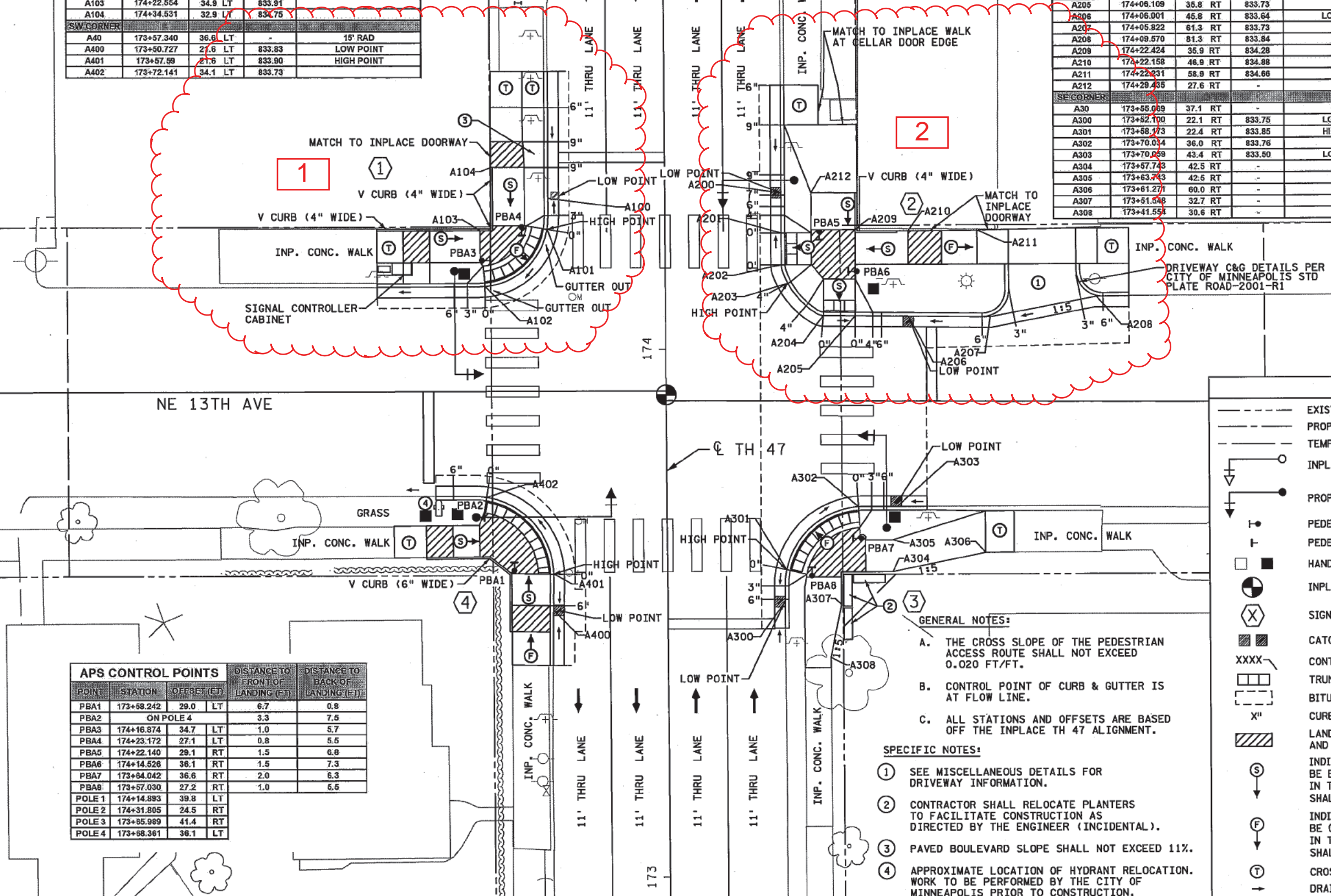


ADA SAMPLE CALCULATIONS

GUTTER CONTROL POINTS				
POINT NO.	STATION	OFFSET (FT)	ELEVATION	DESCRIPTION
NW CORNER				
A10	174+24.930	35.1 LT	-	13' RAD
A100	174+28.624	22.1 LT	833.81	LOW POINT
A101	174+22.737	22.3 LT	833.81	HIGH POINT 5% GUTTER OUT
A102	174+11.971	34.1 LT	833.68	5% GUTTER OUT
A103	174+22.554	34.9 LT	833.91	
A104	174+34.531	32.9 LT	833.75	
SW CORNER				
A40	173+57.340	36.6 LT	-	15' RAD
A400	173+50.727	27.6 LT	833.83	LOW POINT
A401	173+57.99	27.6 LT	833.90	HIGH POINT
A402	173+72.141	34.1 LT	833.73	

GUTTER CONTROL POINTS				
POINT NO.	STATION	OFFSET (FT)	ELEVATION	DESCRIPTION
NE CORNER				
A20	174+15.635	24.9 RT	-	3' RAD
A21	174+19.368	36.3 RT	-	15' RAD
A22	174+09.174	30.0 RT	-	3' RAD
A200	174+29.401	22.0 RT	833.74	LOW POINT
A201	174+21.857	21.9 RT	833.80	
A202	174+15.856	21.9 RT	833.84	
A203	174+12.336	23.1 RT	833.87	HIGH POINT
A204	174+06.187	29.7 RT	833.79	
A205	174+06.109	35.8 RT	833.73	
A206	174+06.001	45.8 RT	833.64	LOW POINT
A207	174+05.822	61.3 RT	833.73	
A208	174+09.570	81.3 RT	833.84	
A209	174+22.424	36.9 RT	834.28	
A210	174+22.158	48.9 RT	834.88	
A211	174+22.231	58.9 RT	834.66	
A212	174+29.435	27.6 RT	-	
SE CORNER				
A30	173+56.069	37.1 RT	-	15' RAD
A300	173+52.100	22.1 RT	833.75	LOW POINT
A301	173+58.173	22.4 RT	833.85	HIGH POINT
A302	173+70.034	36.0 RT	833.76	
A303	173+70.059	43.4 RT	833.50	LOW POINT
A304	173+57.743	42.5 RT	-	
A305	173+63.743	42.5 RT	-	
A306	173+61.271	60.0 RT	-	
A307	173+51.548	32.7 RT	-	
A308	173+41.554	30.6 RT	-	



APS CONTROL POINTS					DISTANCE TO FRONT OF LANDING (FT)		DISTANCE TO BACK OF LANDING (FT)	
POINT	STATION	OFFSET (FT)						
PBA1	173+58.242	29.0	LT	6.7	0.8			
PBA2	ON POLE 4			3.3	7.5			
PBA3	174+16.874	34.7	LT	1.0	5.7			
PBA4	174+23.172	27.1	LT	0.8	5.5			
PBA5	174+22.140	29.1	RT	1.5	6.8			
PBA6	174+14.526	36.1	RT	1.5	7.3			
PBA7	173+64.042	36.6	RT	2.0	6.3			
PBA8	173+57.030	27.2	RT	1.0	5.5			
POLE 1	174+14.893	39.8	LT					
POLE 2	174+31.805	24.5	RT					
POLE 3	173+65.989	41.4	RT					
POLE 4	173+68.361	36.1	LT					

- GENERAL NOTES:**
- THE CROSS SLOPE OF THE PEDESTRIAN ACCESS ROUTE SHALL NOT EXCEED 0.020 FT/FT.
 - CONTROL POINT OF CURB & GUTTER IS AT FLOW LINE.
 - ALL STATIONS AND OFFSETS ARE BASED OFF THE INPLACE TH 47 ALIGNMENT.

- SPECIFIC NOTES:**
- SEE MISCELLANEOUS DETAILS FOR DRIVEWAY INFORMATION.
 - CONTRACTOR SHALL RELOCATE PLANTERS TO FACILITATE CONSTRUCTION AS DIRECTED BY THE ENGINEER (INCIDENTAL).
 - PAVED BOULEVARD SLOPE SHALL NOT EXCEED 11%.
 - APPROXIMATE LOCATION OF HYDRANT RELOCATION. WORK TO BE PERFORMED BY THE CITY OF MINNEAPOLIS PRIOR TO CONSTRUCTION.

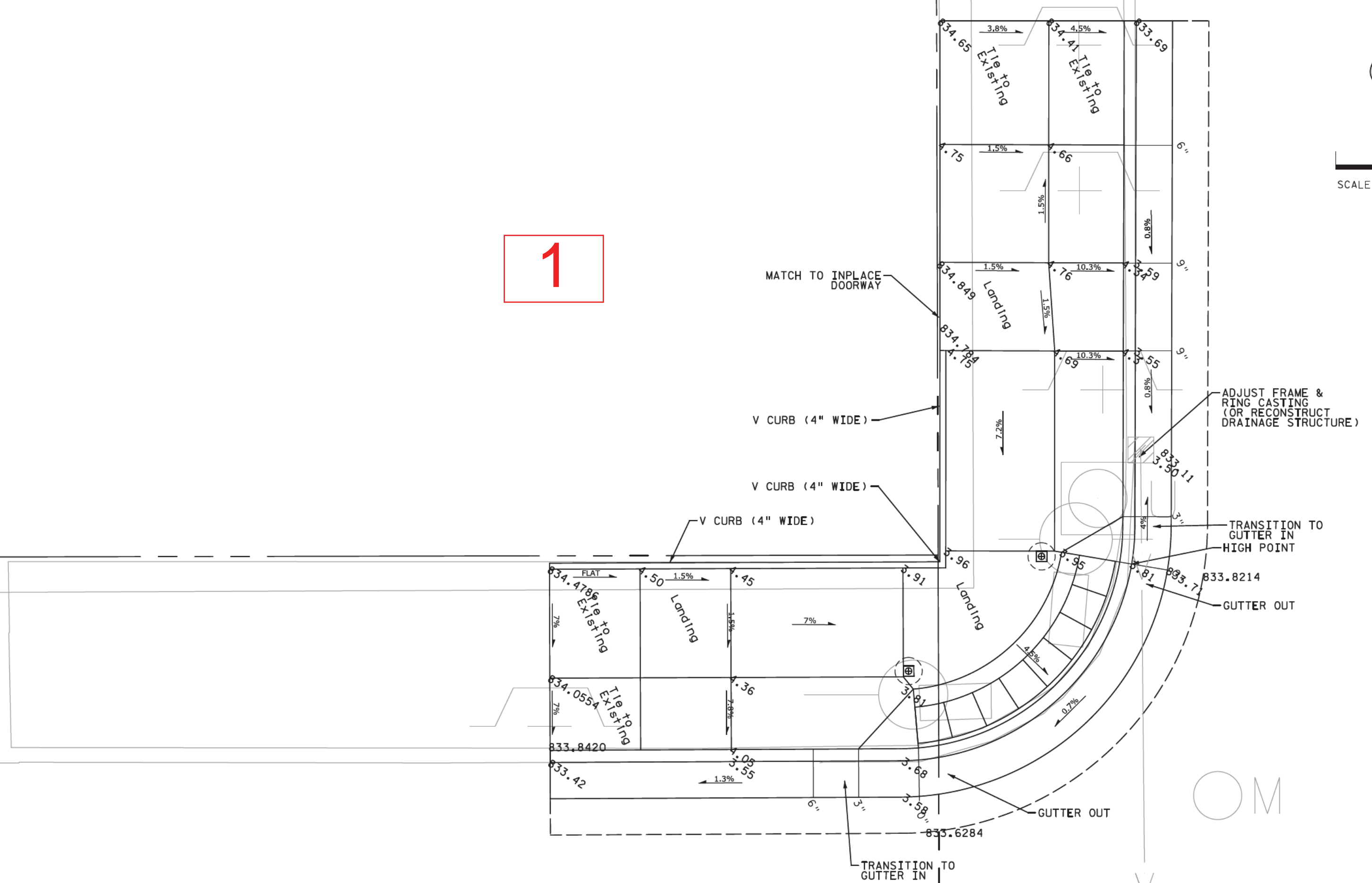
LEGEND

- EXISTING R/W
- PROPOSED R/W
- - - - - TEMPORARY EASEMENT
- INPLACE SIGNAL POLE
- PROPOSED SIGNAL POLE
- ⊙ PEDESTRIAN/BICYCLE PUSH BUTTON STATION
- ⊙ PEDESTRIAN PUSH BUTTON
- HANDHOLE (EXISTING/PROPOSED)
- ⊙ INPLACE SIGNAL
- ⊙ SIGNAL NUMBER
- ▨ CATCH BASIN (EXISTING/PROPOSED)
- XXXX CONTROL POINTS AT GUTTER FLOW LINE
- ▭ TRUNCATED DOMES (SEE STANDARD PLATE 7038)
- ▭ BITUMINOUS PAVING - SEE TABULATIONS
- X" CURB HEIGHT
- ▨ LANDING AREA - 4' X 4' MIN. DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS
- Ⓢ INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%
- Ⓣ INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%
- Ⓣ CROSS SLOPE TRANSITION PANEL(S)
- DRAINAGE FLOW ARROW

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1



UNIVERSITY AVE NE

NE 13TH AVE

OM



2

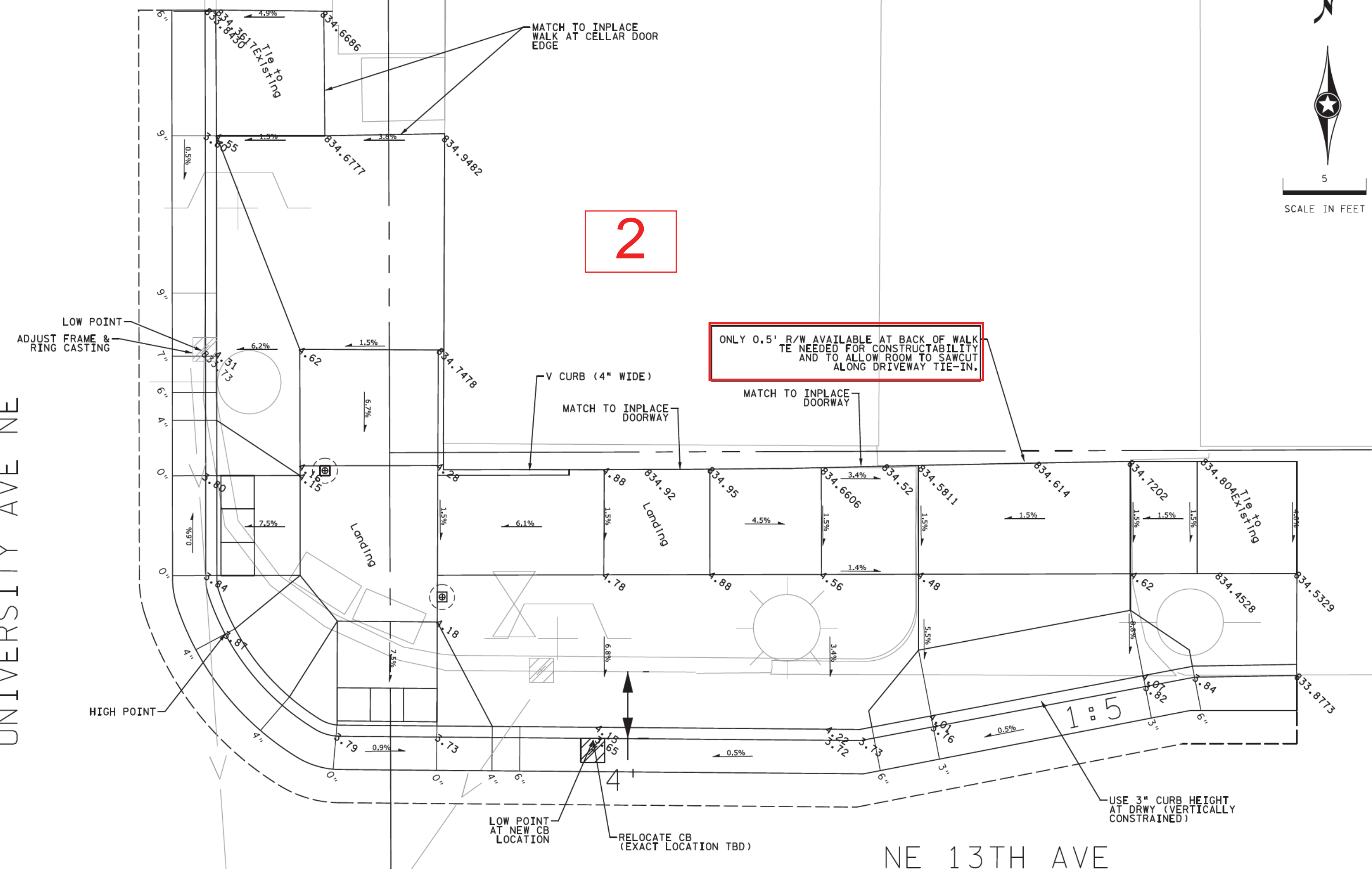
ONLY 0.5' R/W AVAILABLE AT BACK OF WALK
TE NEEDED FOR CONSTRUCTABILITY
AND TO ALLOW ROOM TO SAWCUT
ALONG DRIVEWAY TIE-IN.

UNIVERSITY AVE NE

NE 13TH AVE

LOW POINT
ADJUST FRAME &
RING
CASTING

MATCH TO INPLACE
WALK AT CELLAR DOOR
EDGE



USE 3" CURB HEIGHT
AT DRWY (VERTICALLY
CONSTRAINED)

LOW POINT
AT NEW CB
LOCATION
RELOCATE CB
(EXACT LOCATION TBD)

MATCH TO INPLACE
DOORWAY

MATCH TO INPLACE
DOORWAY

V CURB (4" WIDE)

HIGH POINT

LOW POINT
ADJUST FRAME &
RING
CASTING

UNIVERSITY AVE NE

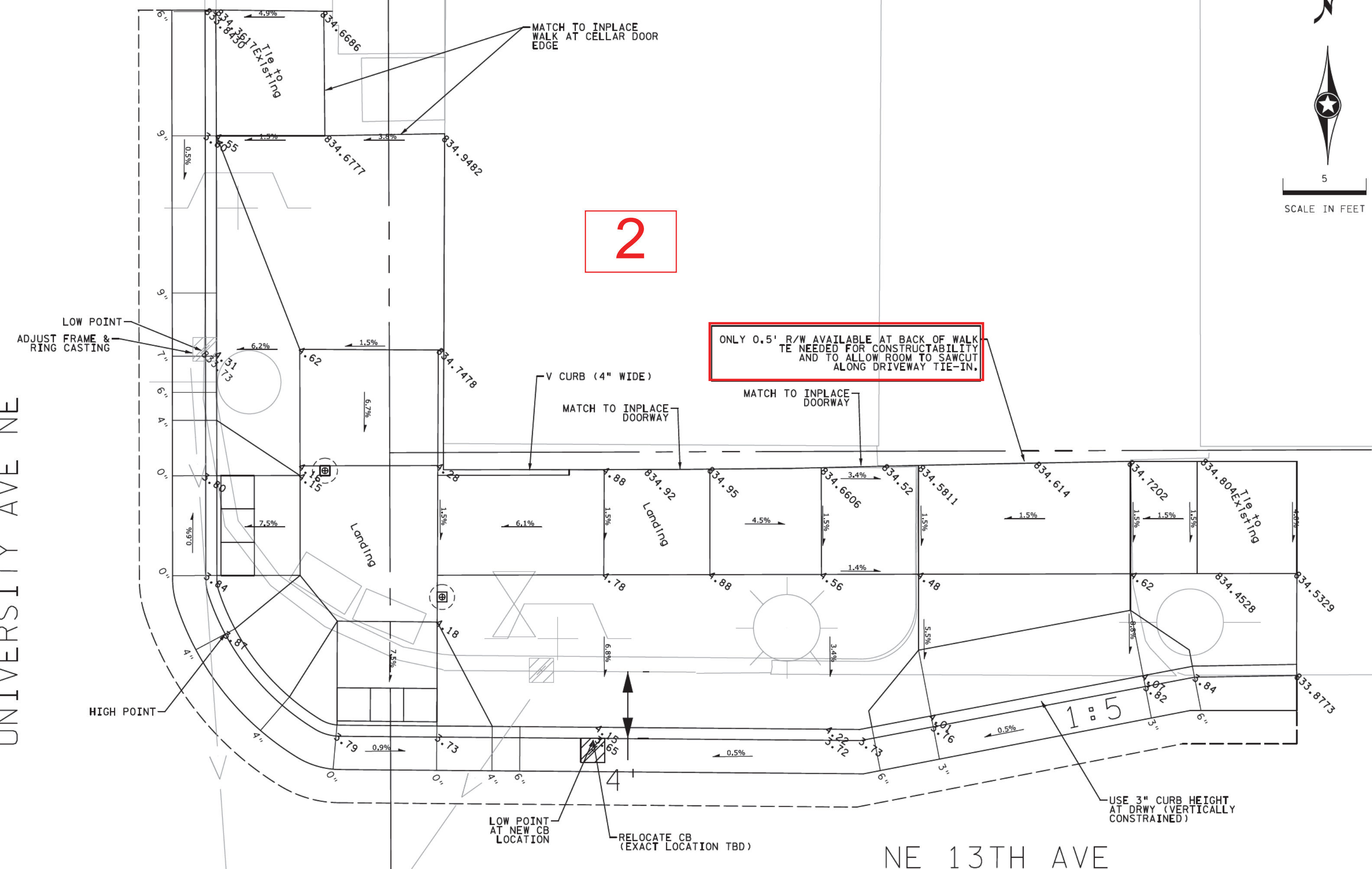
NE 13TH AVE



2

ONLY 0.5' R/W AVAILABLE AT BACK OF WALK
TE NEEDED FOR CONSTRUCTABILITY
AND TO ALLOW ROOM TO SAWCUT
ALONG DRIVEWAY TIE-IN.

MATCH TO INPLACE
WALK AT CELLAR DOOR
EDGE



USE 3" CURB HEIGHT
AT DRWY (VERTICALLY
CONSTRAINED)

LOW POINT
AT NEW CB
LOCATION
RELOCATE CB
(EXACT LOCATION TBD)

MATCH TO INPLACE
DOORWAY

MATCH TO INPLACE
DOORWAY

V CURB (4" WIDE)

HIGH POINT

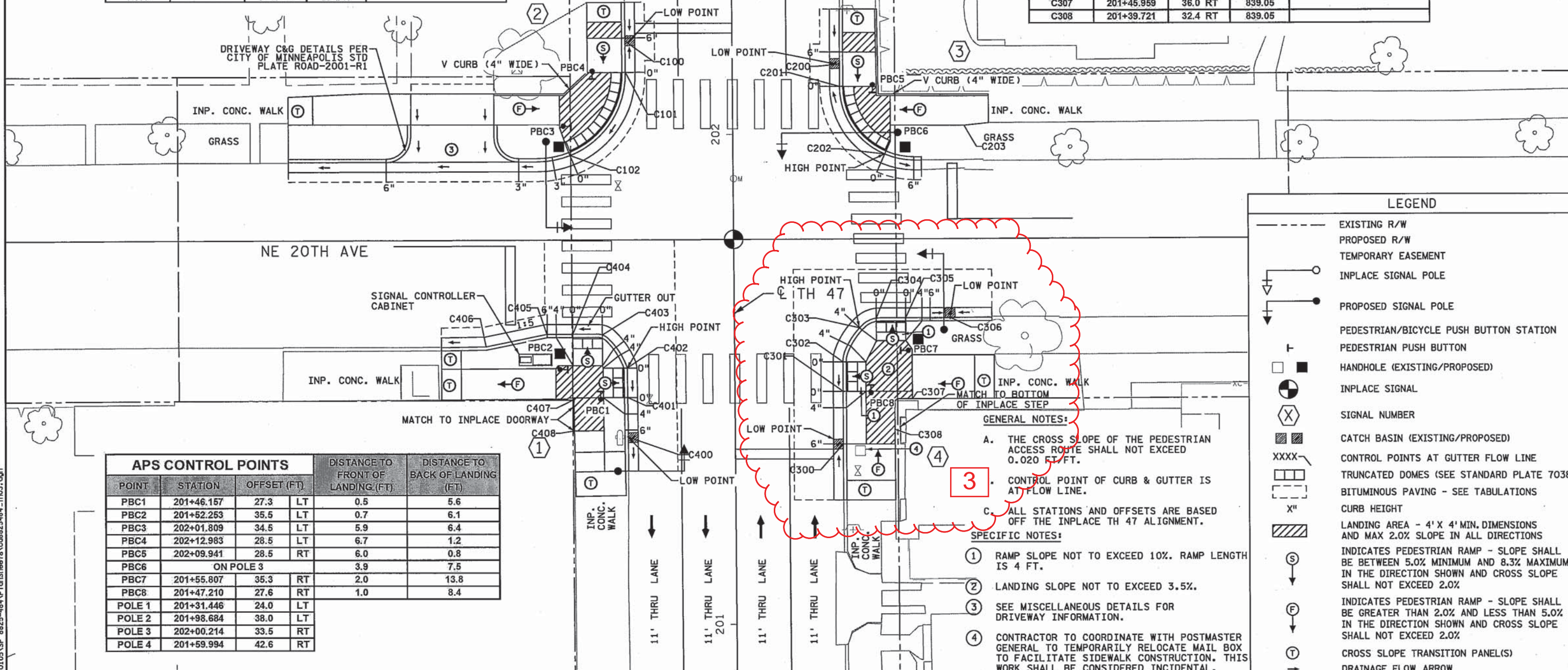
LOW POINT
ADJUST FRAME &
RING
CASTING

UNIVERSITY AVE NE

NE 13TH AVE

GUTTER CONTROL POINTS				
POINT NO.	STATION	OFFSET (FT)	ELEVATION	DESCRIPTION
NW CORNER				
C10	202+12.556	39.8 LT	-	18' RAD
C100	202+19.096	21.8 LT	837.93	LOW POINT
C101	202+12.727	21.8 LT	838.16	HIGH POINT
C102	201+95.828	33.1 LT	837.93	
SW CORNER				
C40	201+52.037	24.9 LT	-	3' RAD
C41	201+47.717	36.1 LT	-	15' RAD
C42	201+56.426	27.9 LT	-	3' RAD
C400	201+38.437	22.0 LT	837.80	LOW POINT
C401	201+46.476	21.9 LT	838.29	
C402	201+52.476	21.9 LT	838.31	HIGH POINT
C403	201+59.277	26.9 LT	838.16	5% GUTTER OUT
C404	201+59.427	32.9 LT	838.10	5% GUTTER OUT
C405	201+59.427	38.1 LT	838.02	
C406	201+56.950	50.5 LT	837.56	
C407	201+45.981	33.0 LT	838.69	
C408	201+39.701	32.8 LT	838.82	

GUTTER CONTROL POINTS				
POINT NO.	STATION	OFFSET (FT)	ELEVATION	DESCRIPTION
NE CORNER				
C20	202+09.604	36.8 RT	-	15' RAD
C200	202+14.231	21.9 RT	838.24	LOW POINT
C201	202+09.679	21.8 RT	838.28	
C202	201+95.845	30.8 RT	838.38	HIGH POINT
C203	202+01.620	47.1 RT	-	
SE CORNER				
C30	201+53.670	27.0 RT	-	5' RAD
C31	201+50.215	36.4 RT	-	15' RAD
C32	201+57.481	29.5 RT	-	5' RAD
C300	201+36.806	21.9 RT	838.30	LOW POINT
C301	201+47.525	22.0 RT	838.36	
C302	201+53.525	22.0 RT	838.40	
C303	201+60.471	25.4 RT	838.53	HIGH POINT 5% GUTTER OUT
C304	201+62.431	28.8 RT	838.45	5% GUTTER OUT
C305	201+62.476	24.8 RT	838.37	5% GUTTER OUT
C306	201+62.590	43.7 RT	838.05	LOW POINT
C307	201+45.959	36.0 RT	839.05	
C308	201+39.721	32.4 RT	839.05	



APS CONTROL POINTS					
POINT	STATION	OFFSET (FT)	DISTANCE TO FRONT OF LANDING (FT)	DISTANCE TO BACK OF LANDING (FT)	
PBC1	201+46.157	27.3	LT	0.5	5.6
PBC2	201+52.253	35.5	LT	0.7	6.1
PBC3	202+01.809	34.5	LT	5.9	6.4
PBC4	202+12.983	28.5	LT	6.7	1.2
PBC5	202+09.941	28.5	RT	6.0	0.8
PBC6	ON POLE 3			3.9	7.5
PBC7	201+55.807	35.3	RT	2.0	13.8
PBC8	201+47.210	27.6	RT	1.0	8.4
POLE 1	201+31.446	24.0	LT		
POLE 2	201+98.684	38.0	LT		
POLE 3	202+00.214	33.5	RT		
POLE 4	201+59.994	42.6	RT		

LEGEND

- EXISTING R/W
- - - PROPOSED R/W
- - - TEMPORARY EASEMENT
- INPLACE SIGNAL POLE
- PROPOSED SIGNAL POLE
- ⊥ PEDESTRIAN/BICYCLE PUSH BUTTON STATION
- ⊥ PEDESTRIAN PUSH BUTTON
- HANDHOLE (EXISTING/PROPOSED)
- ⊙ INPLACE SIGNAL
- ⊙ SIGNAL NUMBER
- ▨ CATCH BASIN (EXISTING/PROPOSED)
- XXXX CONTROL POINTS AT GUTTER FLOW LINE
- ▭ TRUNCATED DOMES (SEE STANDARD PLATE 7038)
- - - BITUMINOUS PAVING - SEE TABULATIONS
- X" CURB HEIGHT
- ▨ LANDING AREA - 4' X 4' MIN. DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS
- ⊙ INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%
- ⊙ INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%
- ⊙ CROSS SLOPE TRANSITION PANEL(S)
- DRAINAGE FLOW ARROW

- GENERAL NOTES:**
- A. THE CROSS SLOPE OF THE PEDESTRIAN ACCESS ROUTE SHALL NOT EXCEED 0.020 FT/FT.
 - B. CONTROL POINT OF CURB & GUTTER IS AT FLOW LINE.
 - C. ALL STATIONS AND OFFSETS ARE BASED OFF THE INPLACE TH 47 ALIGNMENT.
- SPECIFIC NOTES:**
- ① RAMP SLOPE NOT TO EXCEED 10%. RAMP LENGTH IS 4 FT.
 - ② LANDING SLOPE NOT TO EXCEED 3.5%.
 - ③ SEE MISCELLANEOUS DETAILS FOR DRIVEWAY INFORMATION.
 - ④ CONTRACTOR TO COORDINATE WITH POSTMASTER GENERAL TO TEMPORARILY RELOCATE MAIL BOX TO FACILITATE SIDEWALK CONSTRUCTION. THIS WORK SHALL BE CONSIDERED INCIDENTAL.

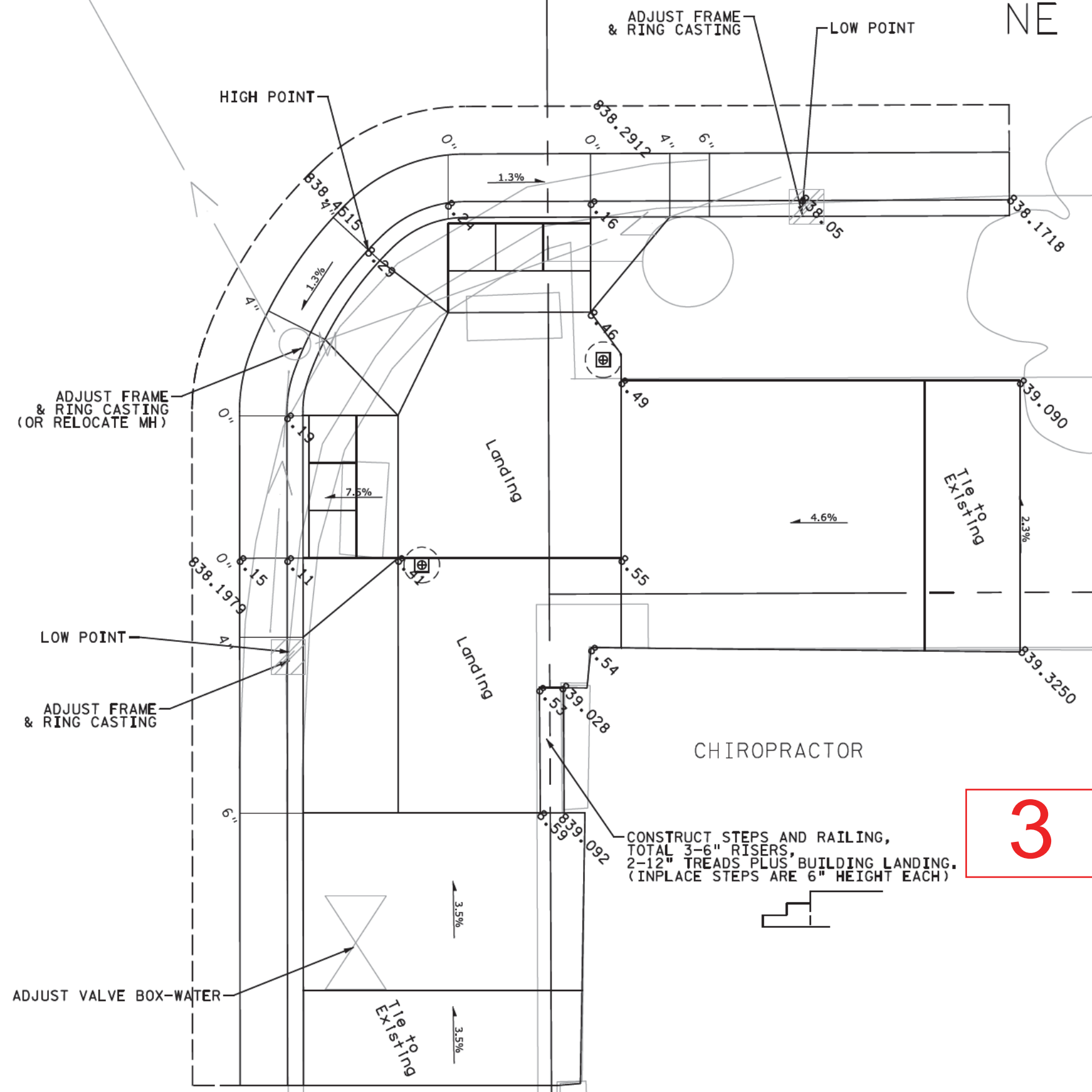
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UNIVERSITY AVE NE

NE 20TH AVE



5
SCALE IN FEET



3

GUTTER CONTROL POINTS					
POINT NO.	STATION	OFFSET (FT)	ELEVATION	DESCRIPTION	
SW CORNER					
H10	66+50.8	35.0 LT	-	5' RAD	
H11	66+40.9	46.3 LT	-	20' RAD	
H12	66+55.8	44.6 LT	-	5' RAD	
H100	66+14.7	35.0 LT	849.36		
H101	66+24.9	33.0 LT	849.31	LOW POINT	
H102	66+39.8	30.0 LT	849.46		
H103	66+44.8	30.0 LT	849.51		
H104	66+50.8	30.0 LT	849.57		
H105	66+60.8	44.7 LT	849.73	HIGH POINT	
H106	66+60.8	50.7 LT	849.67	2% GUTTER OUT	
H107	66+60.7	55.7 LT	849.59	4% GUTTER OUT	
H108	66+56.5	80.5 LT	849.18		
H109	66+42.8	50.0 LT	850.25		
NW CORNER					
H20	66+92.8	43.9 LT	-	5' RAD	
H21	67+05.2	52.4 LT	-	20' RAD	
H22	66+96.8	40.0 LT	-	5' RAD	
H200	66+87.8	49.9 LT	849.65	5% GUTTER OUT	
H201	66+87.8	43.9 LT	849.71	HIGH POINT	
H202	66+96.8	35.0 LT	849.63	0.5% GUTTER OUT	
H203	67+02.8	35.0 LT	849.57	4% GUTTER OUT	
H204	67+10.7	35.0 LT	849.23	LOW POINT	
H205	67+00.3	50.0 LT	850.08		

GUTTER CONTROL POINTS					
POINT NO.	STATION	OFFSET (FT)	ELEVATION	DESCRIPTION	
NE CORNER					
H30	66+97.9	35.0 RT	-	5' RAD	
H31	67+07.5	46.5 RT	-	20' RAD	
H32	66+92.7	44.3 RT	-	5' RAD	
H300	67+36.5	35.0 RT	849.18	LOW POINT	
H301	67+35.5	35.0 RT	849.19		
H302	67+08.9	30.0 RT	849.51		
H303	67+03.9	30.0 RT	849.57		
H304	66+97.9	30.0 RT	849.63		
H305	66+90.0	36.8 RT	849.78	HIGH POINT	
H306	66+87.7	44.3 RT	849.66	2% GUTTER OUT	
H307	66+87.7	50.3 RT	849.57	5% GUTTER OUT	
H308	66+87.8	55.3 RT	849.46		
H309	66+90.4	67.8 RT	849.19		
H310	66+90.4	68.8 RT	849.17	LOW POINT	
H311	67+01.9	49.2 RT	850.11		
SE CORNER					
H40	66+56.6	42.9 RT	-	5' RAD	
H41	66+46.3	53.8 RT	-	20' RAD	
H42	66+52.2	40.0 RT	-	5' RAD	
H400	66+57.5	77.4 RT	849.35	LOW POINT	
H401	66+57.5	76.4 RT	849.41		
H402	66+61.7	53.9 RT	849.57		
H403	66+61.7	48.9 RT	849.62		
H404	66+61.6	42.9 RT	849.68		
H405	66+52.2	35.0 RT	849.75	HIGH POINT	
H406	66+46.2	35.0 RT	849.69	1.75% GUTTER OUT	
				3.75% GUTTER OUT	

APS CONTROL POINTS				DISTANCE TO FRONT OF LANDING (FT)	DISTANCE TO BACK OF LANDING (FT)
PBH1	On Pole 1			2.0	8.9
PBH2	66+52.1	50.6	LT	2.0	7.3
PBH3	On Pole 2			1.3	6.0
PBH4	67+02.8	43.7	LT	2.0	6.3
PBH5	On Pole 3			2.0	10.5
PBH6	66+95.9	50.2	RT	2.0	6.0
PBH7	On Pole 4			2.0	7.3
PBH8	66+46.2	41.7	RT	2.0	7.3
Pole 1	66+44.8	41.7	LT		
Pole 2	66+94.3	49.9	LT		
Pole 3	67+03.9	38.7	RT		
Pole 4	66+53.5	48.9	RT		

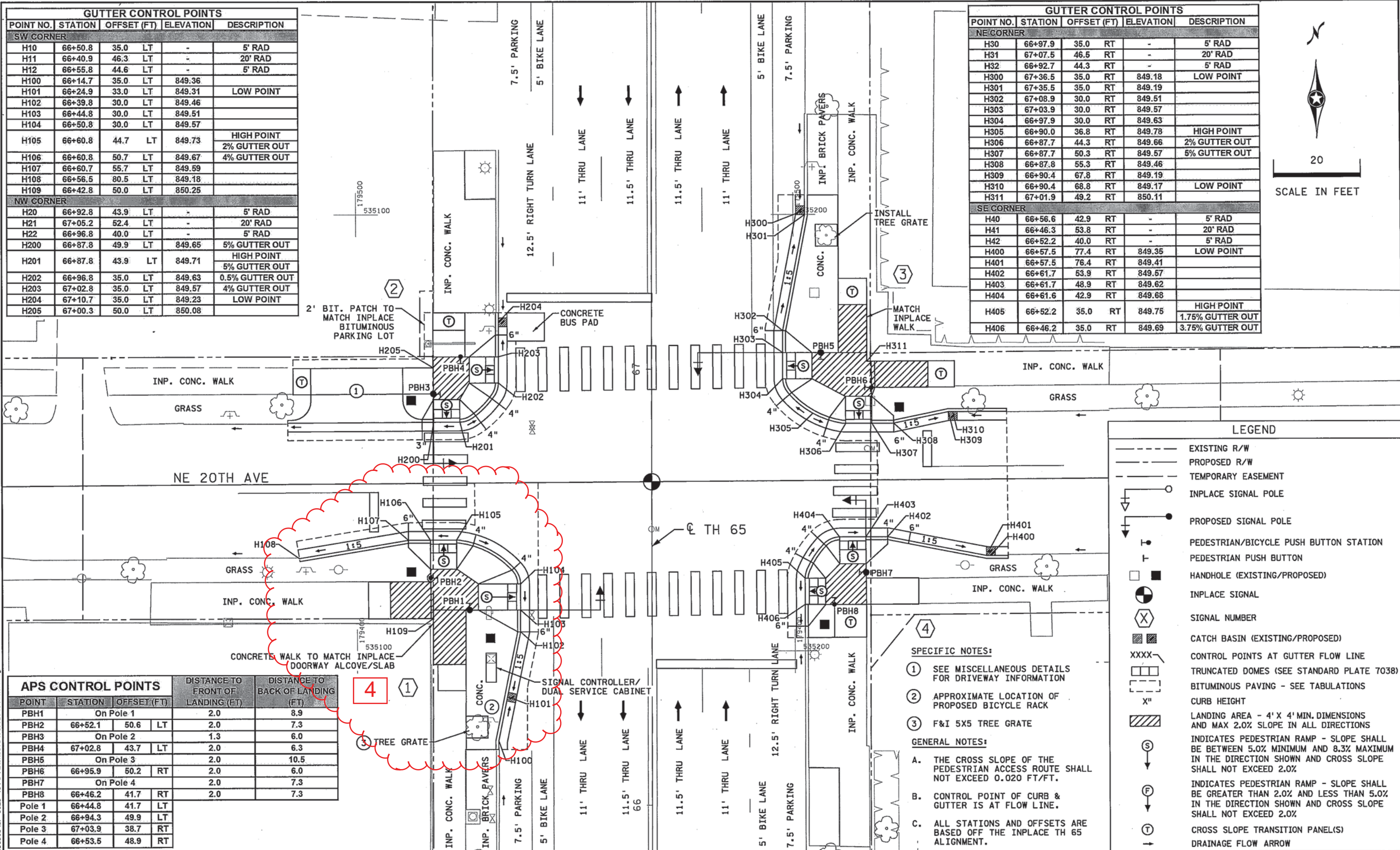


LEGEND

- EXISTING R/W
- - - PROPOSED R/W
- - - TEMPORARY EASEMENT
- INPLACE SIGNAL POLE
- PROPOSED SIGNAL POLE
- ⊙ PEDESTRIAN/BICYCLE PUSH BUTTON STATION
- ⊙ PEDESTRIAN PUSH BUTTON
- HANDHOLE (EXISTING/PROPOSED)
- ⊙ INPLACE SIGNAL
- ⊙ SIGNAL NUMBER
- ▨ CATCH BASIN (EXISTING/PROPOSED)
- XXXX CONTROL POINTS AT GUTTER FLOW LINE
- ▭ TRUNCATED DOMES (SEE STANDARD PLATE 7038)
- - - BITUMINOUS PAVING - SEE TABULATIONS
- X" CURB HEIGHT
- ▨ LANDING AREA - 4' X 4' MIN. DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS
- Ⓢ INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%
- Ⓟ INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%
- Ⓣ CROSS SLOPE TRANSITION PANEL(S)
- DRAINAGE FLOW ARROW

- SPECIFIC NOTES:**
- SEE MISCELLANEOUS DETAILS FOR DRIVEWAY INFORMATION
 - APPROXIMATE LOCATION OF PROPOSED BICYCLE RACK
 - F&I 5X5 TREE GRATE

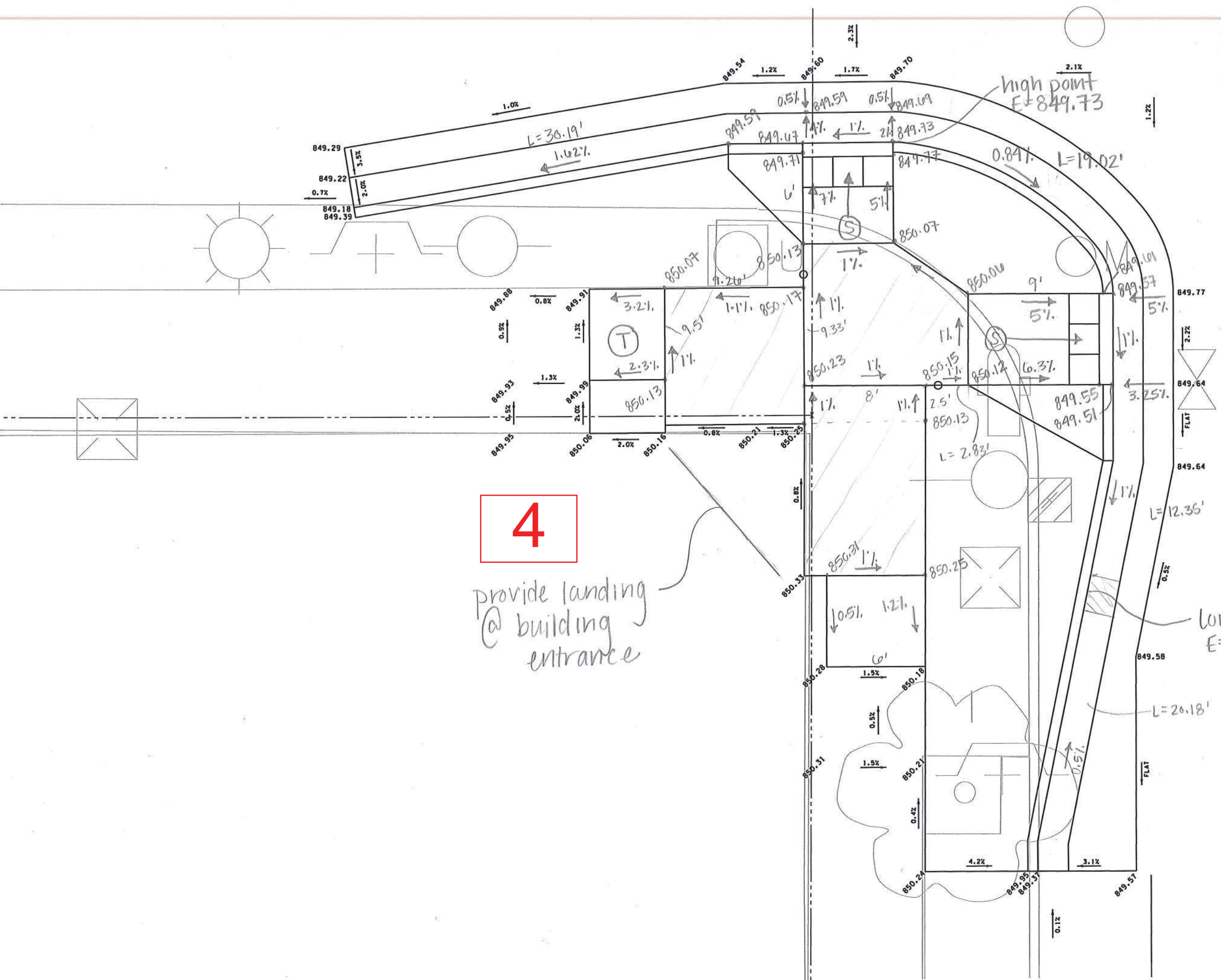
- GENERAL NOTES:**
- THE CROSS SLOPE OF THE PEDESTRIAN ACCESS ROUTE SHALL NOT EXCEED 0.020 FT/FT.
 - CONTROL POINT OF CURB & GUTTER IS AT FLOW LINE.
 - ALL STATIONS AND OFFSETS ARE BASED OFF THE INPLACE TH 65 ALIGNMENT.



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↑ N
↳ SCALE

20th Ave
SW Quadrant
6/28/16



4

provide landing @ building entrance

low point
E=849.39

high point
E=849.73

GENERAL NOTES

1. MAINTAIN A MINIMUM 4' WIDE PEDESTRIAN ACCESS ROUTE OBSTRUCTION TO OBSTRUCTION AND/OR OBSTRUCTION TO EDGE OF WALK.
2. THE CROSS SLOPE OF THE PEDESTRIAN ACCESS ROUTE SHALL NOT EXCEED 0.020 FT/FT.
3. SEE SHEET NO. 20 THRU 24 FOR PEDESTRIAN RAMP DETAILS.
4. CONTROL POINT AT CURB AND GUTTER IS AT FLOW LINE.
5. PANEL LAYOUT AND JOINT TYPE IN THIS PLAN MAY BE CHANGED IF APPROVED BY THE ENGINEER.
6. UTILIZE 15' UNIFORM PANEL JOINT SPACING.
7. CONSTRUCTION HEADER JOINTS, CONTRACTION JOINTS, AND LONGITUDINAL JOINTS SHALL BE CONSIDERED INCIDENTAL AND NO DIRECT COMPENSATION WILL BE MADE.
8. REINFORCEMENT BARS FOR ALL LONGITUDINAL JOINTS ARE CONSIDERED INCIDENTAL AND NO DIRECT COMPENSATION WILL BE MADE.
9. BETWEEN PROPOSED CONCRETE PAVEMENT AND INPLACE CONCRETE PAVEMENT, ALONG LONGITUDINAL JOINTS DRILL AND GROUT EPOXY COATED REINFORCEMENT BARS.
10. FOR PEDESTRIAN RAMP SLOPES GREATER THAN 8.3% THE MAXIMUM CROSS SLOPE IS 0.5%.

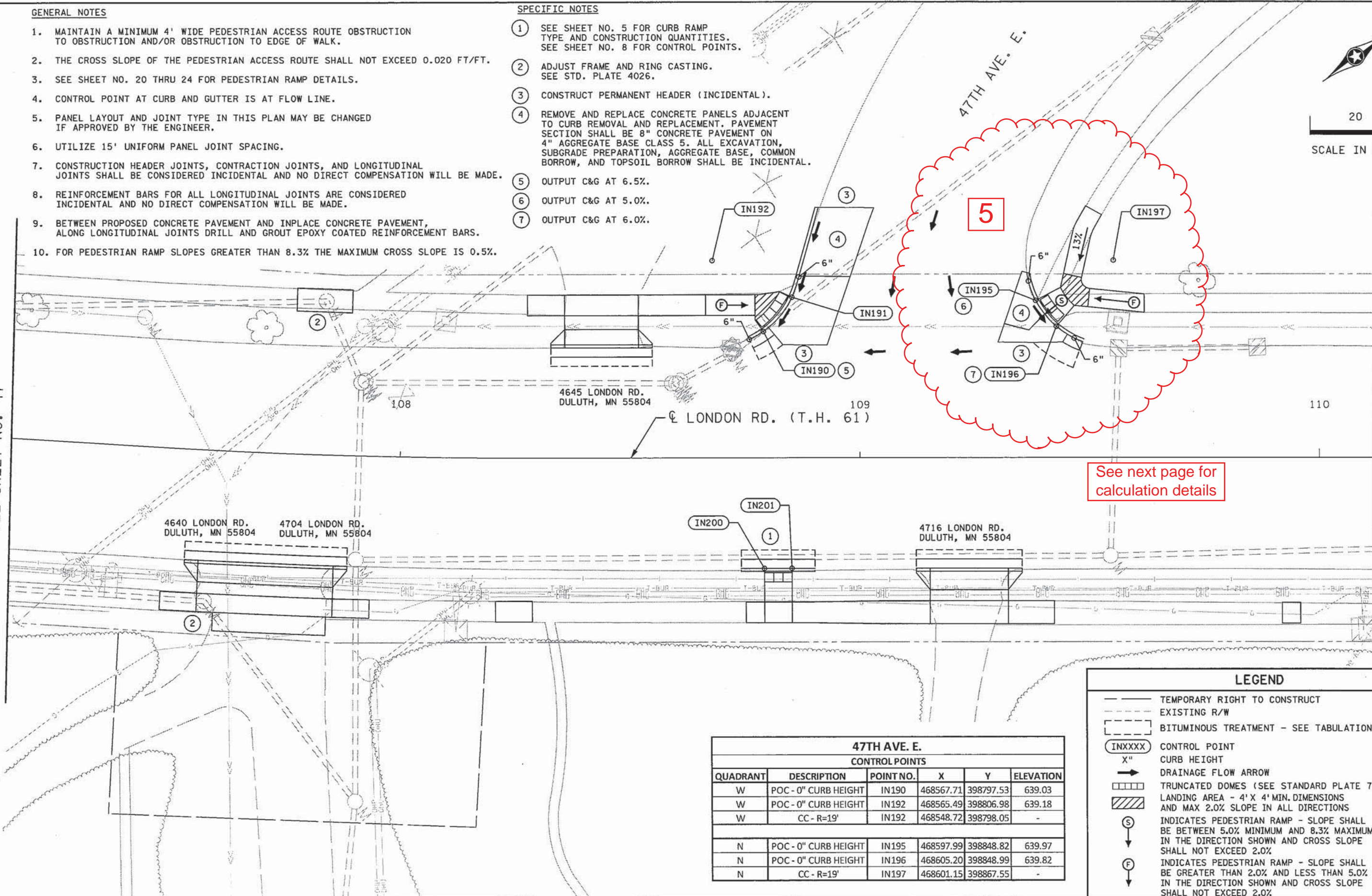
SPECIFIC NOTES

- ① SEE SHEET NO. 5 FOR CURB RAMP TYPE AND CONSTRUCTION QUANTITIES. SEE SHEET NO. 8 FOR CONTROL POINTS.
- ② ADJUST FRAME AND RING CASTING. SEE STD. PLATE 4026.
- ③ CONSTRUCT PERMANENT HEADER (INCIDENTAL).
- ④ REMOVE AND REPLACE CONCRETE PANELS ADJACENT TO CURB REMOVAL AND REPLACEMENT. PAVEMENT SECTION SHALL BE 8" CONCRETE PAVEMENT ON 4" AGGREGATE BASE CLASS 5. ALL EXCAVATION, SUBGRADE PREPARATION, AGGREGATE BASE, COMMON BORROW, AND TOPSOIL BORROW SHALL BE INCIDENTAL.
- ⑤ OUTPUT C&G AT 6.5%.
- ⑥ OUTPUT C&G AT 5.0%.
- ⑦ OUTPUT C&G AT 6.0%.



MATCHLINE C
SEE SHEET NO. 47

MATCHLINE D
SEE SHEET NO. 49



See next page for calculation details

47TH AVE. E.					
CONTROL POINTS					
QUADRANT	DESCRIPTION	POINT NO.	X	Y	ELEVATION
W	POC - 0" CURB HEIGHT	IN190	468567.71	398797.53	639.03
W	POC - 0" CURB HEIGHT	IN192	468565.49	398806.98	639.18
W	CC - R=19'	IN192	468548.72	398798.05	-
N	POC - 0" CURB HEIGHT	IN195	468597.99	398848.82	639.97
N	POC - 0" CURB HEIGHT	IN196	468605.20	398848.99	639.82
N	CC - R=19'	IN197	468601.15	398867.55	-

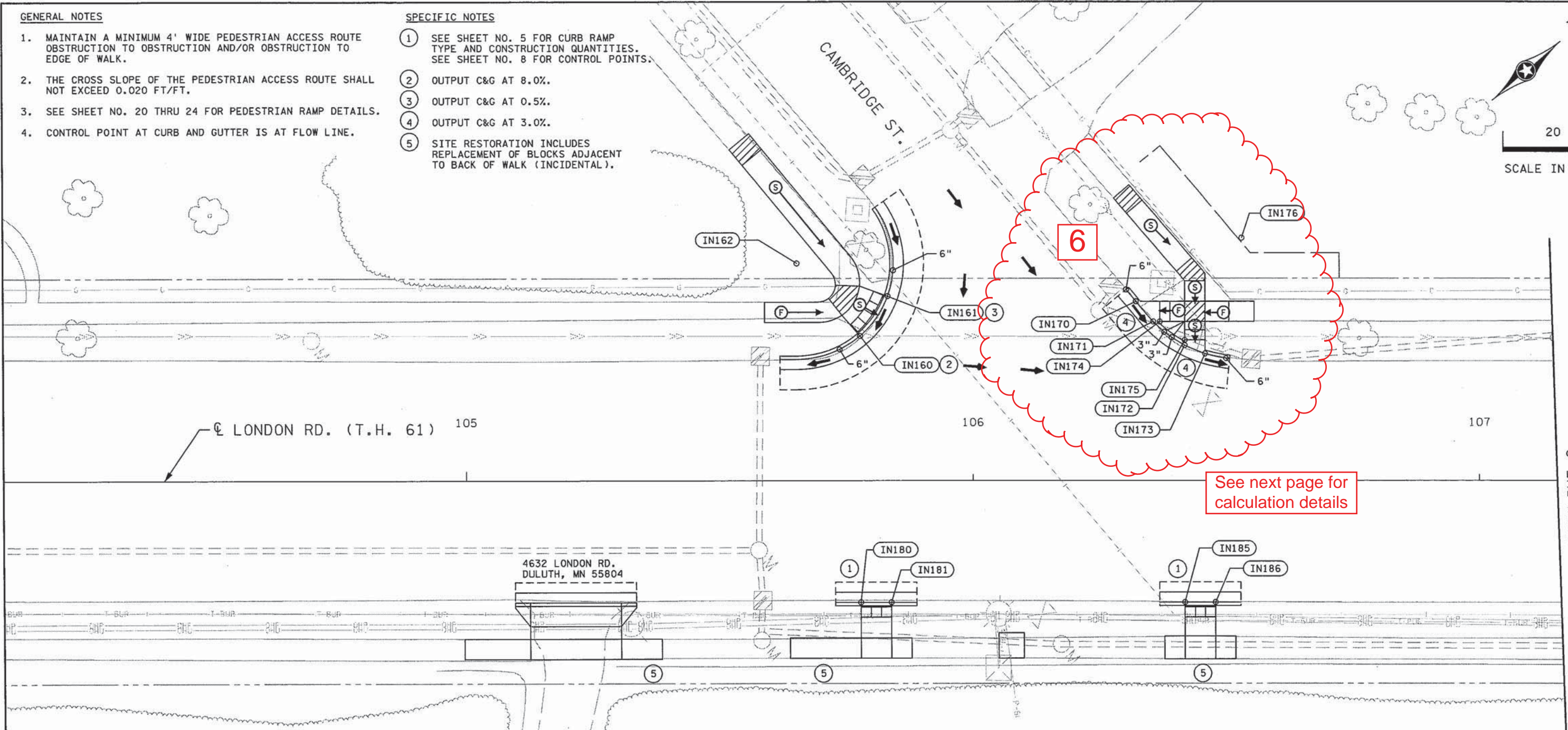
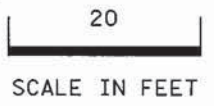
LEGEND	
	TEMPORARY RIGHT TO CONSTRUCT
	EXISTING R/W
	BITUMINOUS TREATMENT - SEE TABULATIONS
	CONTROL POINT
	CURB HEIGHT
	DRAINAGE FLOW ARROW
	TRUNCATED DOMES (SEE STANDARD PLATE 7038)
	LANDING AREA - 4' X 4' MIN. DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS
	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%
	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%

GENERAL NOTES

1. MAINTAIN A MINIMUM 4' WIDE PEDESTRIAN ACCESS ROUTE OBSTRUCTION TO OBSTRUCTION AND/OR OBSTRUCTION TO EDGE OF WALK.
2. THE CROSS SLOPE OF THE PEDESTRIAN ACCESS ROUTE SHALL NOT EXCEED 0.020 FT/FT.
3. SEE SHEET NO. 20 THRU 24 FOR PEDESTRIAN RAMP DETAILS.
4. CONTROL POINT AT CURB AND GUTTER IS AT FLOW LINE.

SPECIFIC NOTES

1. SEE SHEET NO. 5 FOR CURB RAMP TYPE AND CONSTRUCTION QUANTITIES. SEE SHEET NO. 8 FOR CONTROL POINTS.
2. OUTPUT C&G AT 8.0%.
3. OUTPUT C&G AT 0.5%.
4. OUTPUT C&G AT 3.0%.
5. SITE RESTORATION INCLUDES REPLACEMENT OF BLOCKS ADJACENT TO BACK OF WALK (INCIDENTAL).



MATCHLINE C
SEE SHEET NO. 48

CAMBRIDGE ST. CONTROL POINTS					
QUADRANT	DESCRIPTION	POINT NO.	X	Y	ELEVATION
W	POC - 0" CURB HEIGHT	IN160	468376.09	398567.39	636.31
W	POC - 0" CURB HEIGHT	IN161	468373.65	398576.63	636.50
W	CC - R=19'	IN162	468357.09	398567.33	-
N	POC - 0" CURB HEIGHT	IN170	468406.38	398613.12	636.08
N	POC - 0" CURB HEIGHT	IN171	468411.62	398613.07	636.04
N	POC - 0" CURB HEIGHT	IN172	468419.23	398614.72	635.99
N	POC - 0" CURB HEIGHT	IN173	468423.10	398616.67	635.96
N	POT - TRUNCATED DOMES	IN174	468412.46	398614.05	-
N	POT - TRUNCATED DOMES	IN175	468418.44	398615.40	-
N	CC - R=24'	IN176	468410.42	398637.04	-

LEGEND

- TEMPORARY RIGHT TO CONSTRUCT
- - - EXISTING R/W
- [] BITUMINOUS TREATMENT - SEE TABULATIONS
- INXXXX CONTROL POINT
- X" CURB HEIGHT
- DRAINAGE FLOW ARROW
- [] TRUNCATED DOMES (SEE STANDARD PLATE 7038)
- [] LANDING AREA - 4' X 4' MIN. DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS
- (S) INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%
- (F) INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%

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A. Tuttleworth

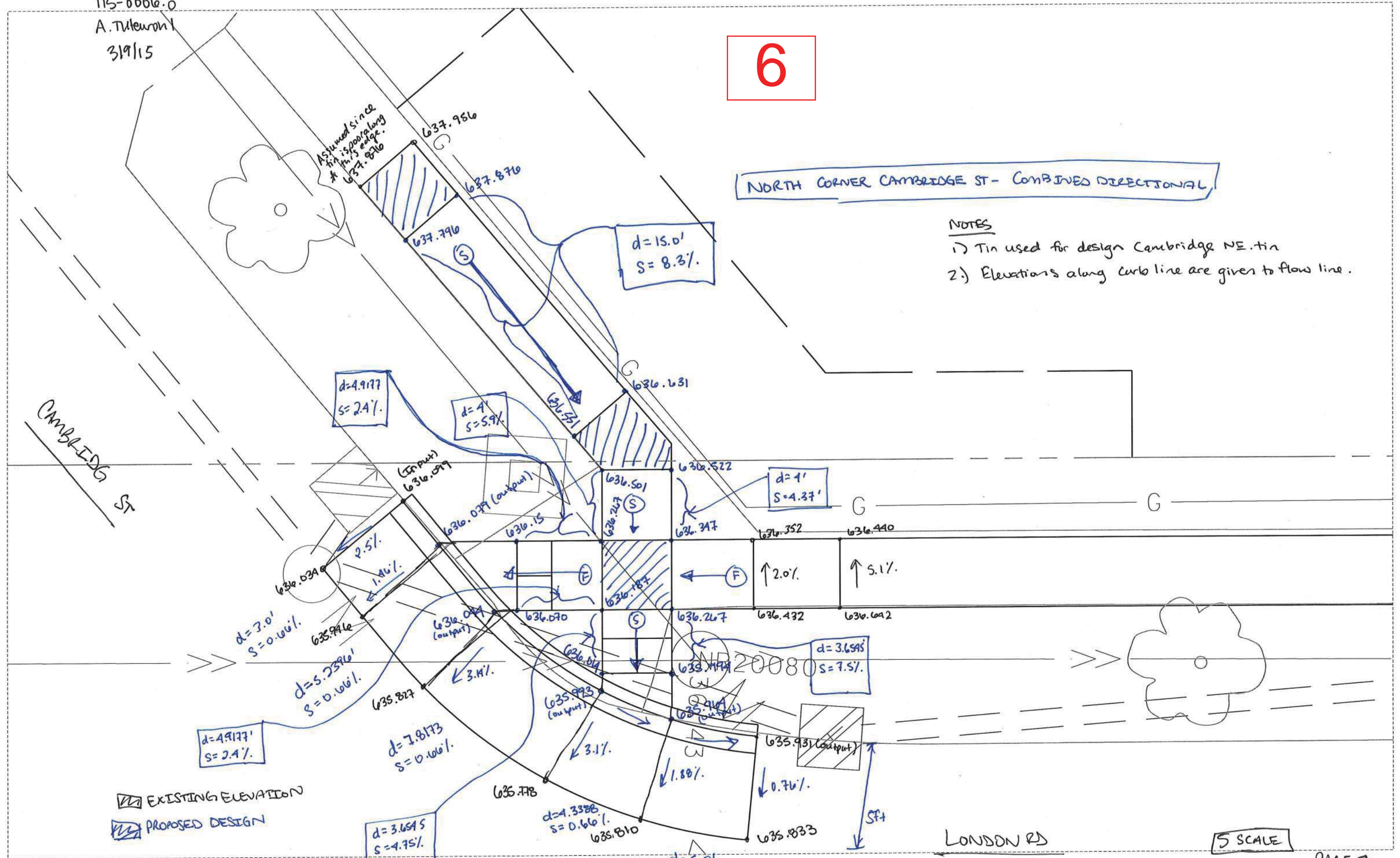
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NORTH CORNER CAMBRIDGE ST - COMBINED DIRECTIONAL

NOTES

- 1) Tin used for design Cambridge NE tin
- 2) Elevations along curb line are given to flow line.

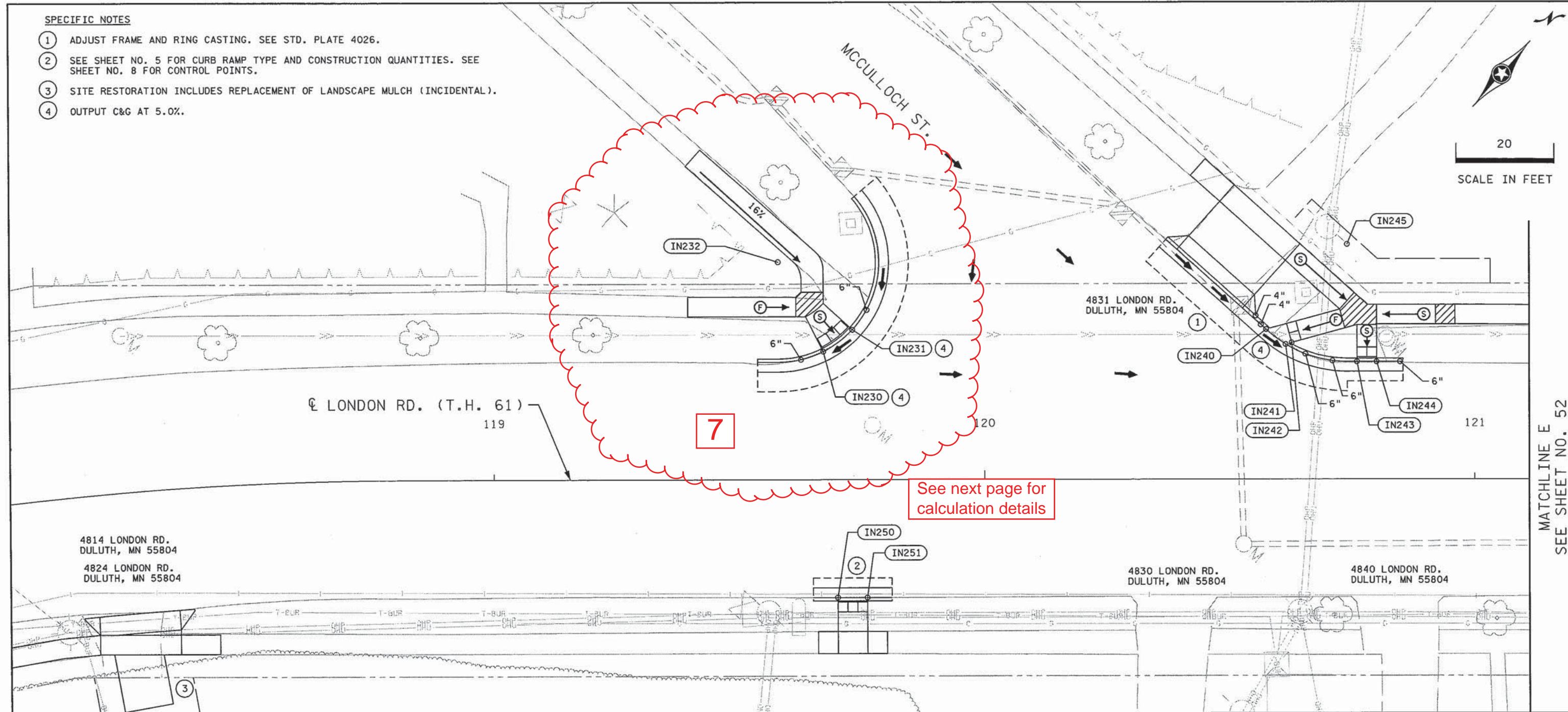
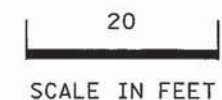


EXISTING ELEVATION
 PROPOSED DESIGN

5 SCALE

SPECIFIC NOTES

- 1 ADJUST FRAME AND RING CASTING. SEE STD. PLATE 4026.
- 2 SEE SHEET NO. 5 FOR CURB RAMP TYPE AND CONSTRUCTION QUANTITIES. SEE SHEET NO. 8 FOR CONTROL POINTS.
- 3 SITE RESTORATION INCLUDES REPLACEMENT OF LANDSCAPE MULCH (INCIDENTAL).
- 4 OUTPUT C&G AT 5.0%.



MATCHLINE E
SEE SHEET NO. 52

MCCULLOCH ST.					
CONTROL POINTS					
QUADRANT	DESCRIPTION	POINT NO.	X	Y	ELEVATION
W	POC - 0" CURB HEIGHT	IN230	469251.00	399647.40	653.21
W	POC - 0" CURB HEIGHT	IN231	469252.50	399654.64	653.36
W	CC - R=20.5'	IN232	469232.00	399655.10	-
NNE	POC - 0" CURB HEIGHT	IN240	469315.82	399710.25	652.90
NNE	POC - 0" CURB HEIGHT	IN241	469320.83	399710.58	652.82
NNE	POT - TRUNCATED DOMES	IN242	469321.54	399711.65	-
NNE	POT - 0" CURB HEIGHT	IN243	469334.14	399717.53	652.58
NNE	POT - 0" CURB HEIGHT	IN244	469337.14	399720.17	652.52
NNE	CC - R=24'	IN245	469316.80	399734.24	-

GENERAL NOTES

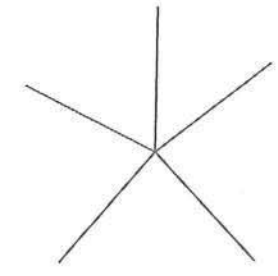
1. MAINTAIN A MINIMUM 4' WIDE PEDESTRIAN ACCESS ROUTE OBSTRUCTION TO OBSTRUCTION AND/OR OBSTRUCTION TO EDGE OF WALK.
2. THE CROSS SLOPE OF THE PEDESTRIAN ACCESS ROUTE SHALL NOT EXCEED 0.020 FT/FT.
3. SEE SHEET NO. 20 THRU 24 FOR PEDESTRIAN RAMP DETAILS.
4. CONTROL POINT AT CURB AND GUTTER IS AT FLOW LINE.
5. FOR PEDESTRIAN RAMP SLOPES GREATER THAN 8.3% THE MAXIMUM CROSS SLOPE IS 0.5%.

LEGEND

- TEMPORARY RIGHT TO CONSTRUCT
- EXISTING R/W
- BITUMINOUS TREATMENT - SEE TABULATIONS
- CONTROL POINT
- CURB HEIGHT
- DRAINAGE FLOW ARROW
- TRUNCATED DOMES (SEE STANDARD PLATE 7038)
- LANDING AREA - 4' X 4' MIN. DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS
- INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%
- INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%

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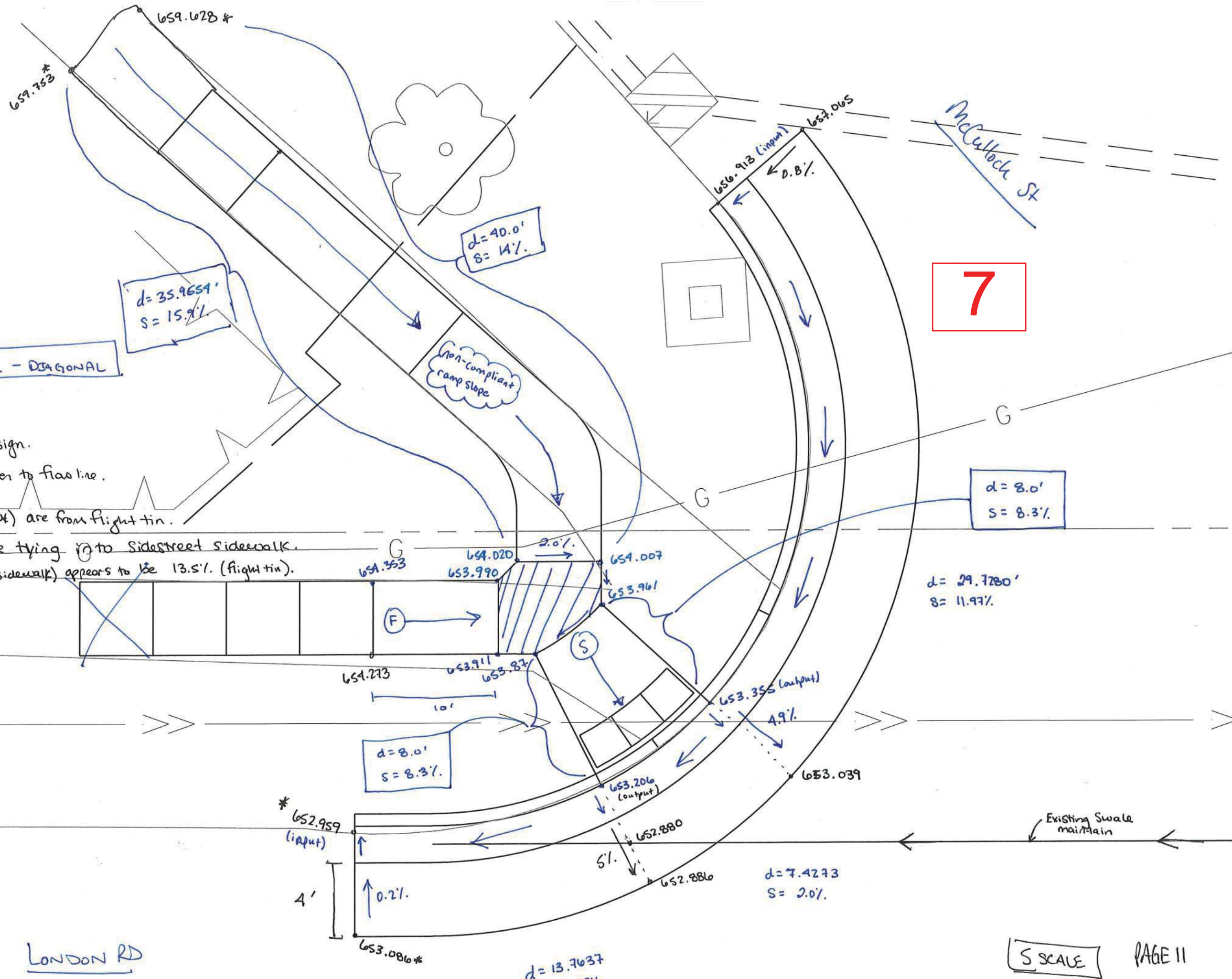
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WEST CORNER McCULLOCH ST. - DIAGONAL

NOTES

- 1.) used tin McCullNW.tin for design.
- 2.) Elevations along curb are given to finish.
- 3.) Elevations denoted with (*) are from flight tin.
- 4.) Non-compliant ramp slope tying to sidewalk sidewalk. Existing sidewalk grade (sidewalk) appears to be 13.5% (flight tin).



EXISTING ELEVATION
 PROPOSED DESIGN

LONDON RD

SCALE PAGE 11