort Frances. Come stay a while!

**COMMUNITY NEWS**



Your quick-stop community news source for Fort Frances and the Rainy River District

**PHOTO OF THE WEEK**



Enjoy photos submitted by community members each week! Submit your own photo

**TOWNSHIPS & MUNICIPALITIES**



Connect with the Town of Fort Frances, and other townships and municipalities across the Rainy River District.

The revamped version of [fortfrances.com](http://fortfrances.com) includes exciting new features like a community calendar, to which anyone can add their upcoming events. —Duane Hicks photo

## New website features calendar

**More from A1**

"If people are coming here for the Fort Frances Canadian Bass Championship, and then see the 'Canadian Coasters' are coming here, too, that's a double draw."

Likewise, the calendar should help local groups plan fundraisers and spread them out more evenly.

"There's quite often a finite amount of money people can spend in one weekend," said Plumridge.

"If we can spread those dates around, it would be more beneficial for everyone," she reasoned.

Plumridge urged people to think ahead when adding to the calendar.

"We'd like people to post their events far in advance," she reiterated.

"If you know your event is going to be next November, put it in now so 14 things don't end up on the same day."

Adding an event to the calendar is simple. Go to the site, click on the "Event calendar" button, then the "Add an event" button.

Then add an event title, event type (i.e., arts and entertainment, sporting, etc.), date and time, and event description.

Right now, the system is set up so events should show up on the calendar immediately after

someone posts it.

But the Times will monitor what is posted to make sure nothing inappropriate is added to the calendar, noted Plumridge.

"If there's any problems, the Times will add an approval process in the future."

Plumridge hopes the community calendar is well-utilized.

"We really encourage people to post their own events," she said. "It's easy to do."

"We'll all be better off if we all know what's happening."

**Submit photos**

Along the same lines as the calendar, the Times is urging the public to submit photos taken here in Rainy River District.

Just click the "Photo of the Week" button, then click on the updates@fortfrances.com link to e-mail a photo.

Be sure to add any relevant information, such as when and where the photo was taken, as well as who took it so they can be credited by name.

Plumridge said the photos can be of just about anything as long as they're taken locally.

"If you have a great photo, put it in," she urged.

"We have so many great photographers in the area that take really great pictures," added Plumridge.

"It says 'Photo of the Week' but it could become 'Photo of

the Day.'"

"If people keep sending up photos, we'll keep putting them up," she pledged.

"It gives us a place to show off our whole area—an event that's happening whatever."

Meant to be a resource for visitors as well as area residents, the site also features a directory of dining and accommodations in Fort Frances and other district communities.

The site also has links to the websites of district municipalities, as well as the Fort Frances Times website.

A major driver for the redesign of the website was to make it mobile-friendly.

Unlike the former version of fortfrances.com, the new one easily can be easily on a desktop computer, tablet, or smartphone.

Its "responsive design" means the site scales its size to fit whatever size of screen it's being viewed on.

"We're really thrilled with the design," said Plumridge, noting Leanne Spry and Cory Westover of the Times Web Design team designed it.

The site was programmed by Confederation College student Danny Miller, who did a co-op at the Times earlier this year, and Bram Van der Kroef of the Times' Web Design team.

## 'Relay for Life' taking shape

**More from A1**

Sus is urging survivors to contact her beforehand at 271-0932 to sign up.

She also noted the committee managed to get 10 teams to register for this year's 'Relay for Life'—something she was worried wouldn't happen.

She conceded, however, that the teams are smaller than in past years.

"Our teams are smaller this year but they've got a lot of heart," Sus enthused.

In fact, she noted one individual on the Riverside Health Care team already has raised \$2,600 in a matter of weeks and had her head shaved.

Other activities throughout the day will include a Zumba warm-up, a dance-off to music from all decades, and a newspaper fashion show.

Samantha Manty, director at the Riverside Foundation for Health Care, said she is happy

the partnership with "Relay for Life" has continued and noted how the money raised has benefited the local hospital over the years.

"This year is special because we were able to continue our partnership with 'Relay for Life,'" noted Manty.

"This allows dollars raised over a certain amount to come back and support cancer care in our community," she explained.

"If we are able to raise more than \$30,000 [this year], everything over that amount will come back and support people right here."

"We're hoping that even if people can't walk, they will think about donating," Manty added.

"Because it's a great way to donate to cancer care, but also support loved ones that are fighting here at home."

"We aren't just supporting the Canadian Cancer Society,

we are supporting local health care," she stressed.

Past money has gone towards a PICC line insertion ultrasound machine and training on how to use it.

Usually, individuals would have to travel to Thunder Bay to receive this type of training, but the money raised allowed Riverside to get it here.

This year, the Foundation is looking at buying comfort chairs for the chemotherapy unit if they're approved.

Meanwhile, Sus noted lunarians are on sale at Pharmacy, From the Grind Up, and the hospital gift shop, and also will be available the day of the relay.

They cost \$5 each.

The committee also is looking for entertainment for the day.

"Anyone interested in donating their time to play a few songs is asked to contact Sus.

## Board honours retirees, volunteers

**Staff**

The Rainy River District School Board honoured its retirees during its final regular board meeting of the 2016-17 school year here last Tuesday evening.

Anne Cox, Paul Elliott, Gregory Scott, Gary Broman, Lloyd Lovelace, Rod Enns, Kim Basaraba, and Cam Keast all were recognized as retiring this spring.

Susan Sieders and Maureen Hahkala also retired this year.

As well, Beverly Arpin, Lorina Fryer, Diane Rumbolt-Carlson, Pam Van Heyst, Kristy Cameron, Edward Ojala, Shauna Sekulich, and David Kerr were honoured for attaining 25 years of service with the board.

Two Volunteer of the Year awards also were presented at last Tuesday's meeting.

Susan Sieders of Rainy River helped out on every hot lunch day at Riverview School.

She also filled in when the hot lunch co-ordinator was unable to make it.

Jimmy Warren of Atikokan,

meanwhile, volunteered to supervise every winter trip of the Outers program at Atikokan High School.

He also assisted with the staff safety plan and transporting equipment.

"Volunteers continue to be an indispensable part of the Rainy River District School Board team," said board chair Dianne McCormack.

"They support and enhance the work of the board, without their many programs, activities, and events just couldn't happen," she stressed.

"We sincerely appreciate all of our volunteers."

Also last Tuesday, the board heard a presentation of the final staff report of the Modified Student Accommodation Review for Atikokan High School and North Star Community School.

It was recommended the schools be consolidated into a new K-12 school on the AHS site.

The final administrative report can be found on the board's website, along with background information, tentative site plans,

and community feedback.

Delegations to the board are scheduled for a special committee of the whole meeting set for this Monday (June 19) at Atikokan High School.

The board is expected to make a final decision in September.

Also at the meeting, trustees: "approved the five-year Strategic Plan Goals that will focus on a rich learning agenda and a focus on relationships;" "heard the winning speech from the district speech contest ('The Hill' by Crossroads student Erica Lemelin);" "approved procurement of Chromebooks up to \$470,000 (increased from \$300,000) to meet an initiative that has been expanded to include Grade 12 students and additional staff (ESP/EAS); and"

"read correspondence covering the EQAO, second-language learning, special education funding, and the establishment of a Standards Development Committee for Education."

## NOTICE OF STUDY COMMENCEMENT

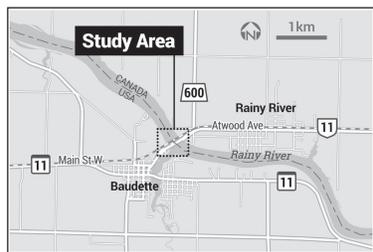
### Baudette/Rainy River International Bridge Replacement Highway 11, Town of Rainy River

**THE STUDY**

The Ontario Ministry of Transportation (MTO), in cooperation with the Minnesota Department of Transportation, has retained Stantec Consulting Ltd. and Parsons Corp. to undertake the Detail Design and Class Environmental Assessment (Class EA) Study for the replacement of the Baudette/Rainy River International Bridge on Highway 11 in the Town of Rainy River. The bridge provides access between the United States and Canada with 24-hour, full-service Port of Entry facilities in the City of Baudette and the Town of Rainy River. The Transportation Environmental Study Report (TESR) prepared during Preliminary Design identified a Recommended Plan of a new replacement bridge to be located directly south (upstream) of the existing Baudette/Rainy River Bridge that will tie into the existing U.S. and Canadian Port of Entry facilities.

**THE PROCESS**

The study will follow the Class Environmental Assessment (Class EA) for Provincial Transportation Facilities (2000) process for a Group 'B' project. It includes confirmation of the Recommended Design, confirming environmental protection and mitigation measures, developing construction staging and traffic management plans, and obtaining all applicable permits. The TESP that documented the Recommended Plan was made available for a 30-day public review period during Preliminary Design in February 2017. No significant issues were raised and the project was approved to proceed to Detail Design. The consultation program for Preliminary Design also included three Public Meetings which provided stakeholders and the public opportunities for input throughout the project.



As part of the consultation process for Detail Design, one Public Meeting will be held. The Public Meeting is tentatively scheduled for Fall 2017 to provide an opportunity to review the details of the Recommended Design and to answer questions about the study.

Upon completion of Detail Design, a Design and Construction Report (DCR) will be prepared and made available for a 30-day public review period. The DCR will be prepared in accordance with the MTO Class EA requirements for a Group 'B' project and will document the design-specific details, applicable permits, and environmental commitments and mitigation measures to be carried forward to construction. Notices regarding the availability of the DCR and the times and location of the Public Meeting will be published in local newspapers and sent to persons on the mailing list.

**COMMENTS**

If you wish to comment on this project, have your name added to the project mailing list, or have any questions about this project, please contact one of the individuals identified below.

**Mr. Isaac Bartlett, P.Eng.**  
 Consultant Project Manager  
 Stantec Consulting Ltd.  
 600-171 Queens Avenue  
 London, ON N6A 5J7  
 tel: 519-675-6643  
 call collect: 519-645-2007  
 fax: 519-645-6575  
 e-mail: isaac.bartlett@stantec.com

**Kevin Saunders**  
 MTO Senior Project Manager  
 Ministry of Transportation - Northwestern Region  
 615 James Street South  
 Thunder Bay, ON P7E 6P6  
 tel: 807-473-2109  
 toll-free: 1-800-465-5034  
 fax: 807-473-2168  
 e-mail: kevin.saunders@ontario.ca

You are encouraged to visit the project website, [www.dot.state.mn.us/d2/projects/baudette-bridge](http://www.dot.state.mn.us/d2/projects/baudette-bridge), to obtain current project information. If you have any accessibility requirements in order to participate in the project, please contact one of the Project Team members listed above. Comments and information are being collected to assist MTO in meeting the requirements of the Ontario Environmental Assessment Act. Information will be collected in accordance with the Freedom of Information and Protection of Privacy Act. All comments will be maintained on file for use during the study and, with the exception of personal information, may be included in study documentation and become part of the public record.

## PUBLIC NOTICE

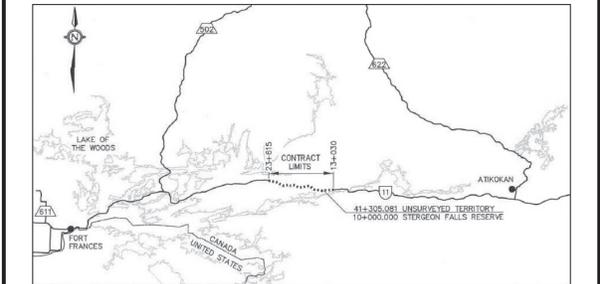
### Improvements to Highway 11 – Mine Centre Easterly

The Ministry of Transportation (MTO) is undertaking a paving project from 0.3 km west of Mine Centre Road and Highway 11 junction, easterly for 20.7 km. Work will commence on June 19<sup>th</sup>, 2017 and is anticipated to be completed by fall 2017.

The General Contractor is **Maple Leaf Construction**

Various sections of Highway 11 will be down to one lane of traffic within the contract limits to construct the work. Traffic control persons/equipment will be present to direct vehicles during construction. Proper signage will also be in place.

### PLEASE DRIVE WITH CAUTION AND OBEY ALL CONSTRUCTION SIGNS



Any questions or concerns regarding the work can be directed to:

**Chris Winch**  
 Project Manager  
 Maple Leaf Construction  
 Ph: (204) 612-5655  
 E-mail: cwinch@mapleleafconstruction.mb.ca

**Adam Walsler**  
 Contract Administrator  
 Ministry of Transportation  
 Ph: (807) 252-9560  
 E-mail: adam.walsler@ontario.ca



---

# **Public Meeting 1**

---



---

To:	K. Saunders, S. Wall	From:	Nevena Gazibara
	MTO Northwestern Region		Stantec-Toronto Office
File:	193803921	Date:	August 23, 2017

---

**Reference: Baudette/Rainy River International Bridge on Highway 11, Town of Rainy River  
Notice of Public Meeting 1 Timing Strategy**

Please find attached the final Notice of Public Meeting 1 timing strategy for the above-mentioned project.

KEY STAGE	DATE
<b>MTO Final Advertisement Approval and Internal Notification</b>	Friday, September 1, 2017
Submit Draft Public Meeting Displays to MTO	Wednesday, September 20, 2017
Notice to MPP (sent by MTO)	Friday, September 8, 2017
External Agency Mailing (Letter, Flyer and Comment Sheet)	Thursday, September 21, 2017
Businesses, Property Owners and Stakeholder Mailing (Flyer)	Thursday, September 21, 2017
Mailing to First Nation and Aboriginal groups (on MTO Letterhead)	Thursday, September 21, 2017
<b>Ontario Government Notice in:</b> <i>Rainy River Westend Weekly</i> (Published on Wednesday)	Wednesday, September 27, 2017
<b>Ontario Government Notice in:</b> <i>Fort Frances Times</i> (Published on Wednesday)	Wednesday, September 27, 2017
<b>Project Website Updated with Public Meeting 1 Materials</b>	Wednesday, October 4, 2017
<b>External Agency Meeting 1 (Rainy River Recreation Centre, Rainy River)</b>	Wednesday, October 4, 2017
<b>Public Meeting 1 (Rainy River Recreation Centre, Rainy River)</b>	Wednesday, October 4, 2017

**Reference: Baudette/Rainy River International Bridge on Highway 11, Town of Rainy River  
Notice of Public Meeting 1 Timing Strategy**

Public Meeting Comments requested by:	Thursday, November 2, 2017
---------------------------------------	----------------------------

**STANTEC CONSULTING LTD.**



Nevena Gazibara, B.Sc., MREM, ENV SP  
Environmental Planner  
Phone: 416-598-7663  
Fax: 416-596-6680  
nevena.gazibara@stantec.com

c. D. Grove, I. Bartlett – Stantec Consulting Ltd.  
J. McKinnon – Minnesota Department of Transportation

**BAUDETTE/RAINY RIVER INTERNATIONAL BRIDGE REPLACEMENT**  
**Highway 11, Town of Rainy River**  
**Notice of Public Meeting 1**

#### THE STUDY

The Ontario Ministry of Transportation (MTO), in cooperation with the Minnesota Department of Transportation, has retained Stantec Consulting Ltd. and Parsons Corp. to undertake the Detail Design and Class Environmental Assessment (Class EA) Study for the replacement of the Baudette/Rainy River International Bridge on Highway 11 in the Town of Rainy River. This notice is to announce the details of the Public Meeting.

The purpose of the Public Meeting is to:

- Present the detailed Recommended Plan;
- Present the Construction Staging and Traffic Management Plan;
- Present protection and mitigation measures; and
- Answer questions about the study.

The Public Meeting is scheduled as follows:

**Date:** October 4, 2017  
**Location:** Rainy River Recreation Centre  
219 Government Road  
Rainy River ON  
**Time:** 4:00 p.m. to 7:00 p.m.

The Public Meeting will be held as a drop-in style, open house format. Representatives of the project team will be available to discuss the study, answer questions, and receive input on the study. We encourage you to attend this meeting to provide us with your comments.

#### THE PROCESS

This study is a Group 'B' project under the *Class Environmental Assessment (EA) for Provincial Transportation Facilities (2000)*, and one Public Meeting will be held.

A *Design and Construction Report (DCR)* will be prepared and made available for a 30-day public review period at the end of the study. Notices regarding the availability of the DCR and the times and locations of where the report will be available for review will be published in local newspapers and sent to persons on the mailing list.

#### COMMENTS

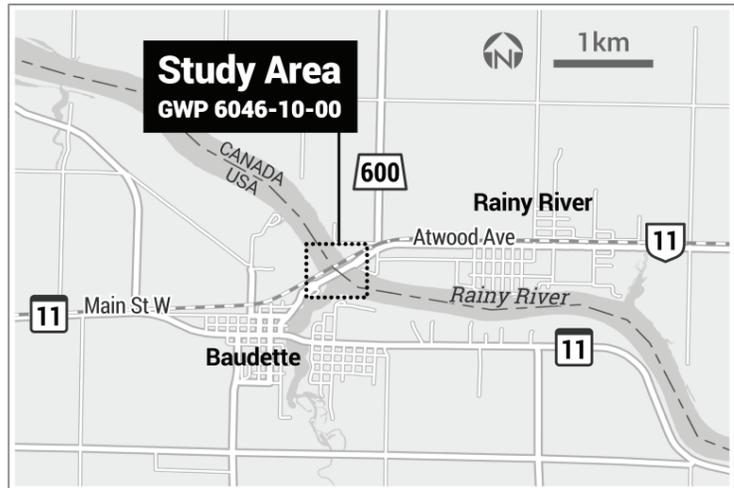
If you wish to comment on this project, have your name added to the project mailing list, or have any questions about this project, please contact one of the individuals identified below.

Isaac Bartlett, P.Eng.  
Consultant Project Manager  
Stantec Consulting Ltd.  
600-171 Queens Avenue  
London ON N6A 5J7  
Tel.: (519) 675-6643  
Call Collect: (519) 645-2007  
Fax: (519) 645-6575  
E-mail: Isaac.bartlett@stantec.com

Kevin Saunders  
MTO Project Manager  
Ministry of Transportation  
Northwestern Region  
615 James Street South  
Thunder Bay ON P7E 6P6  
Tel: 807-473-2109  
Toll free: 1-800-465-5034  
Fax: 807-473-2168  
Email: kevin.saunders@ontario.ca

You are encouraged to visit the project website, [www.dot.state.mn.us/d2/projects/baudette-bridge](http://www.dot.state.mn.us/d2/projects/baudette-bridge), to obtain current project information. If you have any accessibility requirements in order to participate in the project, please contact one of the Project Team members listed above.

Comments and information are being collected to assist MTO in meeting the requirements of the *Ontario Environmental Assessment Act*. Information will be collected in accordance with the *Freedom of Information and Protection of Privacy Act*. All comments will be maintained on file for use during the study and, with the exception of personal information, may be included in study documentation and become part of the public record.



**From:** [Wall, Steven \(MTO\)](#)  
**To:** [Cooper, Doug \(MTO\)](#); [Coulter, Ken \(MNDM\)](#); [Galloway, Iain \(MTO\)](#); [Taylor, John P. \(MTO\)](#); [Piscopo, Annemarie \(MTO\)](#); [Kaszuba, Linda \(MTO\)](#); [McNabb, Marvin \(MTO\)](#); [Weiss, Gary \(MTO\)](#); [scmpp@ndp.on.ca](mailto:scmpp@ndp.on.ca)  
**Cc:** [Saunders, Kevin \(MTO\)](#); [DeCal, Patricia \(MTO\)](#); [Furioso, Karen \(MTO\)](#); [Bartlett, Isaac](#); [Gazibara, Nevena](#)  
**Subject:** RE: G.W.P. 6046-10-00, Rainy River/Baudette International Bridge, Hwy 11 - Ontario Government Notice - Public Information Centre #1\_Detail Design  
**Date:** Friday, September 15, 2017 12:41:59 PM  
**Attachments:** [adv\\_02870\\_PIC1\\_fnl.pdf](#)

---

Please note:

The PIC OGN publication date for the Rainy River Westend Weekly newspaper has changed from Tuesday, September 26, 2017 to Wednesday, September 27, 2017.

There are no changes to the OGN.

All other information associated with the OGN also remains the same.

---

**From:** Wall, Steven (MTO)  
**Sent:** September 11, 2017 9:15 AM  
**To:** Cooper, Doug (MTO); Coulter, Ken (MNDM); Galloway, Iain (MTO); Taylor, John P. (MTO); Piscopo, Annemarie (MTO); Kaszuba, Linda (MTO); McNabb, Marvin (MTO); Weiss, Gary (MTO); [scmpp@ndp.on.ca](mailto:scmpp@ndp.on.ca)  
**Cc:** Saunders, Kevin (MTO); DeCal, Patricia (MTO); Furioso, Karen (MTO); 'Isaac.bartlett@stantec.com'; Gazibara, Nevena <Nevena.Gazibara@stantec.com> (Nevena.Gazibara@stantec.com)  
**Subject:** G.W.P. 6046-10-00, Rainy River/Baudette International Bridge, Hwy 11 - Ontario Government Notice - Public Information Centre #1\_Detail Design

Attached is a copy of a public advertisement to be placed in the following newspapers:

***Rainy River Westend Weekly*** Wednesday, September 27, 2017 [WS – updated publication date]

***Fort Frances Times*** Wednesday, September 27, 2017

This is a routine notice for a Public Information Centre and is a requirement of the ministry's environmental assessment process. The ad has been prepared by our consultant Stantec Consulting Ltd.

Project specific comments and concerns may be directed to Kevin Saunders or Isaac Bartlett, as indicated in the advertisement.

Steve Wall  
Environmental Planner  
Ministry of Transportation – Northwest Region  
615 James Street South  
Thunder Bay, Ontario  
(807) 473-2126

## MINISTRY OF TRANSPORTATION

Provincial Highways Management  
Planning & Design Section  
Northwestern Region  
615 James Street South  
Thunder Bay, Ontario P7E 6P6  
Tel.: (807) 473-2002  
Fax.: (807) 473-2168



October 2, 2017  
File: 193803921

### **Letter to Indigenous Communities**

File: 193803921

#### **Attention:**

**Reference: NOTICE OF PUBLIC MEETING  
Detail Design and Class Environmental Assessment  
Baudette/Rainy River International Bridge on Highway 11, Town of Rainy River**

The Ontario Ministry of Transportation (MTO), in cooperation with the Minnesota Department of Transportation, has retained Stantec Consulting Ltd. and Parsons Corp. to undertake the Detail Design and Class Environmental Assessment (Class EA) Study for the replacement of the Baudette/Rainy River International Bridge on Highway 11 in the Town of Rainy River.

The purpose of this letter is to provide you with a project update and advise you of the upcoming Public Meeting planned for the study. The enclosed Notice provides additional details for your reference. We apologize for the delay in providing this letter to you and would like to provide you with an opportunity to review the Public Meeting displays and have sent a DVD of the Public Meeting displays via post for your review.

External agencies and municipal staff are invited to attend an External Agency Drop-In Meeting on **Wednesday, October 4, 2017 from 3:00 PM to 4:00 PM** at the Rainy River Recreation Centre, 219 Government Road, Rainy River. The External Agency Meeting is being held in advance of a general public session planned between **4:00 PM to 7:00 PM** at the same location. The Public Meeting will provide stakeholders with an opportunity to review the Detail Design Plan, construction staging and traffic management plans, and to provide comments, and address questions or concerns directly with representatives of the project team.

We encourage you to contact the undersigned to provide comments, and address questions or concerns or if you would like to schedule a meeting with ministry staff to discuss the project.

The project team will keep you up-to-date on the status of the study as the project progresses and encourage comments on the displays (provided on DVD via courier and on the project website following the Public Meeting at <http://www.dot.state.mn.us/d2/projects/baudette-bridge/index.html>).

October 2, 2017

Page 2 of 2

**Reference: Detail Design and Class Environmental Assessment Baudette/Rainy River International Bridge on Highway 11, Town of Rainy River  
Notice of Public Meeting**

Should you have any questions, please contact me at (807) 473-2109 or 1-800-465-5034, or by email at kevin.saunders@ontario.ca. If you would prefer a meeting with ministry staff, that could also be arranged. Please contact me at your earliest convenience if you are interested in such a meeting.

Yours Truly,



Kevin Saunders  
MTO Senior Project Manager, Northwestern Region  
Tel: (807) 473-2109  
Fax: (807) 473-2168  
kevin.saunders@ontario.ca

c. D. Grove, I. Bartlett N. Gazibara – Stantec Consulting Ltd.  
J. McKinnon – Minnesota Department of Transportation

**Baudette/Rainy River International Bridge Replacement (GWP 6046-10-00) Aboriginal Communities Mailing List**

<b>Title</b>	<b>First Name</b>	<b>Last Name</b>	<b>Position</b>	<b>Organization</b>	<b>Address</b>	<b>Address 2</b>	<b>City</b>	<b>Province</b>	<b>Postal Code</b>
Chief	Carl	Tuesday	Chief	Big Grassy River First Nation	PO Box 414		Morson	ON	P0W 1J0
Chief	Jim	Leonard II	Chief	Rainy River First Nation	PO Box 450		Emo	ON	P0W 1E0
Chief	Katherine	Kishiqueb	Chief	Ojibways of Onigaming First Nation	PO BOX 160		Nestor Falls	ON	P0X 1K0
Chief	Wayne	Smith	Chief	Naicatchewenin First Nation	PO BOX 15	RR 1	Devlin	ON	P0W 1C0
Chief	Sara	Mainville	Chief	Couchiching First Nation	RR 2; RMB 2027		Fort Frances	ON	P9A 3M3
Chief	Janice	Henderson	Chief	Mitaanjigamiing First Nation	PO BOX 609		Fort Frances	ON	P9A 3M9
Chief	Patricia	Big George	Chief	Anishnaabeg of Naongashiing	PO BOX 335		Morson	ON	P0W 1J0
Chief	Darlene	Ross Sandy	Chief	Northwest Angle No.33	BOX 1490		Kenora	ON	P9N 3X7
Chief	Aileen	Oshie-White	Chief	Northwest Angle No.37	PO Box 267		Sioux Narrows	ON	P0X 1N0
Chief	Chris	Skead	Chief	Anishinabe of Wauzhushk Onigum	PO Box 1850		Kenora	ON	P9N 3X8
Chief	William	Windigo	Chief	Nigigoonsiminikaaning First Nation	PO Box 68		Fort Frances	ON	P9A 3M5
			Tribal Council	Pwi-Di-Goo-Zing Ne-Yaa-Zhing Advisory Services	PO Box 522; 1455 Idyllwild Drive		Fort Frances	ON	P9A 3M8
Grand Chief	Warren	White	Grand Chief	Grand Council Treaty #3	PO Box 1720		Kenora	ON	P9N 3X7
President	Clint	Calder	President	MNO Sunset Country Métis Council	418 Third Street East		Fort Frances	ON	P9A 3M3
Mr.	Hank	Rowlinson	Métis Community Relations Manager	Métis Consultation Unit Métis Nation of Ontario	500 Old St. Patrick Street.; Unit D		Ottawa	ON	K1N 9G4



**Stantec Consulting Ltd.**  
200 - 835 Paramount Drive, Stoney Creek, ON L8J 0B4

September 21, 2017  
File: 193803921

«Organization\_Name»  
«Region»  
«Address»  
«Address\_2»  
«City» «Prov» «PostCode»

**Attention: «First\_Name» «Last\_Name», «Position»                      LETTER TO EXTERNAL AGENCIES**

Dear «Title» «Last\_Name»:

**Reference:        NOTICE OF PUBLIC MEETING  
                      Detail Design and Class Environmental Assessment  
                      Baudette/Rainy River International Bridge Replacement on Highway 11  
                      Town of Rainy River (GWP 6046-10-00)**

The Ontario Ministry of Transportation (MTO), in cooperation with the Minnesota Department of Transportation, has retained Stantec Consulting Ltd. and Parsons Corp. to undertake the Preliminary Design and Class Environmental Assessment (Class EA) Study for the replacement of the Baudette/Rainy River International Bridge on Highway 11 in the Town of Rainy River. The bridge provides access between the United States and Canada with 24-hour, full-service Port of Entry facilities.

The purpose of this letter is to advise you of the upcoming Public Meeting planned for the study. The enclosed Notice provides additional details for your reference.

External agencies and municipal staff are invited to attend an External Agency Drop-In Meeting on **Wednesday, October 4, 2017 from 4:00 Pm to 7:00 PM** at the Rainy River Recreation Centre, 219 Government Road, Rainy River. The External Agency Meeting is being held in advance of a general public session planned between 3:00 PM and 4:00 PM at the same location. The Public Meeting will provide you with an opportunity to review the detailed Recommended Plan, construction staging and traffic management plan, to provide comments, and address questions or concerns directly with representatives of the project team.

If you are unable to attend the External Agency Meeting and would like further information regarding the study, please contact either the undersigned or one of the project team members named in the enclosed information.

Sincerely,  
**STANTEC CONSULTING LTD.**

Nevena Gazibara, B.Sc., MREM, ENV SP  
Environmental Planner  
Phone: (905) 381-3249  
Fax: (905) 385-3534  
nevena.gazibara@stantec.com

Attachment: Notice of Public Meeting

- c. K. Saunders – MTO Northwestern Region
- D. Grove, I. Bartlett – Stantec Consulting Ltd.
- J. McKinnon – Minnesota Department of Transportation

## Baudette/Rainy River International Bridge Replacement (GWP 6046-10-00) External Agencies Mailing List

Category	Title	First Name	Last Name	Position	Organization	Address	Address 2	City	Province	Postal Code
Federal	Mr.	Rob	Dobos	Manager, Environmental Assessment Section	Environment and Climate Change Canada	867 Lakeshore Road	Box 5050	Burlington	ON	L7R 4A6
Federal	Mr.	Rob	Read	Environmental Assessment Officer	Environment and Climate Change Canada	867 Lakeshore Road	Box 5050	Burlington	ON	L7R 4A6
Federal	Mr.	Aaron	Thompson	Senior Water Resources Engineer	Environment and Climate Change Canada	867 Lakeshore Road	Box 5050	Burlington	ON	L7R 4A6
Federal	Ms.	Sheelagh	Hysenaj	Environmental Assessment Officer	Environment and Climate Change Canada	4905 Dufferin Street		Toronto	ON	M3H 5T4
Federal	Mr.	John	Woodward	Senior Environmental Officer, Ontario Region	Canadian Transportation Agency	15 Eddy Street		Gatineau	QC	K1A 0N9
Federal	Mr.	Vince	Harper	Senior Fisheries Protection Biologist	Department of Fisheries and Oceans Canada	103-1800 11th Avenue		Regina	SK	S4P 0H8
Federal	Ms.	Jenny	Archibald	Environmental Assessment Officer	Canadian Environmental Assessment Agency	55 St. Clair Avenue East	Suite 907	Toronto	ON	M4T 1M2
Federal	Ms.	Justine	Knox	Senior Advisor, Indigenous Affairs	Canadian Environmental Assessment Agency	55 St. Clair Avenue East	Suite 907	Toronto	ON	M4T 1M2
Federal	Ms.	Darla	Cameron	Team Leader	Canadian Environmental Assessment Agency	55 St. Clair Avenue East	Suite 907	Toronto	ON	M4T 1M2
Federal	Mr.	David	Zeit	Senior Environmental Officer	Transport Canada	4900 Yonge Street		North York	ON	M2N 6A5
Federal	Ms	Sarah	O'Keefe	Senior Advisor - Environmental Assessment	Transport Canada	330 Sparks Street		Ottawa	ON	K1A 0N5
Federal	Ms.	Sarah	Bunting	Environmental Officer	Transport Canada	4900 Yonge Street		North York	ON	M2N 6A5
Federal	Ms.	Tania	Havelka	Navigable Waters Protection Officer	Transport Canada	100 Front Street South		Sarnia	ON	N7T 2M4
Federal	Ms.	Mary	Johnson	Policy Analyst	Transport Canada	330 Sparks Street		Ottawa	ON	K1A 0N5
Federal	Mr.	Sylvain	Cyr	Director General, Infrastructure and Environmental Operations	Canada Border Services Agency (CBSA)	2265 St. Laurent Boulevard	2nd Floor	Ottawa	ON	K1G 4K3
Federal	Mr.	Christopher	Lazette	Manager, Infrastructure Northern Ontario	Canada Border Services Agency (CBSA)	2265 St. Laurent Boulevard	2nd Floor	Ottawa	ON	K1G 4K3
Federal	Mr.	Paul	Loo	Director	Canada Border Services Agency (CBSA)	79 Bentley Avenue	2nd Floor	Ottawa	ON	K1A 0L8
Federal	Ms.	Tuula	Schuler	Director, Northwestern Ontario District	Canada Border Services Agency (CBSA)	22 Bay Street		Sault Ste. Marie	ON	P6A 5S2
Federal	Ms.	Debbie	Dundas	Project Officer- Infrastructure and Environmental	Canada Border Services Agency (CBSA)	5755 River Road		Niagara Falls	ON	L2G 3K9
Federal	Ms.	Lise	Nordhage-Sangster	Facilities Officer	Canada Border Services Agency (CBSA)	by email:lise.nordhage-sangster@cbsa-asfc.gc.ca				
Federal	Ms.	Kim	Beaudry	Chief of Operations	Canada Border Services Agency (CBSA)	301 Scott Street 2nd Floor		Fort Frances	ON	P9A 1H1
Federal	Ms.	Angela	MacNeil	Facilities Officer for Rainy River (Senior Program Officer)	CBSA - SNC Lavelan (property caretakers)	100 Metcalfe Street, 17th Floor		Ottawa	ON	K1A 0L8
Federal		Rejean	Cantlon	Communications Advisor	Citizenship and Immigration Canada (CIC)	25 St. Clair Avenue East	Suite 700	Toronto	ON	M4T 1M2
Federal	Mr.	Normand	Roy	Staff Sergeant	Royal Canadian Mounted Police (RCMP)	221 Archibald Street North		Thunder Bay	ON	P7C 3Y3
Federal	Ms.	Daniella	Fisher	Public Affairs Officer	Global Affairs Canada	Consulate General of Canada in Minneapolis;	Suite 900	Minneapolis	MN	55415-1899
Federal	Mr.	Kyle	Hipsley	Acting Commissioner International Boundary Commission US and Canada	International Boundary Commission (IBC) - US	2000L Street NW	Suite 615	Washington	DC	20036 USA
Federal	Mr.	Joe	Harrietha	Senior Surveyor	International Boundary Commission - (IBC) Canada	588 Booth Street	Room 210	Ottawa	ON	K1A 0Y7
Federal	Mr.	Daniel	Fortin	Senior Surveyor	International Boundary Commission - (IBC) Canada	588 Booth Street	Room 210	Ottawa	ON	K1A 0Y7
Federal	Dr.	Wayne	Jenkinson	Senior Engineering Advisor	International Joint Commission	234 Laurier Avenue West, 22nd Floor		Ottawa	ON	K1P 6K6
Federal	Ms.	Trish	Morris	Director	International Joint Commission	100 Ouellette Avenue, 8th Fl		Windsor	ON	N9A 6T3
Utility	Ms.	Jackie	Macewicz	Manager of Public Projects	Canadian National Railway (CN)	1625 Depot Street		Stevens Point	WI	54481
Utility	Mr.	John	McEvoy	Implementation Manager	Bell Canada	238 Church Street		Fort Frances	ON	P9A 1C8
Utility	Mr.	Stefan	Linder	Manager of Public Works	Canadian National Railway	4 Welding Way	PO Box 1000	Concord	ON	L4K 1B0
Provincial	Ms.	Ellen	Cramm	Environmental Planner/EA Coordinator	Ministry of the Environment and Climate Change	435 James Street South	Suite 331	Thunder Bay	ON	P7E 6S7

Baudette/Rainy River International Bridge Replacement (GWP 6046-10-00) External Agencies Mailing List

Category	Title	First Name	Last Name	Position	Organization	Address	Address 2	City	Province	Postal Code
Provincial	Ms.	Bonnie	McNulty	Regional Advisor-Thunder Bay Office	Ministry of Tourism, Culture and Sport	435 James Street South	Suite 334	Thunder Bay	ON	P7E 6S7
Provincial	Ms.	Paige	Campbell	Archaeology Review Officer, Archaeology Program Unit	Ministry of Tourism, Culture and Sport	435 James Street South	Suite 334	Thunder Bay	ON	P7E 6S7
Provincial	Mr.	Frank	Bastone	Northern Development Advisor-Tourism, Kenora and Area	Ministry of Northern Development and Mines	810 Robertson Street	Suite 104	Kenora	ON	P9N 4J2
Provincial	Ms.	Kim	Austen	Northern Development Officer, Kenora and Area	Ministry of Northern Development and Mines	810 Robertson Street	Suite 104	Kenora	ON	P9N 4J2
Provincial	Ms.	Jane	Gillon	Northern Development Officer, Kenora and Area	Ministry of Northern Development and Mines	922 Scott Street		Fort Frances	ON	P9A 1J4
Provincial	Mr.	Ken	Mantey	Traffic Staff Sergeant	Ontario Provincial Police, Northwest Region Headquarters	615 James Street South	2nd Floor	Thunder Bay	ON	P7E 6P6
Provincial	Mr.	Dereck	McLean	Administrative Sergeant	Ontario Provincial Police- Rainy River Detachment	320 Portage Avenue		Fort Frances	ON	P9A 3P9
Provincial	Mr.	Ron	Van Straalen	Superintendent-Northwest Region Headquarters	Ontario Provincial Police, Northwest Region Headquarters	615 James Street South	2nd Floor	Thunder Bay	ON	P7E 6P6
Municipal	Mrs.	Veldron	Vogan	Chief Administration Officer	Town of Rainy River	PO Box 488		Rainy River	ON	P0W 1L0
Municipal	Ms.	Deborah	Ewald	Mayor	Town of Rainy River	PO Box 488		Rainy River	ON	P0W 1L0
Municipal	Mr.	Dan	McCormick	Chief Administration Officer	Rainy River District Social Services Administration Board	450 Scott Street		Fort Frances	ON	P9A 1H2
Municipal	Mr.	Ed	Carlson	EMS Deputy Chief	EMS Rainy River District Social Service	801 Scott Street		Fort Frances	ON	P9A 1J3
Municipal	Mr.	Bill	Langner	Mayor	Dawson Township	PO Box 427		Rainy River	ON	P0W 1L0
Municipal	Mr.	Patrick	Giles	Clerk	Dawson Township	PO Box 427		Rainy River	ON	P0W 1L0

# Wynne needs to pay attention to Ontario business for NAFTA negotiations to succeed

by **Christine Van Geyn**, CTF Ontario Director

Premier Wynne is undercutting Ontario manufacturers while attempting to promote free trade, and it couldn't happen at a worse time.

While in Washington DC to promote free trade on September 14, the Premier spoke about Ontario companies that do businesses in the United States, and the benefits these businesses provide to the American and state economies.

Wynne cited the Leland Industries as an example of the success of NAFTA noting the company is investing \$46 million into Illinois.

The problem is ... it isn't true.

Leland founder and CEO,

what our company is doing financially, but this isn't helping," said Nelson.

Leland is a family owned business that manufactures bolts, nuts and screws, and uses North American steel and labour to do so. Nelson stated last year the company is looking to expand in Illinois, but not because of NAFTA. While Nelson says NAFTA does help their business, the reason they are looking to expand into Illinois is because Wynne has made Ontario unaffordable, and the company "can no longer compete with the escalating energy costs we are seeing here in Ontario."

If Wynne is going to act like she is a champion of free trade and Ontario's manufacturing sector, she

"catastrophe," and New York has reached an agreement on "Buy American" legislation that requires the purchase of American-made steel and iron products for state construction.

Since 2005, manufacturing jobs are down 30 per cent in Ontario, with total job losses of 318,000. Over the past 10 years, Ontario has seen the slowest income growth in Canada, trailing the national average of 10.8 per cent growth with a meagre 3.8 per cent growth rate. This stagnation is largely attributed to the gutting of the manufacturing sector.

In such an environment, Canadian businesses need political leaders who can give a staunch, principled, and coherent defence of Canada. Canada is a big up for Ontario businesses and manufacturers, she's going to have to start listening to them first.



Last year the tent was packed.

Rainy River enjoyed a big turnout at the Walleye Tournament and now looks forward to the same for the Pumpkin Festival coming up this weekend under the big tent.

Prizes have increased this year. The biggest pumpkin winner gets \$500 and both the Junior and Senior categories will get \$300 for second, \$200 for third largest and \$100 for fourth. In the best decorated category adults vie for \$50, \$30 and \$10. The Ontario Parliament to use their position to promote outdated and misguided views of

qualify for prizes and so will the most scrumptious desserts.

Everyone who enters and pays at the door will qualify to win a large assortment of door prizes. The Rainy River Library has been fundraising through the summer to cover their deficit and the big draw for the 60" Smart TV will be held at about 2 pm. The last chance to buy tickets, which are only \$10 each, will be at the tent before vendor tables are removed; they are inconsistent with the spirit of reconciliation that is required

kids too. Aside from the races and games, the Railroad Daze Committee will be showing a movie on the big screen.

A Free Will Offering will be taken at the Pot Luck which starts at three in the afternoon. Everyone can bring a dish of their choice and we'll all dig in. The Friends of Hannam Park will have the canteen open during the festivities as well.

Vendor tables are complementary Secretary to the Minister of Indigenous Services.

**Fall Cattle Sale**  
**Saturday, Sept. 30 at 8:30 am**  
 Stratton Sales Barn  
 Broadcast Live through - DLMS network  
 for more information call  
 James Gibson, Sales Barn Manager  
 807-487-2731 home, 271-2005 Cell  
 Sales Barn Phone # 807-483-5354

**DAYCARE**  
  
**Learn, Laugh and Play Children's Centre/Rainy River PlaySchool is seeking an on-call supply staff.**

- Do you enjoy working with children from 12 months to 12 years?
- Energetic and hard working?
- At least 18 years of age?
- Available to work between the hours of 7:30-5:30?
- Willing to undergo a Vulnerable Sector Police Check?
- Flexible?

Please stop by the daycare with your resume or email to [lrcc@hotmail.ca](mailto:lrcc@hotmail.ca)

**REQUEST FOR TENDER DSSAB 17-06**  
**Furnace Replacement, (8 Housing Units)**  
**Fourth St. E. FORT FRANCES, ON**  
**MANDATORY Site Visit, 1133 Fourth St. E.**  
**Tuesday, September 26, 2017 @ 11:00 am**  
 (Local time)  
**CLOSING: 1:30 pm Local Time**  
**TUESDAY, OCTOBER 10, 2017**  
 RFT detailed information available on website [www.rrdssab.on.ca](http://www.rrdssab.on.ca)  
 Or Tender packages can be picked up after **NOON Tuesday, September 19, 2017** at  
 450 Scott St. Fort Frances, ON

**BAUDETTE/RAINY RIVER INTERNATIONAL BRIDGE REPLACEMENT**  
**Highway 11, Town of Rainy River**  
**Notice of Public Meeting 1**

**THE STUDY**  
 The Ontario Ministry of Transportation (MTO), in cooperation with the Minnesota Department of Transportation, has retained **Stantec Consulting Ltd.** and **Parsons Corp.** to undertake the Detail Design and Class Environmental Assessment (Class EA) Study for the replacement of the Baudette/Rainy River International Bridge on Highway 11 in the Town of Rainy River. This notice is to announce the details of the Public Meeting.

The purpose of the Public Meeting is to:

- Present the detailed Recommended Plan;
- Present the Construction Staging and Traffic Management Plan;
- Present protection and mitigation measures; and
- Answer questions about the study.

The Public Meeting is scheduled as follows:  
**Date: October 4, 2017**  
**Location: Rainy River Recreation Centre**  
**219 Government Road**  
**Rainy River, ON**  
**Time: 4:00 p.m. to 7:00 p.m.**

The Public Meeting will be held as a drop-in style, open house format. Representatives of the project team will be available to discuss the study, answer questions, and receive input on the study. We encourage you to attend this meeting to provide us with your comments.

**THE PROCESS**  
 This study is a Group 'B' project under the *Class Environmental Assessment (EA) for Provincial Transportation Facilities (2000)*, and one Public Meeting will be held.

A Design and Construction Report (DCR) will be prepared and made available for a 30-day public review period at the end of the study. Notices regarding the availability of the DCR and the times and locations of where the report will be available for review will be published in local newspapers and sent to persons on the mailing list.

**COMMENTS**  
 If you wish to comment on this project, have your name added to the project mailing list, or have any questions about this project, please contact one of the individuals identified below.

**Isaac Bartlett, P.Eng.**  
 Consultant Project Manager  
 Stantec Consulting Ltd.  
 600-171 Queens Avenue  
 London, ON N6A 5J7  
 tel: 519-675-6643  
 call collect: 519-645-2007  
 fax: 519-645-6575  
 e-mail: [isaac.bartlett@stantec.com](mailto:isaac.bartlett@stantec.com)

**Kevin Saunders**  
 MTO Project Manager  
 Ministry of Transportation, Northwestern Region  
 615 James Street South  
 Thunder Bay, ON P7E 6P6  
 tel: 807-473-2109  
 toll-free: 1-800-465-5034  
 fax: 807-473-2168  
 e-mail: [kevin.saunders@ontario.ca](mailto:kevin.saunders@ontario.ca)

You are encouraged to visit the project website [www.dot.state.mn.us/d2/projects/baudette-bridge](http://www.dot.state.mn.us/d2/projects/baudette-bridge) to obtain current project information. If you have any accessibility requirements in order to participate in the project, please contact one of the Project Team members listed above.

Comments and information are being collected to assist MTO in meeting the requirements of the Ontario *Environmental Assessment Act*. Information will be collected in accordance with the *Freedom of Information and Protection of Privacy Act*. All comments will be maintained on file for use during the study and, with the exception of personal information, may be included in study documentation and become part of the public record.

# Emo woman happy to drive area's first fully-electric vehicle

By Nicholas Donaldson  
Staff writer  
ndonaldson@fortfrances.com

Yoyen Szeder's reason for buying a KIA Soul EV, the first fully-electric car in this area according to her insurance agent, was fairly straightforward.

"So I can save gas," she said. Szeder bought the vehicle at the beginning of August from a dealership in Thunder Bay and has been very happy with it ever since.

"I like it because it's quiet inside, my kids love it, and I don't need to change oil or anything," she told the Times last week.

"Sometimes with the van, I would forget to put oil in or I or that," Szeder noted. "This is just less maintenance."

Szeder said the car has a range of 160 km, which is fine for her because she mostly uses it to get from her home in Emo to work at Young Star House in Chapple.

"I haven't tried it in the winter yet so that is going to be interesting because they said when it's cold, [the range] can drop down like 40 percent," she conceded.

Szeder charges her car with a 120-volt cord, which she could do at home but prefers to do at work.

"My employer supports the electric vehicles, too, so I can charge anytime I want at work and it is free," she remarked.

Szeder said she can go into work, plug in, and get free power without having to worry about it.

It's almost like getting free gas.

"I think it's a good thing when the employer supports it because my employer announced it to all the workers and now quite a few people are interested," she added.

Szeder said with the driving she does, she only needs to charge the vehicle once every couple of days. But sometimes she will do it more frequently just because of the convenience of doing it while she's at work.

Inside the car, instead of a conventional gas gauge, the car gives the number of remaining kilometres, which Szeder said she has to watch carefully but hasn't had any problems yet.

"I don't know about winter," she admitted. "That's why we didn't get rid of the van yet so that we have it if there is ever trouble."

"I told my friend, 'If you get an electric, get the higher range because then you just charge once a week,'" she recalled.

Szeder noted she did look at other models before choosing the KIA Soul, with the Chevrolet Volt being one of the main options.

She said although it did have a larger range, it wasn't as spacious inside and wouldn't fit her three children comfortably.

"I also looked at the hybrid

one from KIA but for me, I am mostly driving over 50 [km/h] so it doesn't benefit me," Szeder said, explaining that once the hybrid reaches 50 km/h, it switches over to gas.

"I think the hybrid is good for the city because you are usually driving slower, but I go speeding on the highway to work," she laughed.

Although the car was purchased from a dealership in Thunder Bay, Szeder said she can take it to Dave Goodman Automotive (the KIA dealership in Emo) for maintenance.

Szeder said she believes fully-electric cars will become popular in the future despite some hesitations.

"I think people are still kind of scared but for me, I just wanted to save money," she reasoned.

"A new vehicle is not a luxury, I needed one," she stressed. "So I looked at how much it cost and the government grant and thought, 'I should take advantage of it.'"

Szeder added she's been telling her friends that if they need a new vehicle to try electric as the vehicle has worked well for her.

"I'm starting to have two jobs now and going to Fort, then to Emo and to Young Star costs too much money for gas," she noted.

"Now I don't feel it and money I would use for gas goes into other payments," she said.



Emo resident Yoyen Szeder stood next to her KIA Soul EV which was purchased at the beginning of August and is the first fully-electric car in the district. So far, Szeder has had no problems and is happy with the cost-saving benefits of the vehicle, which she thinks will become more popular down the road. —Nicholas Donaldson photo



## 'Making Magic'

Riverside Foundation for Health Care's special events committee, including Jan Abbott, left, Livia Lundon, Sandy McKinnon, Lynne Savage, Janice Cousineau, Natalie DeGagne, and Karen Woods, showed off a small sample of items donated for their upcoming fall gala, "Making Magic for District Health Care," which will mark the Foundation's 20th anniversary. While all 180 tickets sold out in a

flash, local businesses and individuals still are urged to support the Foundation and donate items for the auctions and penny table at the Oct. 14 event. For the silent auction, contact Livia Lundon (274-7285); for donations to the penny table, contact Rochelle Duchnicki (274-6581) or Karen Woods (274-1450).

—Duane Hicks photo

# FITS asking for town support

By Duane Hicks  
Staff writer  
dhicks@fortfrances.com

The Fun in the Sun Committee is asking town council for its support of expanded Canada Day festivities in 2018.

Committee chair Cynthia Donald, making a verbal presentation during the public budget meeting held Monday night in conjunction with the regular council meeting, requested council put aside \$2,000-\$5,000 to help them with next year's celebration.

"We would like to extend the July 1 holiday to a two-day event because July 1 is on a Sunday," she noted.

"We would also like to include our senior citizens and the groups that may need assistance to enjoy the celebration."

Donald said FITS was "quite fortunate to have the citizens

of Fort Frances support us on many fundraisers throughout the year."

On top of that, the Moffat Family Fund was "very generous to our cause in such a way that we were able to pay expenses and also provide a new type of log for a log-rolling program that is at the Sportsplex," she added.

The FITS committee also realizes Moffat funding may not be available to it again in 2018, but still will apply for a piece of that annual funding nonetheless.

FITS also is asking council to earmark funds for next year's Canada Day fireworks.

In a letter from committee member Dave Coats, he noted the Town of Fort Frances contributed \$7,500 to the Canada Day fireworks show this past July 1.

"Over the past couple of

years, we have been able to increase our budget through the contributions of the town and many local sponsors," Coats wrote.

"This increased budget has allowed us to put on fireworks displays that are the best the town has ever seen."

"To allow us to continue to put on top-quality displays to end one of the biggest public gatherings of the year, I would ask mayor and council to consider once again contributing \$7,500 towards the purchase of fireworks products," Coats said.

He noted the committee also will pursue additional funds through federal and provincial grants, local sponsors, and fundraising.

Both requests were referred to the 2018 budget process for consideration.

## BAUDETTE/RAINY RIVER INTERNATIONAL BRIDGE REPLACEMENT

Highway 11, Town of Rainy River  
Notice of Public Meeting 1

### THE STUDY

The Ontario Ministry of Transportation (MTO), in cooperation with the Minnesota Department of Transportation, has retained Stantec Consulting Ltd. and Parsons Corp. to undertake the Detail Design and Class Environmental Assessment (Class EA) Study for the replacement of the Baudette/Rainy River International Bridge on Highway 11 in the Town of Rainy River. This notice is to announce the details of the Public Meeting.

The purpose of the Public Meeting is to:

- Present the detailed Recommended Plan;
- Present the Construction Staging and Traffic Management Plan;
- Present protection and mitigation measures; and
- Answer questions about the study.

The Public Meeting is scheduled as follows:

Date: October 4, 2017  
Location: Rainy River Recreation Centre  
219 Government Road  
Rainy River, ON  
Time: 4:00 p.m. to 7:00 p.m.

The Public Meeting will be held as a drop-in style, open house format. Representatives of the project team will be available to discuss the study, answer questions, and receive input on the study. We encourage you to attend this meeting to provide us with your comments.

### THE PROCESS

This study is a Group 'B' project under the Class Environmental Assessment (EA) for Provincial Transportation Facilities (2000), and one Public Meeting will be held.

A Design and Construction Report (DCR) will be prepared and made available for a 30-day public review period at the end of the study. Notices regarding the availability of the DCR and the times and locations of where the report will be available for review will be published in local newspapers and sent to persons on the mailing list.

### COMMENTS

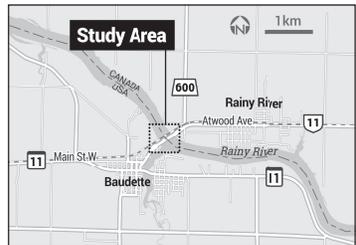
If you wish to comment on this project, have your name added to the project mailing list, or have any questions about this project, please contact one of the individuals identified below.

Isaac Bartlett, P.Eng.  
Consultant Project Manager  
Stantec Consulting Ltd.  
600-171 Queens Avenue  
London, ON N6A 5J7  
tel: 519-675-6643  
cell collect: 519-645-2007  
fax: 519-645-6575  
e-mail: isaac.bartlett@stantec.com

Kevin Saunders  
MTO Project Manager  
Ministry of Transportation, Northwestern Region  
615 James Street South  
Thunder Bay, ON P7E 6P6  
tel: 807-473-2109  
toll-free: 1-800-465-5034  
fax: 807-473-2168  
e-mail: kevin.saunders@ontario.ca

You are encouraged to visit the project website [www.dot.state.mn.us/d2/projects/baudette-bridge](http://www.dot.state.mn.us/d2/projects/baudette-bridge) to obtain current project information. If you have any accessibility requirements in order to participate in the project, please contact one of the Project Team members listed above.

Comments and information are being collected to assist MTO in meeting the requirements of the Ontario Environmental Assessment Act. Information will be collected in accordance with the Freedom of Information and Protection of Privacy Act. All comments will be maintained on file for use during the study and, with the exception of personal information, may be included in study documentation and become part of the public record.



**2017 MODEL BLOWOUT!**

**0% FOR 84 MONTHS!**

ON ALL NEW 2017 INVENTORY OR CASH CREDITS UP TO \$12,000!

**2017 CHEV SILVERADO LT TRUE NORTH 4X4**  
Fully Loaded. Stk# 17187  
MSRP \$49,775

**\$12,780 OFF** JUST **\$36,995\***  
OR **\$219\*** BIWEEKLY

HST & LICENCE EXTRA. INCLUDES VISA BONUS. PAYMENT IS 84 MONTH TERM @ 0%, OAC.

**MacDONALD**  
CHEVROLET GMC  
274-5321 • 1-800-465-7765  
1324 King's Hwy, Fort Frances, ON www.jrmmacdonaldmotors.ca

*Nobody Beats Our Deals!*

---

**APPENDIX D:  
SELECTED CORRESPONDENCE**

---



Summary of External Agency Correspondence

Contact Information	Issue/Concern	Action taken by Project Team
<b>AGENCY COMMENTS</b>		
International Joint Commission (IJC) (Global Affairs Canada and U.S. State Department)	<ul style="list-style-type: none"> <li>Completed review of hydraulics analysis, modelling and report for the project (April-June 2017)</li> <li>Sent letter (June 30, 2017) to confirm that an International Boundary Waters Treaty Act permit would not be required for the project.</li> <li>A review of the hydraulic analysis confirmed that impacts to water levels and flows during and following construction of the new structure would be de minimis</li> <li>During the IJC's review of the hydraulics analysis Environment and Climate Change Canada requested additional hydraulics information and following an additional review provided professional expertise and review of the analysis and noted that hydraulic impacts are negligible</li> </ul>	<ul style="list-style-type: none"> <li>Submitted (April 7, 2017) the project hydraulic analysis and stormwater management plan for the project to Global Affairs Canada and the U.S. State Department for determining the appropriate IJC process for the project</li> <li>As per ECCC's request for additional information, Stantec submitted additional hydraulic and background information to aid in the IJC review process</li> </ul>
International Boundary Commission (IBC) (U.S. and Canada)	<ul style="list-style-type: none"> <li>Telephoned (August 8, 2017) to note that the IBC has no comments on the bridge aesthetic report</li> <li>Emailed (August 8, 2017) to note that the plaques have been removed from the existing bridge in advance of construction and will be placed on the replacement bridge by the IBC once construction is complete</li> </ul>	<ul style="list-style-type: none"> <li>Emailed (August 2, 2017) to note that the removal of the sidewalk on the existing bridge for the construction of the replacement bridge will require that the border plaques be removed earlier than anticipated</li> <li>Emailed (August 4, 2017) to provide final aesthetic report for border delineation features of replacement bridge to IBC for input</li> <li>Submission of International Border Commission Permit application (October 25, 2017)</li> </ul>
Canadian Environmental Assessment Agency (CEAA)	<ul style="list-style-type: none"> <li>Attended several meetings and provided correspondence (November 2016-March 2017) to Stantec and MTO during the preparation of the Project Description document</li> <li>CEAA published the Project Description for a 20-day public review of the document (sent notification letters to agencies and Indigenous communities, made Project Description available on CEAA website for review)</li> <li>Following the public review comment period and CEAA's review, sent letter (April 27, 2017) to confirm that a federal Environmental Assessment will not be required for the</li> </ul>	<ul style="list-style-type: none"> <li>Attended several meetings with CEAA (November 2016 - March 2017) to confirm the Project Description requirements</li> <li>Submitted the Project Description (March 2, 2017) to CEAA for determination of whether a federal EA will be required for the project</li> </ul>

*Summary of External Agency Correspondence*

<i>Contact Information</i>	<i>Issue/Concern</i>	<i>Action taken by Project Team</i>
<p>Department of Fisheries and Oceans Canada (DFO)</p>	<p>project</p> <ul style="list-style-type: none"> <li>• Conference call (August 15, 2017) with Project Team to discuss the project, construction methodology and protection and mitigation measures for fish and fish habitat.</li> <li>• During conference call noted that they would prefer that the in-water timing restrictions were the same on both sides of the border for the construction of the project but that they do not have any authority over activities that take place in the U.S.</li> <li>• DFO also noted that extra consideration should be given for protection and mitigation measures for barge activity to prevent and avoid spills within the Rainy River.</li> <li>• Noted that the DFO do not foresee any issues or authorization requirements with the information provided to-date and that potential impacts to fish and fish habitat as a result of this project appear to be minor/have been minimized</li> </ul>	<ul style="list-style-type: none"> <li>• During conference call with DFO (August 15, 2017) provided information on in-water timing restrictions, the permanent areas of impacts of the bridge piers, temporary areas of impact as a result of construction activities such as temporary towers, crane and tender barges and planned work areas around abutments (as previously provided by Parsons).</li> <li>• Noted that all of the pier construction and related activities would take place within one construction season. Provided an overview of the general fish habitat in the study area and noted that the study area only has general habitat (no specialized fish habitat) and the river is considered a migratory route for Lake Sturgeon.</li> <li>• Stantec noted that they will review and where necessary, include appropriate protection measures during the development of contract specifications.</li> <li>• Contract specifications for project include protection measures for activities on barges to protect against spills in the river</li> <li>• Contract specifications for project include protection and mitigation measures for the transport and use of cement near and in the river</li> </ul>
<p>Transport Canada - Highway, Border &amp; Motor Carrier Policy</p>	<ul style="list-style-type: none"> <li>• Email correspondence (May-August 2017) to provide information on the International Bridges and Tunnels Act permit application requirements, permit review timelines and submission protocol</li> <li>• Sent letter of confirmation on September 29, 2017 confirming receipt of IBTA application</li> </ul>	<ul style="list-style-type: none"> <li>• Telephoned on May 3, 2017 to discuss IBTA permit requirements, level of detail required for construction methodology, construction schedule, security and business case information.</li> <li>• Email correspondence (May-August 2017) to discuss requirements of IBTA submission</li> <li>• Submitted IBTA permit application on</li> </ul>

Summary of External Agency Correspondence

Contact Information	Issue/Concern	Action taken by Project Team
		September 11, 2017
Transport Canada - Navigation Protection Program	<ul style="list-style-type: none"> <li>Sent letter of confirmation on September 29, 2017 confirming receipt of NPP application</li> <li>Correspondence via email (October 26, 2017) providing additional information on application review process</li> <li>Conference call (November 20, 2017) to discuss the Navigation Protection Act permit application</li> </ul> <p>Transport Canada asked the following clarification questions regarding the Rainy River Navigation Protection Act Permit:</p> <p>Will navigation be maintained during the entire construction of the bridge?</p> <ol style="list-style-type: none"> <li>Will navigation be maintained during the entire construction of the bridge?</li> <li>Is there a sequence of construction for the piers or will they all be completed simultaneously?</li> <li>Would the navigational opening move during different phases of construction?</li> <li>Would construction barges be anchored during construction?</li> <li>Would the navigational channel be marked during construction?</li> <li>Would the navigational channel be permanently marked following construction?</li> <li>Can Stantec confirm the U.S. Coast Guard's lighting requirements?</li> </ol> <p>Transport Canada noted that they have confirmed that an environmental review will not be required as part of this permit review (following consultation with other federal agencies including CEAA) but that the Transport Canada Aboriginal Consultation Unit is reviewing the application and will contact Stantec Consulting to discuss consultation efforts and whether additional Indigenous and Aboriginal consultation will be required for the project.</p> <p>Transport Canada also noted that a separate Notice of Works permit application will need to be submitted for the</p>	<ul style="list-style-type: none"> <li>Submitted <i>Navigation Protection Act</i> Permit application on September 11, 2017</li> <li>During conference call (November 20, 2017) noted the following responses to Transport Canada's questions: <ol style="list-style-type: none"> <li>A defined navigational channel will be maintained on both sides of the border throughout construction.</li> <li>Parsons Transportation Group has provided a construction staging plan that shows a sequence of pier construction, starting with Piers 1 and 2 on the U.S. side. However, the construction staging plan shown is a proposed staging plan and the final staging and methodology will be determined by the Contractor.</li> <li>Yes, the navigational opening may shift during different stages of construction, and this will be determined and confirmed by the Contractor.</li> <li>Yes, barges would be anchored to the riverbed during construction.</li> <li>Yes, both the U.S. and Canadian navigational channel would be marked as per navigational lighting standards during construction.</li> <li>Yes, as per the U.S. Coast Guard requirements, navigational lighting would be provided permanently for the two navigational channels. Stantec will provide Transport Canada with the U.S. Coast Guards navigational lighting standards and guidelines for review.</li> </ol> </li> <li>Stantec confirmed that they will provide the U.S. Coast Guard navigational lighting standards and guidelines to Transport Canada to aid in their review (subsequently emailed to Transport Canada on November 21, 2017)</li> </ul>

*Summary of External Agency Correspondence*

<i>Contact Information</i>	<i>Issue/Concern</i>	<i>Action taken by Project Team</i>
	<p>decommissioning of the existing bridge once the bridge removal plan is developed for the project. An authorization to remove the existing bridge will be issued to the owners of the bridge. The permit process is the same process for the construction of the replacement bridge and must be obtained prior to the removal of the existing bridge.</p> <p>Transport Canada doesn't require permanent navigational lighting at the site because there is no navigational lighting currently provided but they will have to confirm their requirements based on the U.S. Coast Guard's requirements for permanent lighting and will communicate their decision to Stantec. In particular, Transport Canada has concerns that navigational markers for both channels may cause confusion and unnecessary border crossings. Transport Canada will provide recommendations for lighting to delineate between the two channels and provide for safe passage for boaters.</p> <p>Transport Canada noted that they have reviewed the bridge plans and will send along their edits to the navigational lighting shown on the plans in order to meet Canadian and U.S. navigational lighting standards. Transport Canada subsequently provided their edits to the bridge plans.</p>	<ul style="list-style-type: none"> <li>• Stantec will continue to communicate and provide any necessary information to Transport Canada to assist in their review of the permit.</li> </ul>
<p>Canadian Border Services Agency (CBSA)</p>	<ul style="list-style-type: none"> <li>• Attended meeting on May 9, 2017 with project team to discuss the proposed bridge and approach reconstruction, and to review construction operations during the construction period. Items discussed included the need for future security cameras, fencing (temporary and permanent), daily communication with the contractor, vetting construction employees, and equipment monitoring.</li> <li>• Attended the Contractor Workshop (July 25, 2017) to explain their security protocols and will also be in attendance at the future Contract Pre-Bid Meeting during tendering.</li> <li>• Attended meeting with project team on September 14, 2017 to obtain a project and schedule update and discuss security during construction, laydown areas, and contractor communication requirements.</li> </ul>	<ul style="list-style-type: none"> <li>• During the May 9, 2017 meeting, the project team provided updated information on the recommended plan, construction methodology and construction schedule</li> <li>• During the September 14, 2017 meeting, the project team provided updated information on the construction schedule, security procedures and construction and laydown areas</li> </ul>

Summary of External Agency Correspondence

Contact Information	Issue/Concern	Action taken by Project Team
Kenora Area Office Thunder Bay Office Ministry of the Environment and Climate Change (MOECC)	<ul style="list-style-type: none"> <li>Telephoned on September 29<sup>th</sup>, 2017 and asked whether a Permit to Take Water (PTTW) will be submitted</li> <li>Asked whether waste disposal sites have been determined yet for the disposal of the existing bridge materials</li> <li>Noted that the local landfills would not be able to fulfill the disposal requirements due to small capacity</li> </ul>	<ul style="list-style-type: none"> <li>During telephone conversation on September 29, 2017 noted that a draft PTTW will be submitted by Stantec during Detail Design</li> <li>Noted that waste disposal sites and facilities would be identified by the Contractor prior to starting construction</li> </ul>
Fort Frances District Ministry of Natural Resources and Forestry (MNRF)	<ul style="list-style-type: none"> <li>A conference call (September 22, 2017) was held to provide a project update, construction methodology information and confirm whether this new information triggers an <i>Endangered Species Act</i> (ESA) permit/authorization, and to discuss any environmental protection and mitigation requirements with MNRF.</li> <li>MNRF noted that Lake Sturgeon (Threatened in Ontario) move up river in spring to spawning areas, and the main concern is timing to not disturb this migration to spawning areas.</li> <li>MNRF noted that abiding by the in-water timing window is an appropriate protection/ avoidance measure, as per the ESA. The preliminary construction schedule was developed to adhere to both US/Canada in-water timing restrictions.</li> <li>ESA requirements were discussed related to listed and threatened species.</li> <li>Sediment and erosion control, amount and disposal of excavated materials, and the percentage of the river blocked by permanent and temporary construction structures were discussed.</li> <li>MNRF noted that, given the information provided during the call, an ESA is not anticipated/required.</li> <li>MNRF will confirm whether timing windows can be adjusted by location of pier (e.g. March 15 to June 30 for piers deeper in water) to provide more flexibility in the construction schedule.</li> <li>As the project proceeds, environmental specifications are confirmed. MNRF will be updated on the project.</li> </ul>	<ul style="list-style-type: none"> <li>During conference call (September 22, 2017) noted the following:</li> <li>The in-water work timing restriction window in Minnesota differs from the timing window in Ontario by about a month and that Lake Sturgeon is not listed in Minnesota. In Ontario in-water work is not allowed from April 1 to July 15, and in Minnesota in-water work is not allowed from March 15 to June 15.</li> <li>Provided a summary of temporary structures required for construction including cofferdams and the temporary causeway required on the U.S. side for the construction of Piers 1 and 2 on the U.S. side (access restrictions).</li> <li>Emailed (October 25, 2017) to confirm that a review for an adjustment of timing windows will not be required for the project (does not benefit the project schedule)</li> </ul>

*Summary of External Agency Correspondence*

<i>Contact Information</i>	<i>Issue/Concern</i>	<i>Action taken by Project Team</i>
EMS Rainy River District Social Services Administration Board	<ul style="list-style-type: none"> <li>Submitted comment form via email (June 22, 2017) to state that the EMS Rainy River District has no concerns about the project if access across the bridge is maintained</li> </ul>	<ul style="list-style-type: none"> <li>Access across the existing bridge will be maintained during the construction of the new bridge</li> </ul>
Bell Canada - Fort Frances	<ul style="list-style-type: none"> <li>Submitted a comment form via email (June 13, 2017) to state that Bell Canada has an existing communication cable on the existing bridge and permits/approvals will be required by Bell Canada for constructing and maintaining a new communication facility within MTO RoW</li> <li>Attended meeting with project team on June 29, 2017</li> <li>During meeting noted that a 4-inch conduit strapped to the side of the new bridge is acceptable and that they will supply the new cable and arrange for the installation of the cable. A temporary line will be installed prior to construction to resolve a conflict with the existing Bell line and the proposed bridge abutment. Upon completion of the new bridge, a new line will be run through the new bridge and connected. At this time, the existing line and the temporary line will be abandoned in place.</li> </ul>	<ul style="list-style-type: none"> <li>Attended meeting on June 29, 2017 and noted that 4-inch conduit would be provided on the new bridge and that Bell Canada will be consulted later on in the project to confirm timing of installation of new cable and to coordinate future work</li> </ul>

**GENERAL PUBLIC COMMENTS (COMMENT SHEETS SUBMITTED AT THE DETAIL DESIGN PUBLIC MEETING)**

Comment 1	<ul style="list-style-type: none"> <li>How will this project affect local business on Water Street?</li> </ul>	Your interest in the property on Water Street is noted. The project is not anticipated to have negative impacts to local businesses in Rainy River or Baudette. It is also not expected that residents in the study area will experience significant delays or disruption during construction. Access will be maintained on the existing bridge while the replacement bridge is built, and the bridge will eventually tie into the existing Canadian Port of Entry plaza and road network.
Comment 2	<ul style="list-style-type: none"> <li>If the Contractor needs an extra laydown area I have 4.3 acres available in close proximity to the CBSA Port of Entry</li> </ul>	Your offer of land for an additional temporary construction laydown area has been noted and circulated to the project team for consideration. The final construction laydown areas will be determined by the Contractor when the project is tendered for construction. If there is a need for

*Summary of External Agency Correspondence*

<i>Contact Information</i>	<i>Issue/Concern</i>	<i>Action taken by Project Team</i>
		additional laydown areas, the Contractor will contact you.
Comment 3	<ul style="list-style-type: none"><li>• Presentation was very good and easy to follow</li></ul>	Comment noted



---

**APPENDIX E:  
PUBLIC MEETING MATERIALS**

---



# Welcome

*to the Detail Design*

## Public Meeting

*for the replacement of the*

### Baudette – Rainy River

### International Bridge

This is the Public Meeting for the Detail Design Study for the Baudette – Rainy River International Bridge Replacement on Trunk Highway 72 in the City of Baudette, Minnesota and Highway 11 in the Town of Rainy River, Ontario

### *Please sign-in*

*and take time to review the information, or discuss any aspect of the project with the project team members in attendance. A comment sheet is available for you to fill out at today's meeting and submit to the project team.*



REPLACEMENT OF THE  
Baudette – Rainy River International Bridge

Detail Design Public Meeting 1  
October 4, 2017



# Objectives

## Purpose and Objectives

The purpose and objectives of the Public Meeting, are as follows:

- Present the detailed recommended plan
- Display and seek input on the construction staging and traffic management plan
- Present protection and mitigation measures
- Answer questions about the study

## Contact Information

### Dale Grove

Consultant Project Manager (US)  
Stantec Consulting Ltd.  
6188 Rome Circle NW  
Rochester MN 55901-4846  
USA

Tel: 507-529-6039

Fax: 507-282-3100

Email: dale.grove@stantec.com

### Isaac Bartlett

Consultant Project Manager (Canada)  
Stantec Consulting Ltd.  
600-171 Queens Avenue  
London, ON N6A 5J7  
CANADA

Tel: 519-675-6643 (+0 collect)

Fax: 519-645-6575

Email: isaac.bartlett@stantec.com

### Joe McKinnon

MnDOT Project Manager  
Minnesota Department of Transportation  
3920 Highway 2 West  
Bemidji MN 56601  
USA

Tel: 218-755-6517

Email: joseph.mckinnon@state.mn.us

### Kevin Saunders

MTO Senior Project Manager  
Ministry of Transportation  
Northwestern Region  
615 James Street South  
Thunder Bay ON P7E 6P6  
CANADA

Tel: 807-473-2109

Fax: 807-473-2168

Email: kevin.saunders@ontario.ca



REPLACEMENT OF THE  
Baudette – Rainy River International Bridge

Detail Design Public Meeting 1  
October 4, 2017



# Study Process



This study follows multiple approved processes:

- Ontario Ministry of Transportation Class Environmental Assessment for Provincial Transportation Facilities (MTO Class EA)
- Canadian Environmental Assessment Act (CEAA)
- U.S. National Environmental Policy Act (NEPA)
- Minnesota Environmental Policy Act (MEPA)

## The MTO Class Environmental Assessment Process

The MTO Class EA process is an approved process for highway planning, design, and construction projects. The study is following a Group 'B' process which is completed for major improvements to existing provincial transportation facilities.

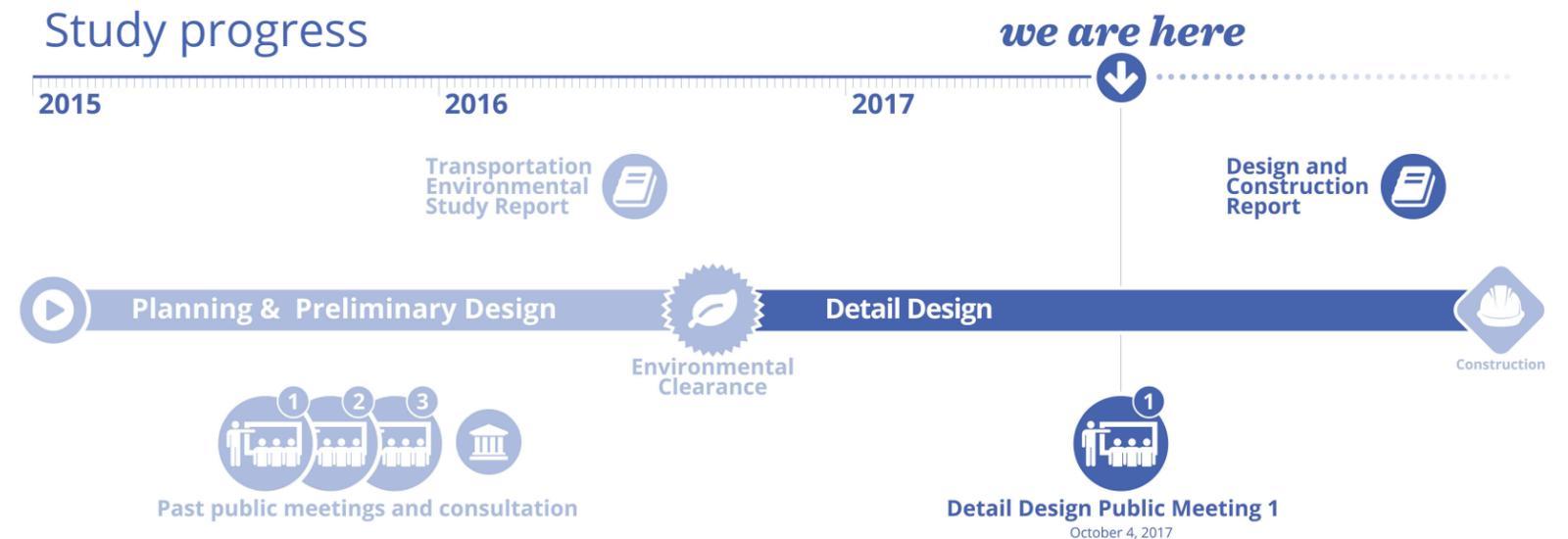
A *Design and Construction Report* (DCR) will be filed for a 30-day public review period at the end of the study. The DCR will summarize the study process, including a description of the project and its purpose, the consultation process, specific environmental effects and mitigation measures, and details of the Recommended Plan.

A project description was submitted to the Canadian Environmental Assessment Agency to determine whether a federal environmental assessment of the designated project was required. Following the submission of the project description and a 20-day public comment period, the CEA Agency determined that a federal EA would not be required.

## The MnDOT Environmental Review Process and NEPA

Minnesota Department of Transportation's (MnDOT's) environmental review has followed Minnesota's environmental review process set by the National Environmental Policy Act (NEPA), and Minnesota Environmental Policy Act (MEPA), to fulfill requirements at both the state and federal level. A combined Environmental Assessment / Environmental Assessment Worksheet (EA/EAW) will be prepared. The EA/EAW uses public input and technical analysis to determine the needs, deficiencies, impacts, mitigation and design of the proposed project.

## Study progress



# Background



## The Baudette – Rainy River International Bridge

is located on Minnesota Trunk Highway 72 and Ontario Highway 11 and spans the Rainy River between Baudette, Minnesota and Rainy River, Ontario.

The bridge was originally built in 1959 and serves as a vital link between the two communities.

It carries approximately 1,350 vehicles per day and provides access between the U.S. and Canadian full-service, 24-hour Ports of Entry for the movement of international traffic and commerce.

The existing bridge is a six-span truss structure with six steel beam approach spans that carry two lanes of traffic over the Rainy River. It is 391 m long (1,285 ft) and has a 7.3 m wide (24 ft) two-lane roadway with an open steel grate deck. There is also a 1.8 m wide (6 ft) sidewalk cantilevered on the south side of the bridge.



# Investigations

In accordance with the MTO Class Environmental Assessment, this study includes engineering and environmental specialists who are carrying out background studies and site-specific work to confirm details of the recommended plan, and identify impacts and mitigation.

The ongoing investigations for this study include, but are not limited to, the following:

- Bridge
- Drainage and hydrology
- Foundations
- Pavement design
- Electrical (lighting)
- Aquatic resources (confirm potential impacts)
- Vibrations assessment



# Existing Conditions, Mitigation, and Commitments

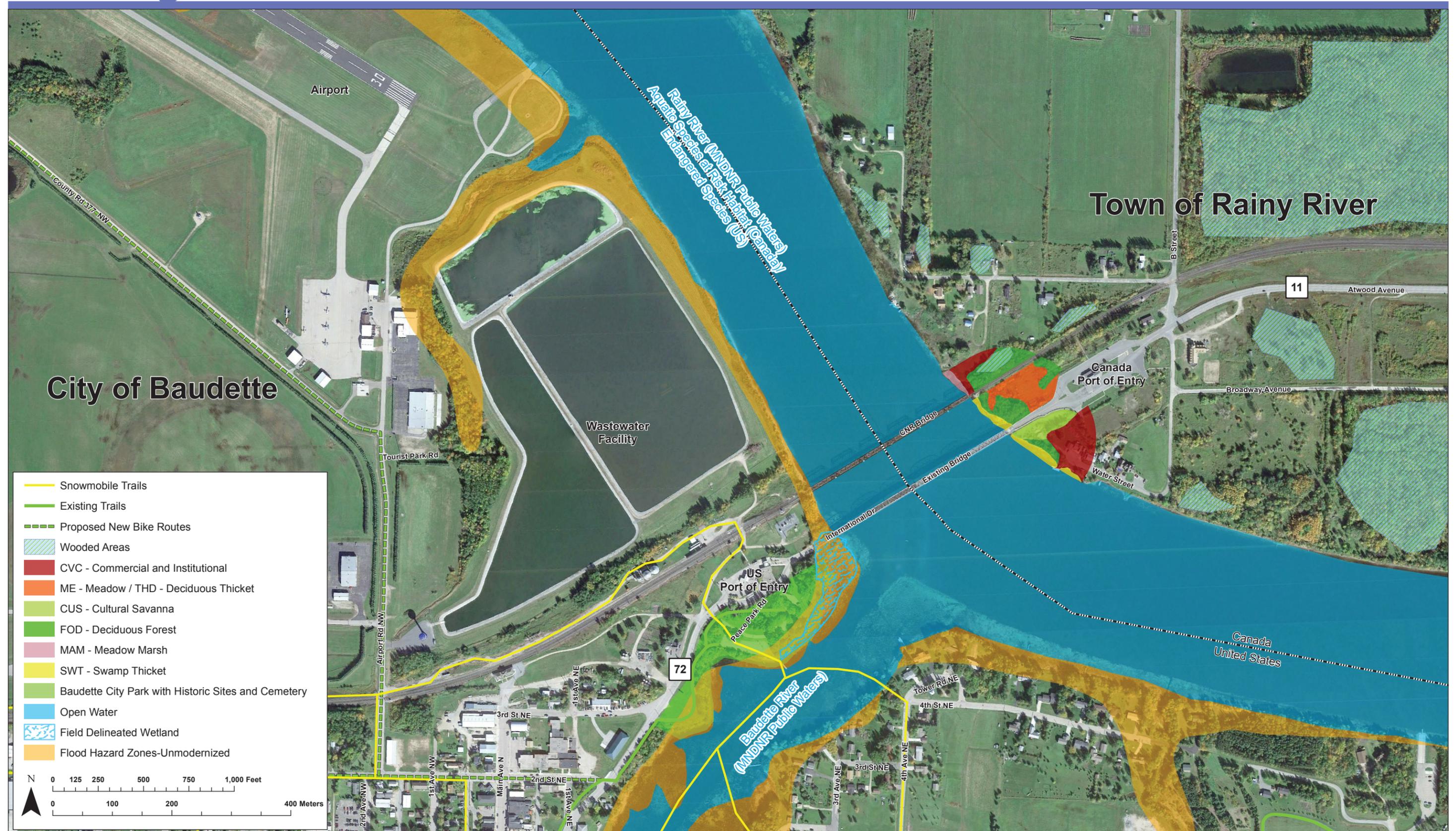
Environmental factors were assessed during the 2017 Preliminary Design Study to determine existing environmental conditions and to identify appropriate protection and mitigation measures to minimize and/or address potential impacts associated with the Baudette/Rainy River International Bridge Replacement.

Environmental issues and commitments to prevent or reduce environmental impacts will be confirmed and documented in this Detail Design phase (i.e., Design and Construction Report, Contract provisions).

The table below summarizes the factors that will be considered as the project is designed and constructed.

Factor	Issue	Commitments / Mitigation
<i>Fish, fish habitat, and species-at-risk</i>	<ul style="list-style-type: none"> <li>Impacts to fish habitat and aquatic species-at-risk</li> </ul>	<ul style="list-style-type: none"> <li>Coolwater timing window- No work in water between March 15 through July 15</li> <li>Significant fisheries impacts are not anticipated</li> <li>Appropriate protection and mitigation measures for fish and fish habitat will be documented in contract specifications</li> </ul>
<i>Vegetation protection and restoration</i>	<ul style="list-style-type: none"> <li>Impacts to sensitive vegetation communities affected by construction</li> </ul>	<ul style="list-style-type: none"> <li>Minimal impacts to vegetation are anticipated</li> <li>Vegetation protection and restoration measures (including allowable clearing windows) will be specified in contract specifications</li> <li>Areas will be restored to pre-existing conditions</li> </ul>
<i>Surface water</i>	<ul style="list-style-type: none"> <li>Drainage associated with bridge replacement</li> <li>Potential temporary impacts during construction</li> </ul>	<ul style="list-style-type: none"> <li>Appropriate drainage and stormwater facilities planned for replacement bridge</li> <li>Document appropriate spill protection measures during construction in contract specifications</li> </ul>
<i>Air quality</i>	<ul style="list-style-type: none"> <li>Impacts to air quality during construction</li> </ul>	<ul style="list-style-type: none"> <li>Identify equipment standards and operations constraints for Contractor and document in contract specifications</li> </ul>
<i>Noise</i>	<ul style="list-style-type: none"> <li>Temporary noise impacts during construction</li> </ul>	<ul style="list-style-type: none"> <li>Identify appropriate hours of operation and work during construction in contract specifications</li> </ul>
<i>Socio-economic environment</i>	<ul style="list-style-type: none"> <li>Emergency service access</li> <li>Utilities access</li> <li>Property owners nearby study area</li> </ul>	<ul style="list-style-type: none"> <li>Ongoing consultation with municipalities, agencies and property owners and coordination with the Town of Rainy River</li> <li>Consultation and coordination with EMS provider-temporary road closures of maximum 15 minute duration during construction will be allowed</li> <li>Notify property owners in advance of construction through notification letters and notices</li> <li>Meeting with public during construction</li> </ul>
<i>Cultural environment</i>	<ul style="list-style-type: none"> <li>No known archaeological sites directly in study area</li> </ul>	<ul style="list-style-type: none"> <li>Stage 1 and Stage 2 Archaeological Assessments have determined that the study area has low potential for archaeological finds</li> </ul>
<i>Property, waste, and contamination</i>	<ul style="list-style-type: none"> <li>Excess materials in proximity to sensitive areas</li> <li>Potential for wastes to enter river during construction and decommissioning of existing bridge</li> </ul>	<ul style="list-style-type: none"> <li>Document appropriate spill protection measures and contaminated soils disposal requirements during construction</li> </ul>

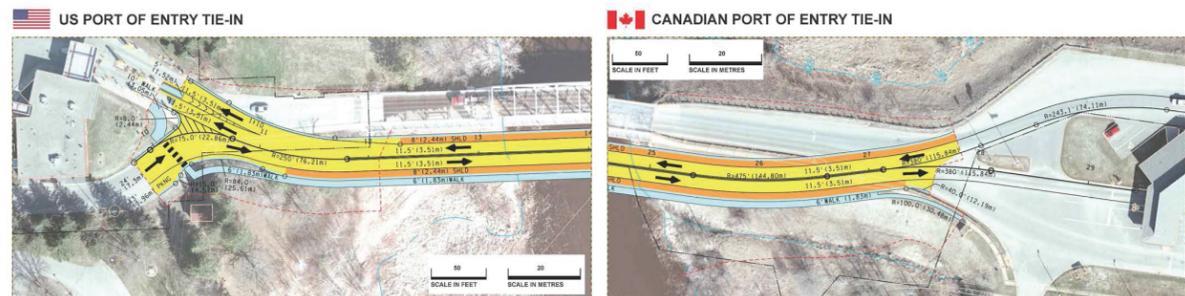
# Existing Conditions



# Recommended Plan

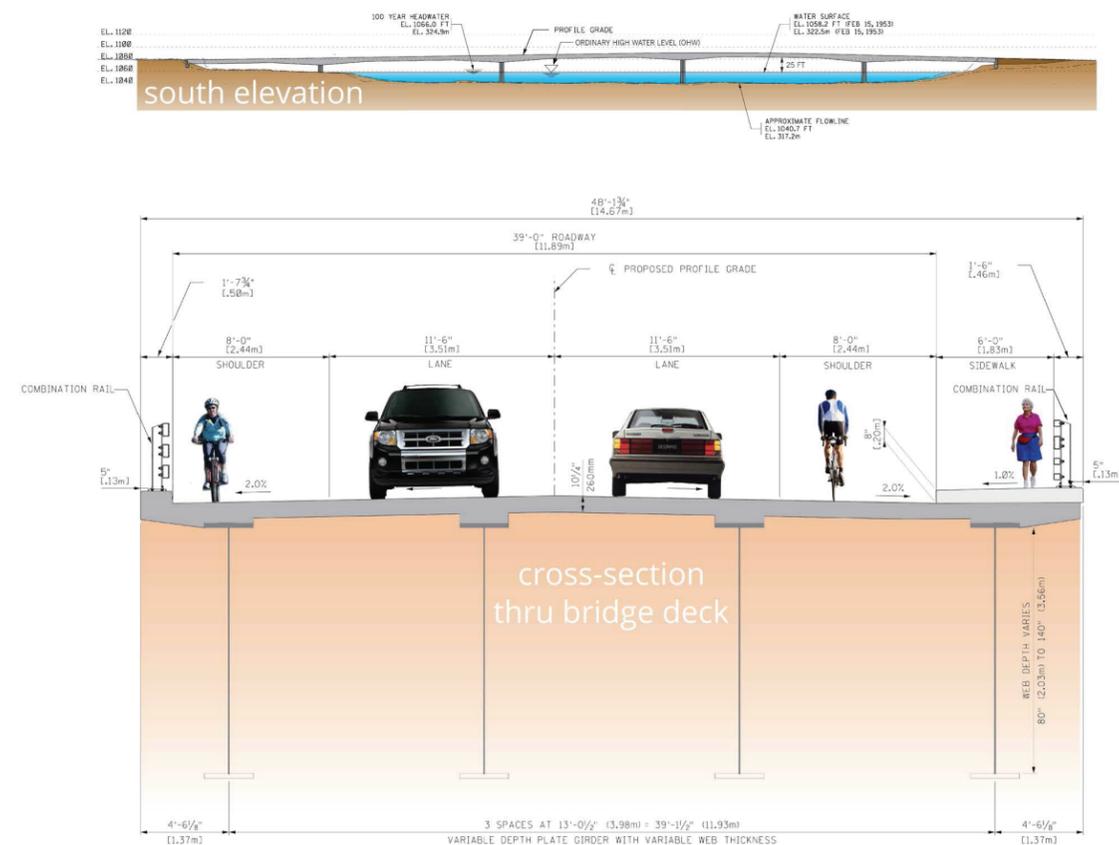


rendering of the new bridge



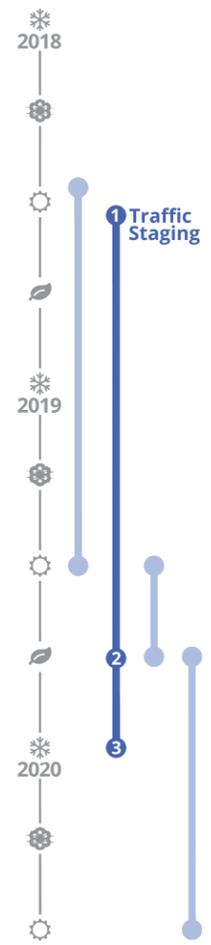
## The recommended plan includes the following:

- The new bridge will be a four-span steel girder structure with a concrete deck. The total length of the bridge will be 411 m
- The replacement of the existing bridge with a new bridge on a new alignment located approximately 3 ft (1m) upstream of the existing bridge
- Alignment and profile improvements that will improve sight distance across the bridge
- Realignment and regrading of Port of Entry tie-in to accommodate the new bridge location
- Drainage improvements
- Maintaining two lanes of traffic on the existing bridge during construction
- A sidewalk on the south (upstream) side of the bridge and shoulders on both sides of the roadway



# Traffic Staging

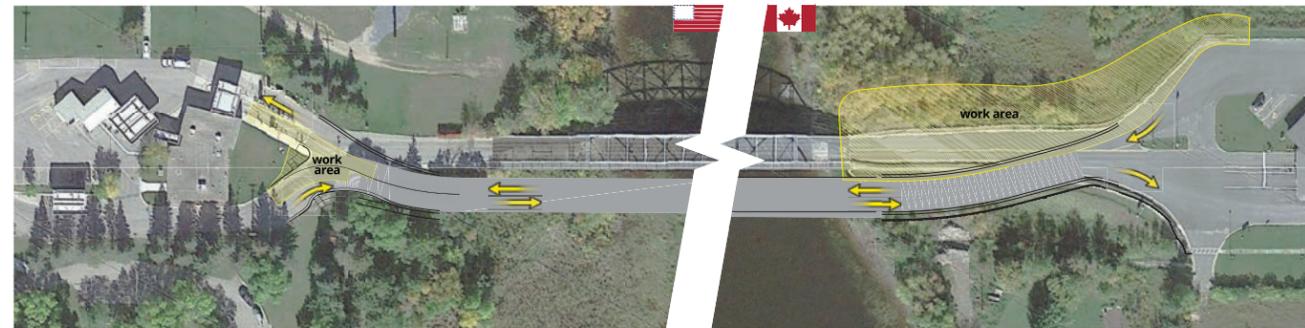
## CONSTRUCTION PHASES



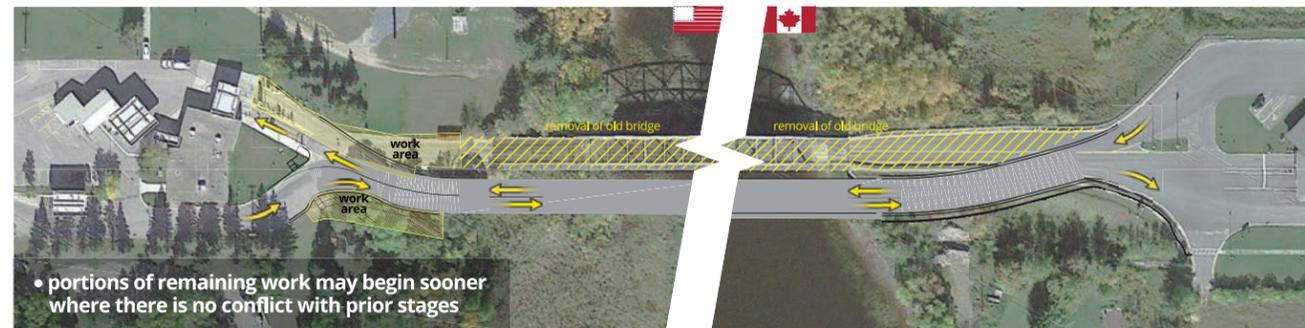
Stage **1**  
traffic on *old* bridge



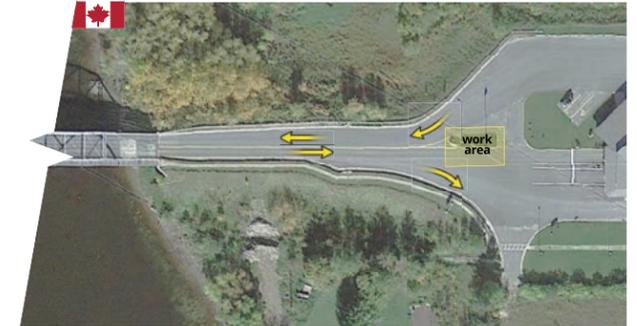
Stage **2**  
traffic on *new* bridge



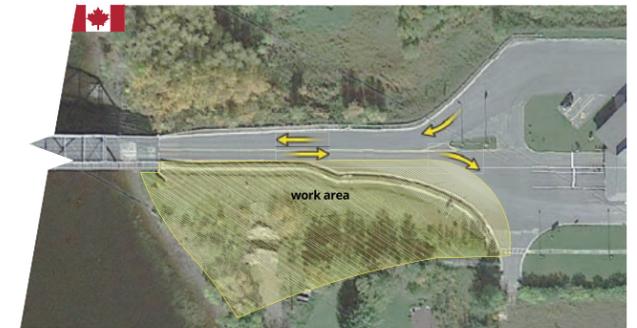
Stage **3**  
remaining work



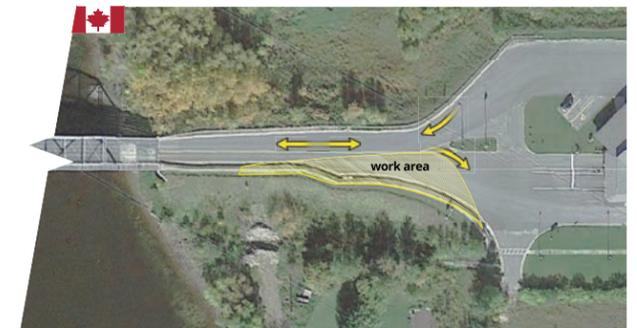
Stage **1a**



Stage **1b**



Stage **1c**

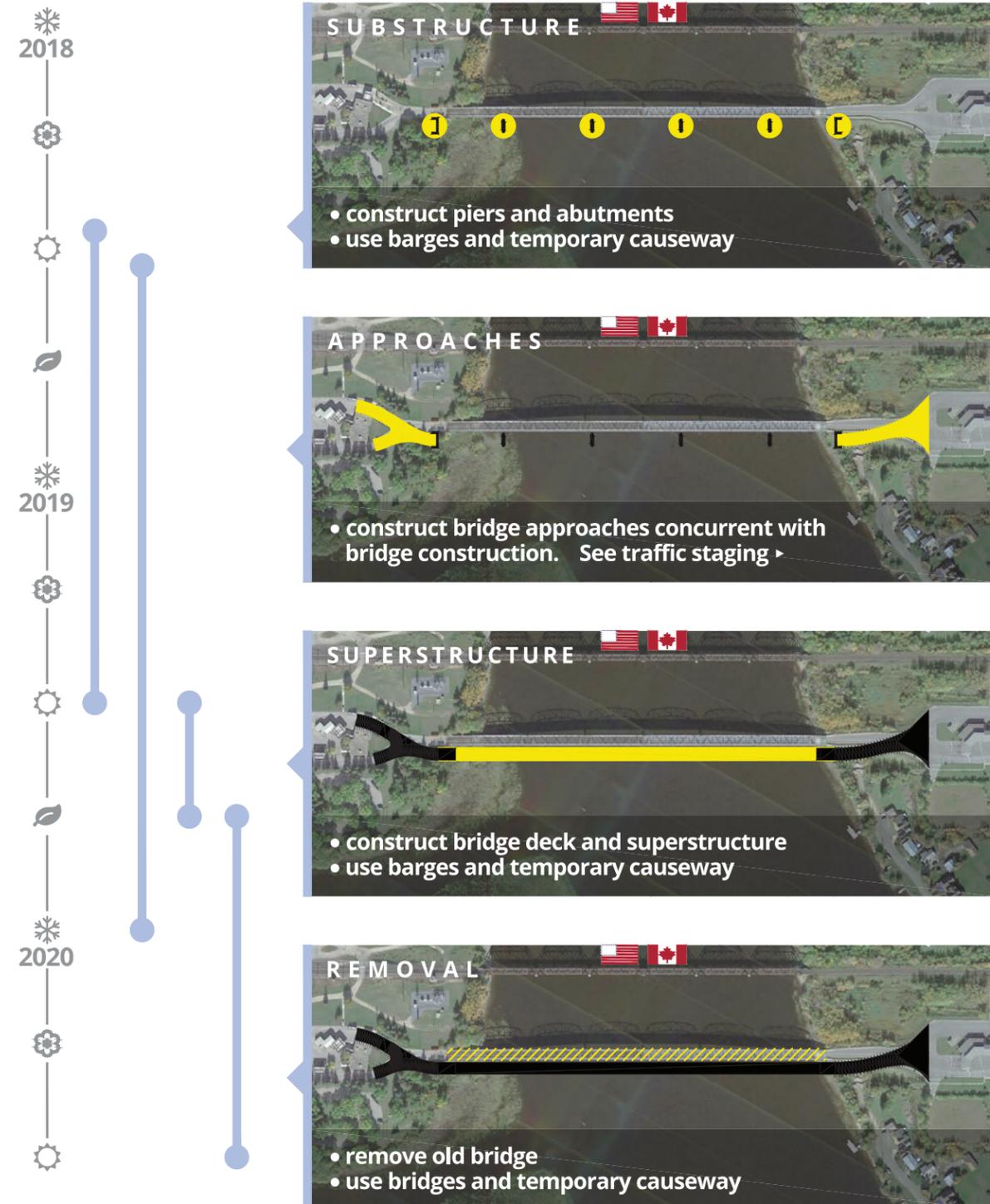


# Construction

## What to expect during construction

- Access to the bridge and across the border will be maintained on the existing bridge while the new bridge is built
- Temporary road closures (maximum 15 minute closures) will be allowed to complete the tie-in of the new bridge to the Port of Entry facilities and roadway
- Sidewalk on the existing bridge will be removed for the construction of the new bridge
- Construction areas will be fenced off for safety of pedestrians during construction and decommissioning of the existing bridge
- Temporary (minor) noise impacts due to construction activities may occur. Timing restrictions for construction will be implemented throughout the contract
- Once the replacement bridge is complete, access across the border will be switched to the new bridge and the existing bridge will be decommissioned
- Construction staging and laydown areas will be confirmed by the Contractor prior to construction
- If private property is required for construction staging and laydown areas, the Contractor will work with property owners to make appropriate arrangements

## CONSTRUCTION PHASES



# Thank you for attending

To provide your comments,  
please fill out a comment sheet and place it in the comment box at today's  
meeting, or send your comments to:

**Nevena Gazibara, B.Sc., MREM**

Environmental Planner  
Stantec Consulting Ltd.  
200-835 Paramount Dr  
Stoney Creek ON L8J 0B4  
CANADA

☎ Tel: 905-381-2149

☎ Fax: 905-385-3534

✉ Email: [nevena.gazibara@stantec.com](mailto:nevena.gazibara@stantec.com)

*We would appreciate receiving your comments by  
November 2, 2017*

## Your input is important!

If you have any accessibility requirements in order to participate in this project,  
please contact one of the project team members listed above.

### Freedom of Information & Protection of Privacy Act

Comments and information regarding this study are being collected to satisfy the  
requirements of the *Environmental Assessment Act*, and in accordance with the  
*Freedom of Information and Protection of Privacy Act*.

With the exception of personal information, all comments will be part of the public record.

